

UNIVERSITY PLACE

DESIGN STANDARDS

November 13, 2020



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I. INTRODUCTION

INTRODUCTION

INTENT, VISION & FRAMEWORK

INTENT

The Special Use Permit (“SUP”) modification is intended to accommodate the evolution of a traditional enclosed mall into a vibrant mixed use community. The owner intends to preserve existing buildings and infrastructure where possible while maintaining enough flexibility to develop new improvements in a manner and at a time that is responsive to market demand. The owner seeks to create a public realm with the pedestrian experience in mind. The owner proposes to modify and create an internal street network through the site to create a more complete street network that will allow pedestrian, bicycle and automobile traffic to travel through the site. The overall development plan is intended to allow the site to evolve with the surrounding area, resulting in a well-designed, quality experience for the visitors, employees and residents, as well as the larger community. These Design Standards provide the basic framework for the development plan, addressing allowable uses, street frontages, building massing, building locations, entry locations and glazing/transparency of each building.

VISION

The redevelopment will transform University Place into a walkable town center and introduce new uses to complement the existing commercial uses. To realize this vision, parts of the existing mall will be retained and other parts may be demolished over time to make room for new buildings and outdoor spaces. Those parts retained will be reconfigured to convert space internally focused into new outward facing shops and restaurants designed for the pedestrian. Several public gathering spaces will be created of varying scale with everything from smaller semi-private areas and outdoor space for smaller groups to large social spaces. The larger outdoor spaces will act as focal points, providing space for things like the farmers market, outdoor fitness classes and other regular programming as well as special events throughout the year.

FRAMEWORK

The redevelopment of University Place will be defined and guided by development standards specific to each pod, which includes the buildings’ massing, form, uses, location and relationships to the street frontages, along with the buildings’ entry locations and glazing. These elements are what will define the built environment and what will create the character of a walkable town center.



INTERPRETATION OF LANGUAGE

All provisions, terms, phrases and expressions contained in these Design Standards shall be liberally construed in order that the true intent and meaning of the Planning Staff may be fully implemented. All words and terms used have their commonly accepted and ordinary meaning unless they are specifically defined in the Town of Chapel Hill Land Use Management Ordinance and Design Manual or the context in which they are used clearly indicates to the contrary. The Planning Staff and/or Town Manager are responsible for making any interpretations of the language in these Design Standards. When vagueness or ambiguity is found to exist as to the meaning of any word or term used, any appropriate canon, maxim, principle or other technical rule of interpretations or construction used by the courts of this state may be employed to resolve vagueness and ambiguity in language.

TERMS

In addition to the terms listed below, refer to all definitions in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual.

1. **Affordable dwelling unit (for sale):** A dwelling unit intended for sale that is restricted for occupancy by a household within the target income prescribed in Section 3.10 of the LUMO.
2. **Affordable dwelling unit (rental):** A dwelling unit intended for rent that is restricted for occupancy by a household within the target income as further defined herein.
3. **Applicant:** Any person or his/her duly authorized representative who submits an application as defined herein.
4. **Blank wall area:** Blank wall area means a portion of the exterior façade of the building that does not include: windows or doors; columns, pilasters or other articulation greater than twelve (12) inches in depth; or a substantial material change (paint color is not considered a substantial change).
5. **Building depth:** The largest total dimension of a building footprint measured perpendicular to the primary frontage.
6. **Building façade:** The face of a building that delineates the edge of conditioned floor space.
7. **Building height:** Building height is measured in both the number of stories and in feet. Building height is the vertical distance from the average grade at the foundation to the highest portion of the structure. Average grade is determined by calculating the average of the highest and lowest elevation along natural or improved grade (whichever is more restrictive) along the front of the building parallel to the primary street setback line.
8. **Build-to Zone (BTZ):** The area on the lot where a certain percentage of the front building façade must be located, measured as a minimum and maximum setback range from the edge of the right-of-way.
9. **Building pass-through:** Allows access from one side of a building to another in a large development as an alternative to a street. A pass-through should be designed to provide safe and enjoyable public passage.
10. **Building step back:** The building step back shall be measured as the horizontal change in the building wall plane, perpendicular to the applicable frontage or property line.
11. **Building width:** The largest total dimension of a building footprint measured parallel to the primary frontage.
12. **Certificate of occupancy:** A document issued by the building inspector certifying compliance with all applicable state and local laws, including all terms of an approved zoning compliance permit, and authorizing occupancy of a building or structure.
13. **Decision maker:** The entity or agency which has authority to render a final decision as to the approval, conditional approval, or denial of an application, modification or design alternative.
14. **Design alternative:** Where a proposed alternative to the requirements still satisfies the intent and provisions of the Design Standards in terms of building form, aesthetic quality, orderly development, and high-quality public realm, and where the Planning Staff makes a finding that a proposed design alternative could provide an equivalent or better result that meets the purpose and intent of the Design Standards, the Planning Staff may approve such an alternative design as part of a minor modification to the Special Use Permit (SUP). Design alternatives do not constitute a full modification to the SUP. If Planning Staff does not approve the design alternative, the applicant may seek approval from the Town Manager.

TERMS (CONTINUED...)

15. **Developer:** A person who undertakes development activities.
16. **Development:** Any man-made change to improved or unimproved real estate, including, but not limited to: the construction, structural alteration, enlargement, or rehabilitation of any buildings or other structures, including farm buildings; mining; dredging; filling; grading; paving; excavation or drilling operations; clearing vegetation; division of a parcel of land into two (2) or more parcels or some changes in use of structures or land. Development may also include any land disturbing activity on real estate that changes the amount of impervious surfaces on a parcel.
17. **Development project:** A project in which one (1) or more lots, tracts, or parcels of land are to be developed or redeveloped as a coordinated site for a complex of uses, units, or structures, including, but not limited to, planned development and/or cluster development for residential, commercial, institutional, recreational, open space, and/or mixed uses as provided for in this zoning code.
18. **Driveway:** Vehicular way, other than a street or alley, that provides vehicular access from a street to or through off-street parking and/or loading areas.
19. **Dwelling unit:** A single unit providing complete independent living facilities for one (1) or more persons, including permanent provisions for living, sleeping, eating, cooking, and sanitation.
20. **Dwelling units, multifamily:** A dwelling or combination of dwellings on a single lot consisting of three (3) or more dwelling units.
21. **Exterior architectural features:** Exterior architectural features shall include the architectural style, general design, and general arrangement of the exterior of a building or other structure, including the kind and texture of the building material, the size and scale of the building, and the type and style of all windows, doors, light fixtures, signs, and other appurtenant fixtures. In the case of outdoor advertising signs, exterior architectural features shall be construed to mean the style, material, size, and location of all such signs.
22. **Floor:** The top surface of an enclosed area in a building (including basement), i.e. top of slab in concrete slab construction or top of wood flooring in wood frame construction. The term does not include the floor of a garage used solely for parking vehicles.
23. **Floor area ratio (FAR):** A decimal fraction that, when multiplied by the gross land area of a zoning lot, determines the maximum floor area permitted within the zoning lot. The floor area ratios for the various zoning districts are in the schedule of intensity regulations.
24. **Front building setbacks:** Measured from the edge of the nearest right-of-way line.
25. **Gross land area:** All area within the boundaries of a zoning lot (net land area) plus half of the following areas located within or adjoining the lot: (1) publicly-owned or otherwise permanently dedicated open space, such as parks, recreation areas, water bodies, cemeteries and the like, and (2) public rights-of-way; provided that the total amount of credited open space and public streets shall not exceed ten (10) percent of the net land area of the zoning lot.
26. **Ground floor elevation:** The height of the ground floor relative to the height of the sidewalk and it is measured from top of the abutting curb to the top of the finished ground floor.
27. **Height (of a structure or part thereof):** The vertical distance from the average finished grade at the foundation along the street façade to the highest portion of the structure, or part thereof. To determine mean finished grade, take the spot elevations from the highest and lowest points of the foundation. The average of these two (2) spot elevations is the mean finished grade and the elevation from which height measurements are made.
28. **Height limitation, setback height:** The maximum height allowed for any structure located at the perimeter setbacks on a zoning lot, as defined by the applicable minimum setback lines. The setback height is the lower limit of the vertical portion of the building envelope. Setback heights are established in division 3.8.2(f).

TERMS (CONTINUED...)

- 29. Hotel or motel:** A building or group of buildings containing in combination ten (10) or more lodging units intended primarily for rental or lease to transients by the day or week, as distinguished from residence halls, in which occupancy is generally by residents rather than transients.
- 30. Impervious surface:** A surface composed of any material that impedes or prevents natural infiltration of water into the soil.
- 31. Land Use Management Ordinance (LUMO):** Appendix A of the Town of Chapel Hill Code of Ordinances.
- 32. Minimum parking requirements:** The minimum number of parking spaces required pursuant to subsection 5.9.7(c) of this appendix.
- 33. Minor modification to parking:** Minor modifications to parking areas shall include parking areas that are generally remaining the same and undergoing modifications to the layout of the spaces only.
- 34. Major modification to parking:** Major modifications include adding parking area somewhere where there was previously no parking area.
- 35. Minor Change or Modification of the Special Use Permit (SUP):** The Planning Staff is authorized to approve minor changes in the approved final plans as long as such changes continue to comply with the approving action of the town council and all other applicable requirements, but shall not have the authority to approve changes that constitute a modification of the special use permit. Any change requiring evidential support in addition to that presented at a public hearing on applications for the original special use permit or subsequent modifications of special use permit shall constitute a modification of the special use permit, instead of a minor change. This is a defined process that should reference Section 4.5.4 of the LUMO. If the proposed action is determined to be a modification, the Planning Staff shall require the filing of an application for approval of the modification. An application for modification of a special use permit shall be reviewed in accord with the procedures established in subsection 4.5.3 of the LUMO. If Planning Staff does not approve the minor change or modification to the SUP, the applicant may seek appeal and approval from the Town Manager.
- 36. Module offset:** The module offset shall be measured as the horizontal change of a portion of the building wall plane at ground level, perpendicular to the applicable frontage. The module width shall be measured for the portion of the wall plane closer to the sidewalk, while the width of offset shall be measured for the portion further from the sidewalk. The dimensions of modules and offsets should maintain a sense of proportion to the building as a whole.
- 37. Outdoor amenity space:** Outdoor amenity space must be provided on the lot, or lands permanently designated as publicly accessible open space, and must be available as unenclosed exterior space appropriately improved as a pedestrian amenity or for aesthetic appeal and cannot include areas used for vehicles, except for incidental service, maintenance or emergency actions. Outdoor amenity space shall be made available to the general public.
- 38. Pedestrian connectivity:** A publicly accessible route between buildings that allows pedestrians to move from one (1) side of a building or lot to another through a privately owned or publicly dedicated area. The route must connect to or allow future connection to other such routes, sidewalks, greenways, or thoroughfares. Pedestrian connectivity may include a through-street or alley, and may be designed as a trail, greenway or other similar passage. Vehicular use may be allowed as desired by the applicant, provided that the design of the pedestrian route prioritizes pedestrian movements.
- 39. Planning Staff:** The Planning Staff of the Town of Chapel Hill.
- 40. Primary elevation of a building:** The elevation is the side of a building which fronts the main street serving the building. It will usually contain the main architectural features such as large windows, the main entrance door, lighting, signage and canopies or awnings.
- 41. Primary height:** The primary height limitation is the maximum height allowed for any structure located at the minimum setback required for the structure.

TERMS (CONTINUED...)

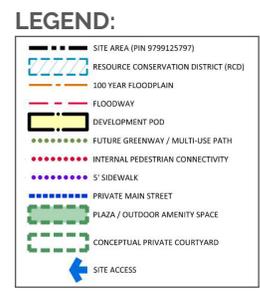
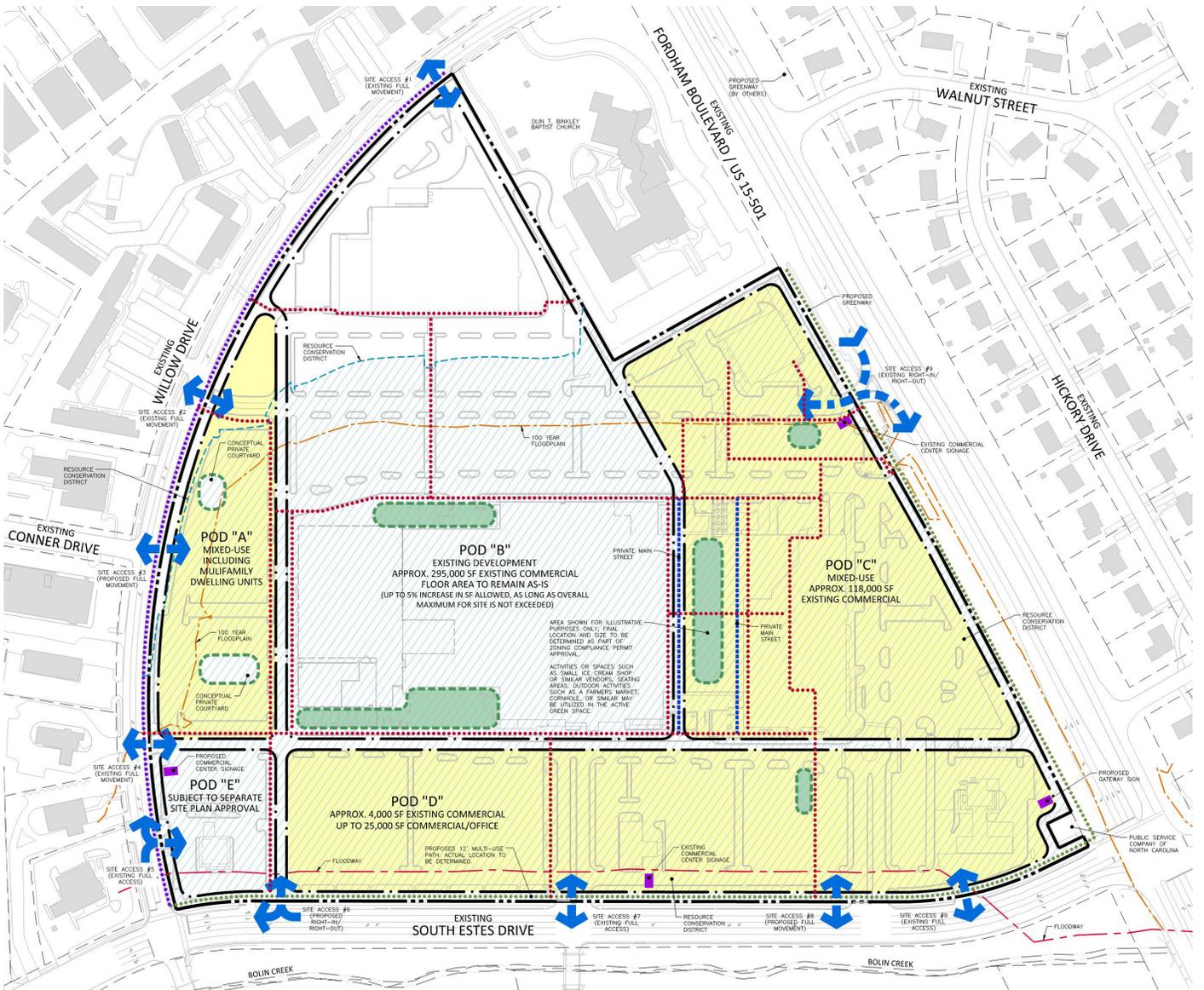
- 42. Primary entrance:** An entrance providing both ingress and egress, operable to residents at all times or to customers during normal business hours that is required along each side of the building facing a public streetscape or any other non-vehicular, publicly accessible area.
- 43. Proportions:** The ratios established by length, width and height and may exist as planar or volumetric measurements.
- 44. Proposed development:** The development requested by an applicant that includes all buildings and land uses subject to an application.
- 45. Public land:** Land owned by the Town of Chapel Hill, or any other governmental entity or agency thereof.
- 46. Public realm:** The streetscape or any other non-vehicular, publicly accessible area located along the designated frontage of a street, alley shared between sites, or non-vehicular thoroughfare.
- 47. Right-of-way:** A fee simple dedication of private property or an easement, whereby public access and utility easements are granted.
- 48. Reconfiguration:** A change in the form or design of an existing development or structure. Reconfiguration shall be treated as development if it involves substantial improvement as defined in this section.
- 49. Renovation:** The act of improving a structure or development by renewing and restoring component parts. Renovation shall be treated as development if it involves substantial improvement as defined in this section.
- 50. Resource Conservation District (RCD):** An overlay zoning district to protect stream corridors and prevent property damage from floods. The RCD is intended to preserve water quality, minimize danger and property damage from flooding, protect streams from erosion and sedimentation, and preserve urban wildlife corridors and plant habitats. The RCD is also used to manage development in FEMA regulatory floodplains.
- 51. Secondary height:** The secondary height is the absolute maximum height allowed for any structure and increases at a rate of one foot in height for every one foot of distance interior to the lot, measured away from the setback of any public street frontage. The secondary height allowed as measured in number of stories shall increase at a rate of 1 additional story for every 10' feet of distance interior to the lot, measured away from the setback of any public street frontage.
- 52. Setback, street:** The horizontal distance between the street lot line of a zoning lot and any structure on such zoning lot, measured perpendicular to the street lot line.
- 53. Sidewalk clear zone:** An area of the sidewalk equivalent to the minimum required width for which pedestrians have a safe and adequate place to walk free of any obstructions. Any entryways, doors, door swings, outdoor dining, sandwich boards, benches, lighting or other streetscape features shall be placed outside of the clear zone.
- 54. Site specific development plan:** A special use permit issued by the council authorizing the development of a zoning lot.
- 55. Special Use Permit (SUP):** A permit issued by the council authorizing the development of a zoning lot for a special use or a planned development.
- 56. Story height:** The height of each story of building and it is measured from the top of the finished floor to the ceiling above.
- 57. Street:** A right-of-way or easement thirty (30) feet or more in width containing a roadway which provides or is used primarily for vehicular circulation.
- 58. Street facing façade:** A building façade which directly abuts an arterial, collector, local or district street.
- 59. Street frontage width:** The horizontal distance measured along a straight line connecting the points at which the street lot line abutting a street intersects with interior lot lines and/or other street lot lines.
- 60. Street, private:** A street consisting of a private easement and a privately maintained roadway.

TERMS (CONTINUED...)

- 61. Street, public:** A street consisting of a publicly dedicated right-of-way and a roadway maintained by the Town of Chapel Hill or the State of North Carolina.
- 62. Thoroughfare:** A route provided for the purposes of creating connectivity and/or establishing blocks, to include all types of streets, alleys, and non-vehicular paths and greenways with a defined right-of-way.
- 63. Town council:** The governing body of the Town of Chapel Hill, consisting of a mayor and eight (8) council members, as established in the Charter of the Town of Chapel Hill.
- 64. Town Manager:** The Town Manager of the Town of Chapel Hill, or his or her designee.
- 65. Transparency:** The minimum percentage of windows and doors that must cover a ground or upper story façade. Transparency is required for any building façade facing a street.
- 66. Use:** The specific activity or function for which land, a building, or a structure is designated, arranged, intended, occupied, or maintained.
- 67. Use, accessory:** A use on the same lot or in the same structure with, and of a nature and extent customarily incidental and subordinate to, the principal use of the lot or structure.
- 68. Use, principal:** The primary or main use of land or structures, as distinguished from a secondary or accessory use.
- 69. Use, special:** A use of land, buildings, or structure that is identified in this appendix as a use that because of its inherent nature, extent, and external effects, requires special care in the control of its location, design, and methods of operation in order to ensure protection of the public health, safety, and welfare.
- 70. Variance:** A relaxation of the strict terms of a specific provision of this appendix authorized by the board of adjustment in accord with the provisions of section 4.6 of this appendix.
- 71. Vertical mixed use building:** A building in which commercial uses are located on the first floor; commercial, residential, or office uses are located on the second to fourth floors; and residential uses are located on any floors above the fourth floors. A building must include both (1) commercial and/or office uses, and (2) residential uses, in order to be considered a vertical mixed use building.
- 72. Zoning Compliance Permit (ZCP):** A permit issued by the town manager authorizing the recipient to make use of property in accord with the requirements of this appendix.

II. FRAMEWORK

The site plan configures the property into different development areas referred to as “Pods”. The configuration of each Pod is based on the current built environment (buildings, road network and areas protected by tenant’s lease rights), the proposed future built environment, as well as the natural resources surrounding the property such as the floodway, floodplain and Resource Conservation District (“RCD”). Within each Pod, development is dictated by specific rights designated to that Pod as well as overall densities allowed for the property as a whole. While overall densities are outlined in the Data Table on the site plan, the Design Standards will serve as the framework for development within each Pod. Plazas and green spaces are placed throughout the property’s development and are connected by pedestrian-oriented walkways and streetscapes.



Each Pod within the Site Plan contains allowable use types that include commercial, office, residential and hotel, each of which are further defined below. For uses not specifically defined within the Design Standards for University Place, the uses permitted within Community Commercial (“CC”) zoning district shall govern allowed uses unless otherwise listed herein as specifically prohibited.

PERMITTED USES					
RESIDENTIAL USES	POD A	POD B	POD C	POD D	POD E
Dwelling Units, as listed below:					
Single Family	--	--	--	--	--
Single Family w/ accessory apartment	--	--	--	--	--
Duplex	--	--	--	--	--
Multifamily - 3 to 7 dwelling units	P	--	P	--	--
Multifamily - Over 7 dwelling units	P	--	P	--	--
Live-Work	--	--	--	--	--
Triplex	--	--	--	--	--
Upper Story	--	--	--	--	--
Fraternity Dwelling	--	--	--	--	--
Group Care Facility	--	--	--	--	--
Home Occupation	A	--	A	--	--
Home Occupation, Major	--	--	--	--	--
Independent Senior Living Facility	P	--	P	--	--
Manufactured Home Park	--	--	--	--	--
Manufactured Home, Class A	--	--	--	--	--
Manufactured Home, Class B	--	--	--	--	--
Residence Hall	--	--	--	--	--
Residential Support Facility	--	--	--	--	--
Rooming House	--	--	--	--	--
Shelter	--	--	--	--	--
Short-Term Rentals	--	--	--	--	--
Tourist Home	--	--	--	--	--
OFFICE USES	POD A	POD B	POD C	POD D	POD E
Business, Office-Type	P	P	P	P	P
Clinic	P	P	P	P	P
Hospital	--	--	--	--	--
Institutional Uses, as listed below:					
College or University	P	P	P	P	P
Fine Arts Educational Institution	P	P	P	P	P
Place of Worship	P	P	P	P	P
Public Cultural Facility	P	P	P	P	P

KEY

- P = Permitted as Principal Use
- A = Permitted as Accessory Use
- S = Permitted as Special Use
- = Use Not Permitted

Public Service Facility	A	A	A	A	A
Public Use Facility	P	P	P	P	P
School, Elementary or Secondary	P	P	P	P	P
Vocational School	P	P	P	P	P
Flex Office	--	--	--	--	--
Flex Space	--	--	--	--	--
Maintenance & Storage Facility	A	A	A	A	A
Research Activities	P	P	P	P	P
Research Activities, Light	--	--	--	--	--
HOTEL USES	POD A	POD B	POD C	POD D	POD E
Hotel or Motel	P	--	P	--	--
COMMERCIAL USES	POD A	POD B	POD C	POD D	POD E
Adult Day Care Facility	A	A	A	A	A
Agriculture, except as listed below:	--	--	--	--	--
Community Garden	A	A	A	A	A
Automatic Teller Machines (Walk-up)	P	P	P	P	P
Automatic Teller Machines (Drive-up)	P	P	P	P	P
Automotive Repair	P	P	P	P	P
Automotive, Trailer & Farm Sales or Rental	--	--	--	--	--
Bank	P	P	P	P	P
Barber Shop / Beauty Salon	P	P	P	P	P
Business - Convenience	P	P	P	P	P
Business - General	P	P	P	P	P
Business - Wholesale	P	P	P	P	P
Car Wash	A	A	A	A	A
Cemetery	--	--	--	--	--
Child Day Care Facility	P	P	P	P	P
Club	P	P	P	P	P
Drive-in Window / Drive-thru Facilities	P	P	P	P	P
Essential Services	P	P	P	P	P
Extraction of Earth Products	--	--	--	--	--
Food Truck	A	A	A	A	A
Funeral Home	--	--	--	--	--
Hangar, Medical Aircraft	--	--	--	--	--
Kennel	A	A	A	A	A
Landfill	--	--	--	--	--
Manufacturing, Light	A	A	A	A	A

KEY

P = Permitted as Principal Use
 A = Permitted as Accessory Use
 S = Permitted as Special Use
 -- = Use Not Permitted

Outdoor Skateboard Ramp	--	--	--	--	--
Park/Ride	A	A	A	A	A
Parking, Off-Street	A	A	A	A	A
Personal Services	P	P	P	P	P
Place of Assembly, +2,000 Seating Capacity	--	--	--	--	--
Place of Assembly, up to 2,000 Seating Capacity	P	P	P	P	P
Publishing and/or Printing	A	A	A	A	A
Recreation Facility, Commercial	P	P	P	P	P
Recreation Facility, Non-Profit	P	P	P	P	P
Recreation Facility, Outdoor Commercial	A	A	A	A	A
Self-Storage Facility, Conditioned	--	--	--	--	--
Service Station/Convenience Store	--	--	--	P	P
Solid Waste Management Facility	--	--	--	--	--
Supply Yard	A	A	A	A	A
Temporary Portable Building, Construction Related	A	A	A	A	A
Temporary Portable Building, Not Construction Related	--	--	--	--	--
Veterinary Hospital or Clinic	P	P	P	P	P
Water & Wastewater Treatment Plant	--	--	--	--	--
WIRELESS COMMUNICATION FACILITIES	POD A	POD B	POD C	POD D	POD E
Collocation on existing tower or base station	P	P	P	P	P
Small Wireless Facility concealed: new base station or new dual purpose tower	P	P	P	P	P
Small Wireless Facility non-concealed: new base station or new tower	P	P	P	P	P
Macrocell Facility: new concealed base station	P	P	P	P	P
Macrocell Facility: new concealed dual-purpose tower	S	S	S	S	S
Macrocell Facility: new non-concealed base station	P	P	P	P	P
Macrocell Facility: new non-concealed tower	S	S	S	S	S

KEY

P = Permitted as Principal Use
 A = Permitted as Accessory Use
 S = Permitted as Special Use
 -- = Use Not Permitted

NOTES

1. A lot or building must be occupied with only the principal uses permitted on the permitted use table above. Accessory uses and structures customarily incidental to a permitted principal or special use shall be permitted.
2. If residential uses are constructed as permitted herein, a minimum of 15% of all residential units constructed must be designated as affordable dwelling units.
3. All affordable dwelling units within residential uses designated as “for sale” shall be subject to the requirements of Section 3.10 of the Land Use Management Ordinance (LUMO).
4. All affordable dwelling units within residential uses designated as “rental units” shall be subject to the requirements contained herein:
 - a. Rental units designated as affordable dwelling units shall remain affordable for a period of 30 years from certificate of occupancy.
 - b. Rental units designated as affordable dwelling units shall be units provided for households earning 80% or less of the area median income (AMI) based on household size for the Durham-Chapel Hill Metropolitan Statistical Area, as determined by the United States Department of Housing and Urban Development (HUD).
 - c. If the required number of affordable dwelling units includes a fraction, the applicant shall have the option to (i) round up and provide one additional affordable dwelling unit, or (ii) the fraction of a unit shall be provided in the form of a payment in lieu of providing the affordable dwelling unit as further outlined in Section 3.10.3 of the LUMO.
 - d. Each rental unit designated as an affordable dwelling unit must meet the following minimum floor area:
 - i. Studio/Efficiency – 500 SF*
 - ii. 1 Bedroom – 700 SF*
 - iii. 2 Bedroom – 850 SF*
 - iv. 3 Bedroom – 1,100 SF*
 - v. 4 Bedroom – 1,200 plus 250 SF per additional bedroom above 4*

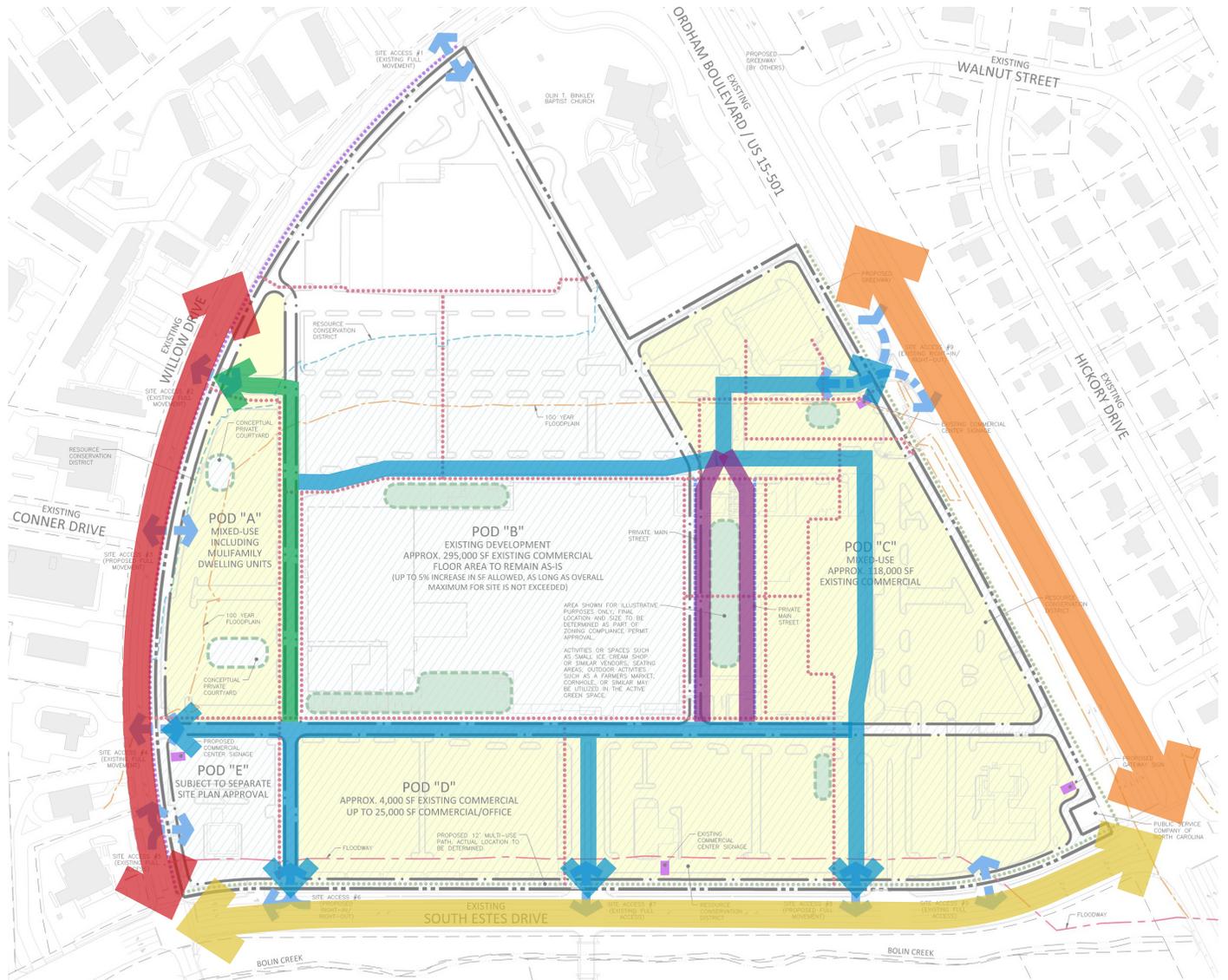
*If unrestricted, market rate units in a building are constructed at sized below those stated above, the minimum floor area for affordable units may be reduced to the size of such comparable unrestricted units in the building.
 - e. The owner may seek alternatives to on-site development of affordable dwelling units as outlined in Section 3.10.3 of the LUMO.
 - f. An affordable housing plan shall be required as outlined in Section 3.10.4 of the LUMO.
 - g. An affordable housing performance agreement shall be required as outlined in Section 3.10.5 of the LUMO.
 - h. The owner shall be entitled to all development cost offsets allowed under Section 3.10.6 of the LUMO.
 - i. Rental units designated as affordable dwelling units shall be subject to all requirements under Section 3.10.7 of the LUMO.
5. If residential uses are constructed as permitted herein and are designated as “senior housing/senior living” with minimum age requirements (typically 55+), the affordable dwelling unit requirements outlined above shall not apply so long as the units remain age restricted.
6. Food Trucks shall be a permitted use by right, however, Food Trucks will need to comply with all vendor requirements under Sec. 6.20 of the LUMO.

NOTES (CONTINUED...)

7. In order to regulate use, categories of uses have been established above, providing a systematic basis for assigning land uses to appropriate categories with other similar uses. Use categories classify land uses and activities based on common functional, product or physical characteristics. The Town Manager is responsible for categorizing all uses into the use categories above. When determining the use category in which a proposed use fits, the Town Manager must consider the following criteria:
 - a. the actual or projected characteristics of the proposed use
 - b. the relative amount of site area or floor area and equipment devoted to the proposed use
 - c. relative amounts of sales
 - d. the customer type
 - e. the relative number of employees
 - f. hours of operation
 - g. building and site arrangement
 - h. types of vehicles used and their parking requirements
 - i. the number of vehicle trips generated
 - j. how the proposed use is advertised
 - k. the likely impact on surrounding properties
 - l. whether the activity is likely to be found independent of the other activities on the site
 - m. where a use not listed is found by the Town Manager not to be similar to any other permitted use, the use is not permitted.
8. The Business - Convenience use as permitted herein which includes the use for eating and drinking establishments shall include brewery, cidery, seltzery, distillery & winery uses, so long as any such use includes a retail component open to the public. No production-only facilities shall be permitted.
9. Drive-thru facilities, including drive-up automatic teller machines and drive-in windows shall be permitted, subject to the drive-thru standards contained herein.
10. Co-working space shall be permitted under Business, Office-Type.
11. Short-Term Rentals, which is defined as the rental of a residence, or part thereof, to a transient for a limited duration, usually 30 days or less, shall be specifically prohibited.
12. The property contains an existing Service Station / Convenience Store use located on Pod D. No additional independent Service Station / Convenience Store uses shall be permitted by right without first obtaining a Special Use Permit.

III. DESIGN CRITERIA

To help facilitate a pedestrian friendly and walkable environment, street frontage requirements shall be used to apply certain development standards along thoroughfares. Frontage requirements shall be used to dictate a building's proximity to the road through the use of a Build-to Zone ("BTZ"), where new buildings must be placed in order to qualify as a building frontage. New buildings shall also be required to have a minimum percentage of building facade along the linear street frontage. Given the environmental constraints along Estes Drive, there shall be no frontage requirements for buildings in Pod D that are fronting Estes Drive, however, streetscape standards shall apply as detailed herein. Note that these Design Standard street frontage requirements shall take the place of perimeter buffer requirements that are outlined in LUMO Section 5.6.



LEGEND:

█	WILLOW DRIVE
█	FORDHAM BOULEVARD
█	ESTES DRIVE
█	INTERNAL MAIN STREET DRIVEWAY
█	INTERNAL TYPICAL DRIVEWAY
█	SERVICE ALLEY

1. WILLOW DRIVE

BUILDING LOCATIONS

- Front Setback/BTZ (min/max) 0-20'
- Building Façade in BTZ (min) 50%

Given that development will occur in phases, the required building façade in BTZ shall be calculated based on the new building area within the BTZ against the land area being disturbed for that specific building or phase of development.

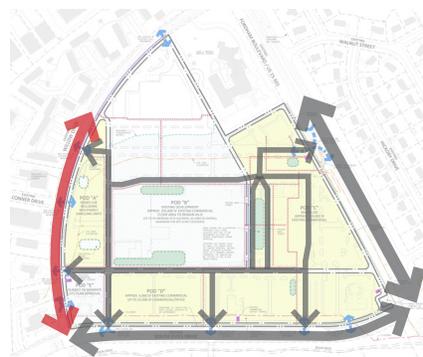
PARKING

No new parking shall be permitted between new buildings and the Willow Drive frontage.

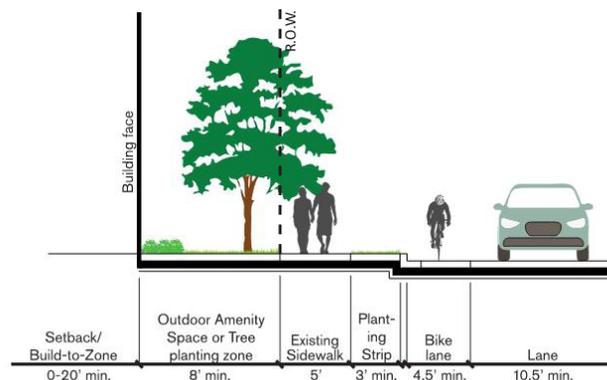
STREETSCAPE

- Sidewalk (min): 5' (Existing to remain)
- Planting strip (min): 3'
- Outdoor Amenity Space/ Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.
- Bike Lane (min): 4.5'

Where conflicts exist due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Planning Staff.



STREET KEY PLAN



SECTION

2. FORDHAM BOULEVARD

BUILDING LOCATIONS

- Front Setback/BTZ: 0 (min)
 - 77' (max with parking)
 - 0-20' (max without parking)
- Building Façade in BTZ (min): 40%

Given that development will occur in phases, the required building façade in BTZ shall be calculated based on the new building area within the BTZ against the land area being disturbed for that specific building or phase of development.

PEDESTRIAN ZONE

- Sidewalk (min): 8'
 - (See Street Frontages note 5)

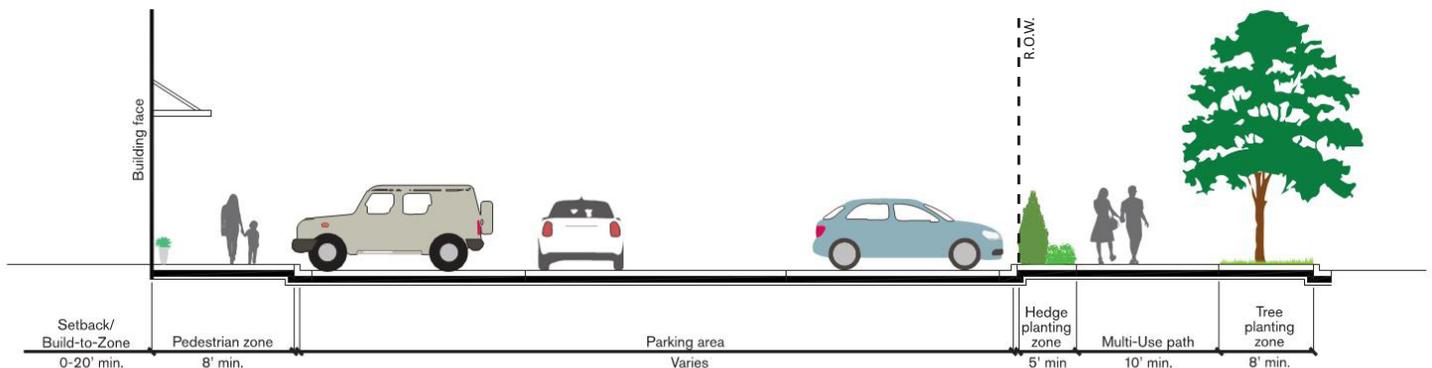
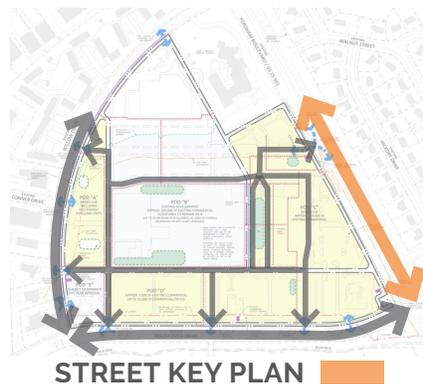
PARKING AREA

- Parking Area (max, if provided): 62'
- Hedge Planting Zone (min): 5' (only required when there is parking that is to be screened)

STREETScape

- Sidewalk (min): N/A*
 - *Multi-use path proposed as alternative to sidewalk
- Multi-use Path: 10' (with 2' clear zone)
- Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.

Where conflicts exist due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Planning Staff.



3. ESTES DRIVE

BUILDING LOCATIONS

- Front Setback/BTZ (min/max): N/A
- Building Façade in BTZ (min): N/A

Given that the Estes Drive frontage is within the FEMA floodway, there shall be no required BTZ since buildings cannot be placed within the floodway.

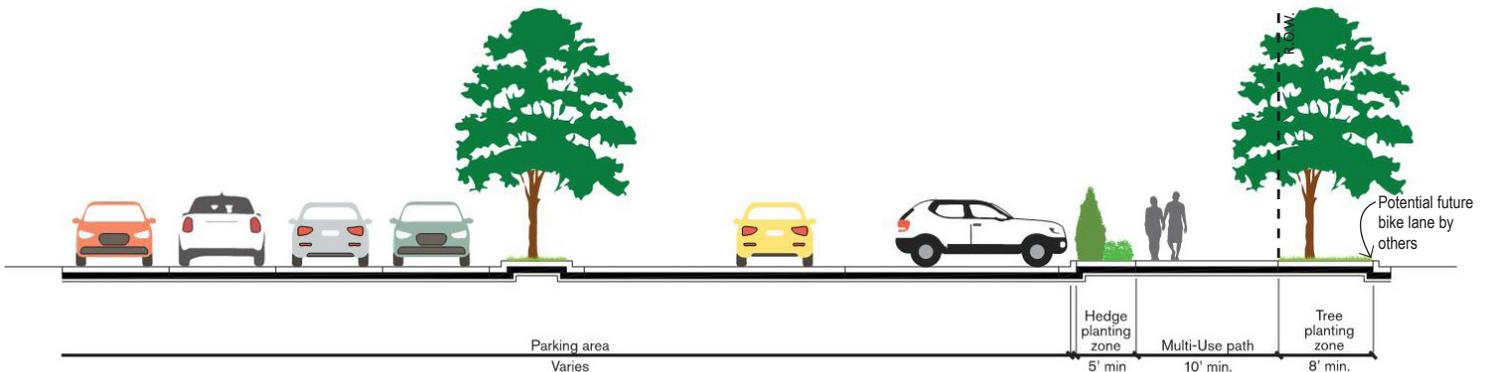
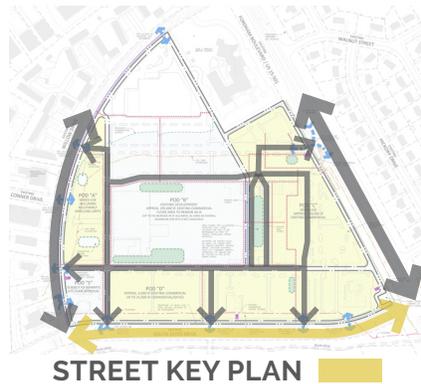
PARKING

- Parking Area (min): Varies
- Hedge Planting Zone (min): 5' (only required when there is parking that is to be screened)

STREETScape

- Sidewalk (min): N/A*
 - *Multi-use path proposed as alternative to sidewalk.
- Multi-use Path: 10' (with 2' clear zone)
- Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.

Where conflicts exist due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Planning Staff.



4. INTERNAL MAIN STREET DRIVEWAY

DESIGN INTENT & FLEXIBILITY

The internal main street driveway section shown below is mainly shown for design intent only. There may be a need for flexibility in this layout to accommodate specific urban amenities and groups that will utilize this space such as a farmer’s market, events lawn, and other events such as hosting larger community gatherings. **Note that if a design alternative is proposed for this example streetscape section, Planning Staff can approve the alternative so long as it meets the overall intent.

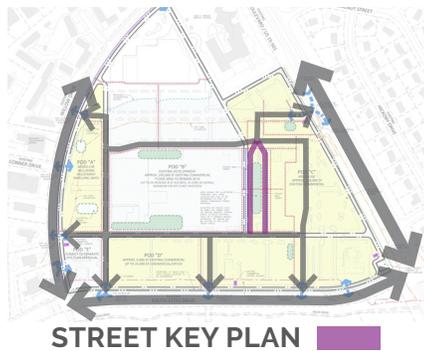
BUILDING LOCATIONS

- Front Setback/BTZ (min/max): 0-20’
- Building Façade in BTZ (min): 50%

Given that development will occur in phases, the required building façade in BTZ shall be calculated based on the new building area within the BTZ against the land area being disturbed for that specific building or phase of development.

PARKING

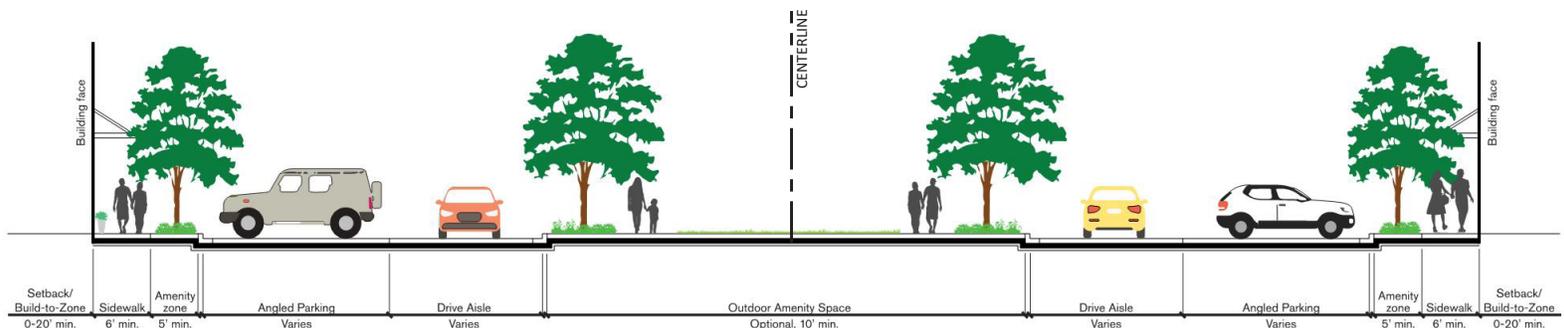
- Angled or parallel parking
- Ensure pedestrians have convenient access from the parking area directly to the sidewalk and building facades.



STREETSCAPE

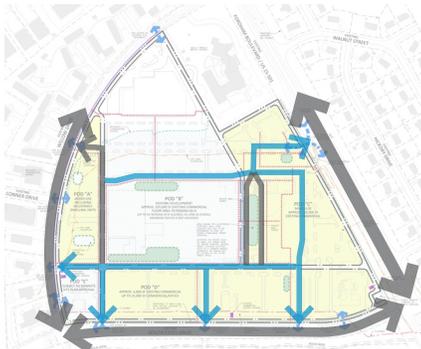
- Sidewalk (min): 6’
- Amenity Zone (min): 5’
 - Includes but not limited to: tree planting zone, light poles, bollards, tree grates, benches, planters, street furnishings, and hardscape areas/ extended sidewalk.
- Tree Spacing (on center, avg): 40’
 - A tree shall be provided at least every 40’ measured linearly along each side of the internal main street driveway. These trees may be placed within the outdoor amenity space, streetscape amenity zone or in the parking islands; however, only the trees located on each side of the centerline may count towards that half of the streetscape’s tree requirement.
 - Tree planting requirements of the parking landscape standards in section 5.9.6 of the LUMO shall apply.
- Outdoor Amenity Space (min, if provided): 10’
 - Refer to requirements in these Design Standards to follow.
 - Retail kiosks and other structures may be placed in the outdoor amenity space.

Where conflicts exist due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Planning Staff.



EXAMPLE SECTION**

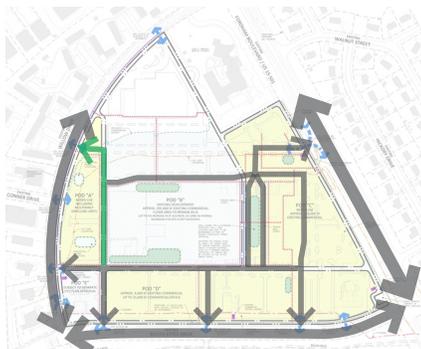
5. INTERNAL TYPICAL DRIVEWAY



STREET KEY PLAN

Throughout the site there are numerous existing internal driveways that may remain as-is or be modified over time. Given the various configurations existing today, internal drives shall not be subject to any specific cross-section; however, for proposed buildings with a primary entrance facing an internal drive, a minimum 6' sidewalk and 5' amenity zone shall be required between the building and the internal drive. Alternative designs may be proposed, reviewed and approved by the Planning Staff where conflicts exist due to utility locations, fire access, required sight lines or other conditions merit.

6. SERVICE ALLEY



STREET KEY PLAN

The service alley is located near the rear of the building and not visible from the public right-of-ways as much as possible given existing parameters of the site. The alley is oriented to easily access necessary service areas around Pod A and Pod B.

NOTES

1. Setbacks/BTZ shall be measured from the edge of the right-of-way line. No interior, side or rear yard setbacks shall be required; however, if building is not placed on the side or rear property line and a setback is provided, it must be a minimum of 5 feet from the property line.
2. The required percentage of building façade in the BTZ is measured by the width of the building along the street frontage divided by the buildable width of the lot. Since the project may occur in phases, the buildable width of the lot shall only include the portion of the lot included and being disturbed in any such phase of construction. If any such phasing creates a non-conforming frontage request until a future phase is built, a design alternative may be reviewed and approved by the Town Manager.
3. Outdoor seating and dining areas shall qualify as building façade for the purposes of calculating the BTZ percentage so long as the outdoor seating and dining area is not more than 2/3 the width of the building and is separated from the sidewalk by a wall or fence no higher than 4' above the sidewalk. Each outdoor seating and dining area may only include up to 20' of qualifying width with no more than two outdoor seating and dining areas included in the BTZ calculation for any one building.
4. Drive-thru windows or similar structures related to drive-thru facilities, whether attached or detached to the use it serves shall not be counted towards the BTZ percentage requirement, nor shall walk-up automatic teller machines.
5. Along Fordham Blvd. frontage, if the primary building entrance is facing Fordham Blvd, an 8' sidewalk shall be required within the pedestrian zone. If the primary building entrance is perpendicular to Fordham Blvd., an 8' sidewalk shall be required along the primary building façade and a 5' sidewalk shall be required adjacent to the building façade parallel to and fronting Fordham Blvd. If no building fronts Fordham Blvd., then a pedestrian zone sidewalk is not required in addition to the multi-use path.
6. While only one row of parking (62' max) shall be permitted between any new buildings constructed and the Fordham Blvd. right-of-way, so long as the minimum BTZ percentage requirement has been met, additional rows of parking shall be allowed beside those buildings constructed or in the areas between where buildings are constructed along Fordham Blvd.
7. Where a new building is being constructed on a portion of the property that is outside of the BTZ, such construction shall be permitted when the BTZ percentage required has been met for the frontage directly adjacent to where the non-conforming building is being proposed. Note that if a building fronts the Main Street it is exempt from other frontage/BTZ requirements.
8. Building additions to any existing structures shall be permitted and shall not be subject to BTZ requirements unless the building addition proposed is within the BTZ of any street frontage. Note that this statement also applies to a structure that exists in the future and is then expanded.
9. Buildings within 100' of a second right-of-way shall be subject to the frontage requirements of both streets. Any building outside of 100' of a second right-of-way shall only be subject to one frontage requirement based on the nearest right-of-way to the building being constructed unless otherwise exempt from frontage requirements. This requirement excludes the plans that are currently under review in Pod E.
10. Note that streetscape improvements are only required along areas that are being redeveloped.
11. All landscaping for street fronts shall meet requirements set forth in Section 5.9.6 of the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual, with the exception of any modifications, variances or design alternatives that are approved with the Special Use Permit. For any NCDOT roads, streetscape & landscaping requirements are subject to NCDOT approval and may be modified as needed so long as Planning Staff agrees with the requested changes.
12. Given that screening and other requirements are met, structured parking may be located in the BTZs.

MASS

Building mass variation is the way the form and shape of a building changes to establish a sense of human scale. This may be achieved by changing the heights of different parts of a building and by creating offsets in wall planes to express individual building modules. All new buildings in University Place shall be subject to the building height requirements listed below. In addition, all buildings 4 stories in height or greater shall meet either the building step back or module offset criteria listed below.

1. BUILDING HEIGHT

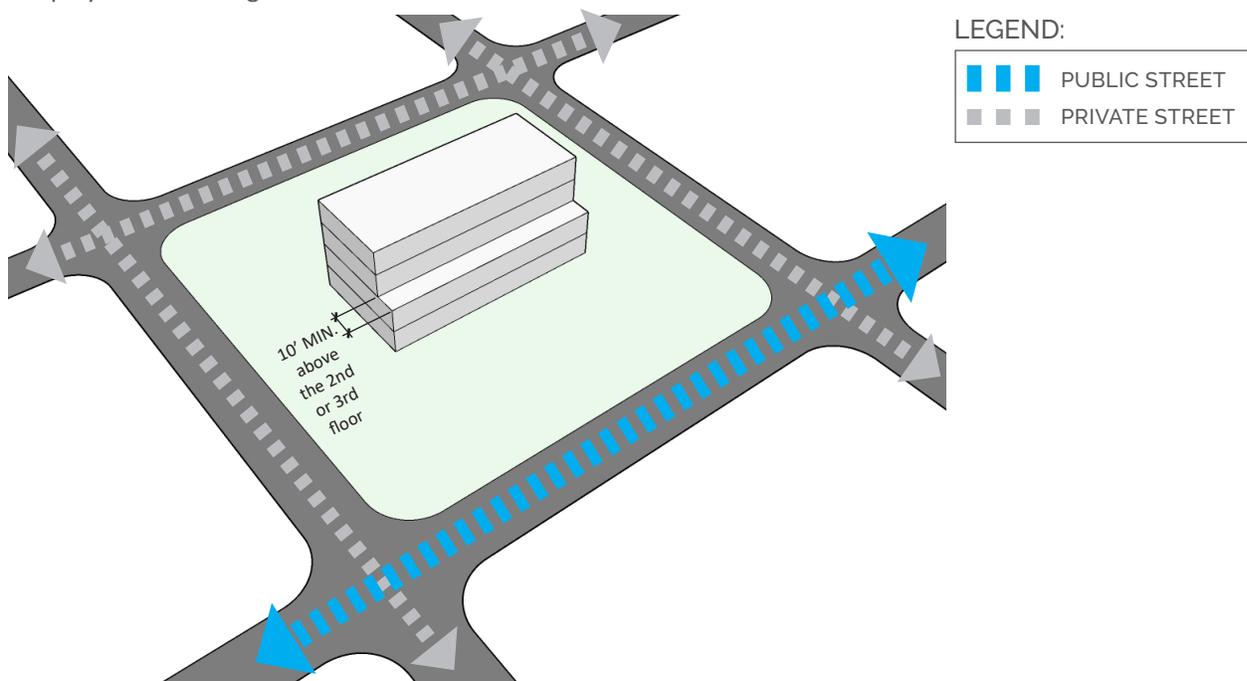
Building height shall be measured from the average grade at the foundation to the highest portion of the structure. Building height requirements are broken down into primary and secondary. Primary height shall be the maximum height allowed for any structure located at the minimum setback required for such structure, while secondary height shall be the absolute maximum height allowed for any structure. The maximum heights for University Place shall be as follows:

	Primary Height (max.)	Secondary Height (max.)
Pod A	75' (5-story)	90' (7-story)
Pod B	N/A - No Change	N/A - No Change
Pod C	75' (5-story)	90' (7-story)
Pod D	34' (3-story)	45' (3-story)
Pod E	N/A - No Change	N/A - No Change

Ground Floor Height: In addition, ground floor height for residential and hotel uses shall be a minimum of 9' from floor to ceiling and a minimum of 12' from floor to ceiling for commercial and office uses with upper story height being a minimum of 9' from floor to ceiling for all permitted uses.

2. BUILDING STEP BACK

The building step back shall be measured as the horizontal change in the building wall plane, perpendicular to the applicable frontage or property line. Stepped heights of buildings are to be proposed only from buildings directly fronting public streets. A 10' foot building step back above the second or third floor is required for buildings 4 stories or greater, unless module offset is provided. The building step back requirements are also displayed in the diagram below:



BUILDING STEP BACK DIAGRAM

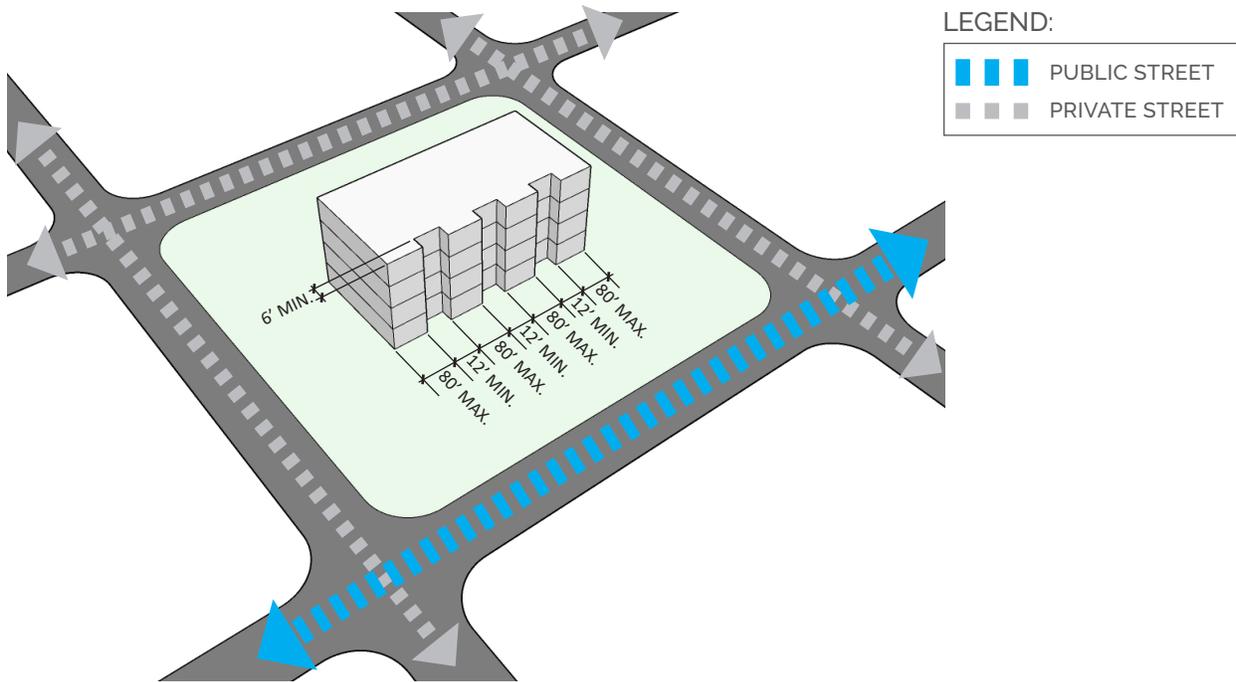
MASS (CONTINUED...)

3. MODULE OFFSET

Building modules refer to a set of subordinate volumes that compose the total mass of a building. Modularity also can be expressed by the following, but not limited to, changes in wall planes, building materials and architectural details. The module offset requirement shall be measured as the horizontal change of a portion of the building wall plane at ground level, perpendicular to the applicable frontage. The module width shall be measured for the portion of the wall plane closer to the sidewalk, while the width of offset shall be measured for the portion further from the sidewalk. The dimensions of modules and offsets should maintain a sense of proportion to the building as a whole. Module offset is required for buildings 4 stories or greater, unless a building step back is provided. Requirements for module offset in University Place shall be as follows:

Average Module Width (max)	80'
Depth of Offset (min)	6'
Width of Offset (min)	12'

The module offset requirements are also displayed in the diagram below:



MODULE OFFSET DIAGRAM

FORM

The building form design requirements should encourage human interaction and activity at the street level with clear connections to building entries and edges. In conjunction, the Street Frontage standards defined in this package will ensure an appropriate pedestrian framework is created to safely connect people to different buildings. Primary entrances should be distinctive and identifiable to pedestrians with clear lines of sight. There should be an emphasis on street level transparency to facilitate interaction between people in the interior of a building and people near the exterior of a building. The primary elevation of a building is considered as that side which fronts the main street serving the building. It will usually contain the main architectural features such as large windows, one or more primary entries, lighting, signage and canopies or awnings. To facilitate these goals mentioned above, the following shall be required on the primary elevations of buildings:

Transparency - Ground Floor (min)	20% (Residential/Hotel Uses) 50% (Commercial/Office Uses)
Transparency - Upper Floors (min)	20% (All Uses)
Blank Wall Distance (max)	50'
Primary Entrance Facing Public Realm	Required
Building Pass-through	330' maximum spacing
Width (min)	12'
Width (min) for Buildings 4-stories or more	20'
Height (min)	Equal to height of adjacent first floor ceiling



NOTES

1. The primary height limitation is the maximum height allowed for any structure located at the minimum setback required for the structure. The secondary height is the absolute maximum height allowed for any structure and increases at a rate of one foot in height for every one foot of distance interior to the lot, measured away from the setback of any public street frontage. The secondary height allowed as measured in number of stories shall increase at a rate of 1 additional story for every 10' feet of distance interior to the lot, measured away from the setback of any public street frontage.
2. Transparency is the minimum percentage of windows and doors that must cover a ground or upper story façade. Transparency is required for any building façade facing a public street. The transparency requirement on ground story façades is measured between 2 and 9 feet above the adjacent sidewalk. The transparency requirement on upper story façades is measured from the top of the finished floor to the top of the finished floor above. When there is no floor above, upper story transparency is measured from the top of the finished floor to the top of the wall plate. Glass is considered transparent where it has a transparency higher than 80% and external reflectance of less than 15%. Windows must be clear, unpainted, or made of similarly-treated glass; spandrel glass or back-painted glass does not comply with this provision. Transparency applies to street-facing façades only. For ground story commercial uses, a minimum of 60% of all windows must allow views into the ground story for a depth of at least 6 feet.
3. Blank wall area and distance means a portion of the exterior façade of the building that does not include: windows or doors; columns, pilasters or other articulation greater than 12 inches in depth, art or murals, or a substantial material change (paint color is not considered a substantial change). Blank wall area and distance applies in both a vertical and horizontal direction. Blank wall area is not permitted to exceed the maximum blank wall distance as measured in both the vertical and horizontal direction. Blank wall area and distance applies only to ground and upper story street-facing façades.
4. An entrance providing both ingress and egress, operable to residents at all times or to customers during normal business hours, is required along each side of the building facing a public streetscape or any other non-vehicular, publicly accessible area to meet the public realm primary entrance requirements. Additional entrances are permitted. An angled (clipped corner) entrance may be provided at any corner of a building along the street to meet the street entrance requirements. A primary entrance may be oriented perpendicular to the adjacent public realm where the entrance is clearly defined by a building element. For a residential building façade, entries to individual units are considered primary entrances.
5. Building pass-throughs shall be a minimum height equal to the first floor ceiling height of the adjacent building except when life safety service is required. For buildings 4-stories or greater, a building pass-through shall be a minimum width of 20'. For building 4-stories or less, the building pass-through shall be a minimum width of 12'. Building pass-throughs shall be adequately lit. Building pass-throughs shall serve as a publicly accessible passage between or through buildings that allows pedestrians to move from one side of a building to another, through a privately owned or publicly dedicated area.
 - a. The maximum building pass-through spacing may be increased by five percent (5%) through a minor modification where one or more of the following applies:
 - i. proposed to protect sensitive natural areas or save healthy existing trees
 - ii. required to protect natural conditions, such as watercourses, riparian buffers, natural rock formations or topography
 - iii. required based on some unusual aspect of the development site or the proposed development that is not shared by landowners in the surrounding area (e.g., unusual lot size or configuration)
 - iv. required due to the presence of existing utilities or other easements
 - v. proposed because there are no other options for ingress and egress.

NOTES (CONTINUED...)

- b. Where the Planning Staff makes a finding that a proposed design alternative for building pass-through will provide access that at a minimum meets the purpose or intent the pass through requirement and where one or more of the site constraints listed below applies, the Planning Staff may approve an alternatively designed building pass-through:
 - i. proposed to protect sensitive natural areas or save healthy existing
 - ii. required to protect natural conditions, such as watercourses, riparian buffers, natural rock formations or topography
 - iii. required based on some unusual aspect of the development site or the proposed development that is not shared by landowners generally in the surrounding area (e.g., unusual lot size or configuration)
 - iv. required due to the presence of existing utilities or other easements.
 - c. Modifications to existing structures shall not trigger any requirement for a building pass-through. Only new buildings shall be subject to the pass-through requirement.
 - d. Where building pass-throughs cannot be implemented because of limitations caused by existing conditions, a public amenity must be provided in its place to continue a piece of the pedestrian experience in an alternative way. Given the significant grade change and the interior street being a service alley for trash and deliveries, no building pass-through shall be required on Pod A; however, a public outdoor amenity space will be provided in lieu of the building pass-through.
6. Average grade of a building is determined by calculating the average of the highest and lowest elevation along natural or improved grade (whichever is more restrictive) along the front of the building parallel to the primary street setback line.
 7. Any height encroachment not specifically listed is expressly prohibited except where the Town Manager determines that the encroachment is similar to a permitted encroachment listed below.
 - a. The maximum height limits do not apply to spires, belfries, cupolas, domes not intended for human occupancy; monuments, water tanks/towers or other similar structures which, by design or function, must exceed the established height limits.
 - b. The following accessory structures may exceed the established height limit of the district provided they do not exceed the maximum height by more than fifteen (15) percent of the maximum height limitation that defines the portion of the building envelope penetrated by such structures:
 - i. Chimney, flue or vent stack, spire, smokestack, water tank, windmill
 - ii. Rooftop deck, patio, shade structure
 - iii. Monument, steeple, flagpole
 - iv. Accessory radio or television antenna, relay tower
 - v. Transmission pole, tower or cable
 - vi. Garden, landscaping
 - vii. Skylight
 - viii. Cupola, clock tower or decorative tower not exceeding twenty (20) percent of the principal building footprint
 - ix. Parapet wall
 - x. Solar panel, wind turbine, rainwater collection system
 - c. The following accessory structures may exceed the established height limits provided they do not exceed the maximum building height by more than ten (10) feet, do not occupy more than twenty-five (25) percent of the roof area, and are set back at least ten (10) feet from the edge of the roof: Elevator or stairway access to roof, Greenhouse and Mechanical equipment.
 - d. An accessory structure located on the roof must not be used for any purpose other than a use incidental to the principal use of the building.

Parking is an essential element to a successful mixed use project, but access and sight-lines must not interfere with the pedestrian experience or safety. Given that the project has existing tenants with varying degrees of protection over existing parking areas, a portion of the existing surface lots will remain as-is in the near-term, but will offer more opportunities for redevelopment over time. In addition to the parking regulations outlined in the Street Frontage requirements herein, the minimum and maximum number of required parking spaces for vehicles and bicycles for new buildings shall be as follows:

PARKING REQUIREMENTS

	VEHICLE		BICYCLE	
	MIN.	MAX.	MIN.	SHORT/ LONG TERM
RESIDENTIAL USES				
Multifamily Dwelling Units, as follows:				
Studio / 1 Bedroom	1.0 per Unit	1.25 per Unit	1 per 4 units	20% / 80%
2 Bedroom	1.25 per Unit*	1.75 per Unit	1 per 4 units	20% / 80%
3 Bedroom	1.75 per Unit	2.25 per Unit	1 per 4 units	20% / 80%
4+ Bedroom	2.0 per Unit	2.5 per Unit	1 per 4 units	20% / 80%
Independent Senior Living Facility	0.5 per Unit	1.0 per Unit*	1 per 2 units*	20% / 80%
COMMERCIAL USES				
All Commercial Uses, Except:	1 per 300 SF*	1 per 200 SF*	1 per 2,500 SF*	80% / 20%
Restaurant/Bar	1 per 150 SF*	1 per 75 SF*	1 per 2,500 SF*	80% / 20%
OFFICE USES				
All Office Uses	1 per 300 SF*	1 per 200 SF*	1 per 2,500 SF*	50% / 50%*
HOTEL USES				
Hotel or Motel	0.9 per Lodging Unit	1.25 per Lodging Unit	1 per 15 Lodging Units	20% / 80%

*Denotes a modification from LUMO Standards.

NOTES

1. Existing buildings may be renovated or otherwise modified without providing additional bicycle or vehicular parking, so long as there is no increase to the overall building's finished square footage. If an existing building's overall finished square footage is increased, additional parking is only required for the new square footage added. A change of use to any existing structure shall not require additional parking. Note that this statement also applies to a structure that exists in the future and is then expanded.
2. Minor modifications to parking areas shall include parking areas that are generally remaining the same and undergoing modifications to the layout of the spaces only. Major modifications include adding parking area somewhere where there was previously no parking area. If minor modifications are made to existing parking areas, the owner will not be required to bring those entire parking areas into full compliance so long as the result of the minor modifications are closer to compliance than the existing conditions. If major modifications are made, the parking area must be brought into full compliance per requirements of the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual. If the applicant and Planning Staff disagree, the Town Manager shall ultimately determine whether or not changes to the parking fields are considered major or minor modifications.
3. If parking, access or other internal driveways, etc. are modified to accommodate the reconfiguration of existing buildings and not new buildings, those modifications shall not be subject to any frontage, BTZ or other requirements unless modifications are to accommodate new buildings, in which case they shall be subject to all requirements for new buildings including frontage, BTZ, etc.
4. The minimum parking requirement (vehicle or bicycle) may be reduced by up to 30% for shared parking between a mix of uses. Any reduction to the minimum parking shall be reviewed and approved by the Town Manager.
5. Short-term bicycle parking minimums required may be reduced if existing short-term bicycle parking is located within 100' of the proposed building. The requirement shall be reduced on a one for one basis for each space located within 100' up to 25% of the total required short-term bicycle parking.
6. Any structured parking visible to the public shall be required to include architectural screening compatible with the principal building it serves. All parking structure screening must be reviewed and approved by the Planning Staff. When feasible, consideration shall be given to providing commercial, office or other active uses on the ground floor of parking structures.
7. In the event that on-street parking is constructed as part of the construction of new buildings at the property, each on-street parking space may be used to count towards the required minimum parking.
8. Bicycle parking shall be required in accordance with the table above. Short-term parking shall be publicly accessible and be located within 100' from a building entrance that the bicycle rack is intended to serve. Long-term parking is intended for employees and residents and must be covered, weather-resistant and within 100 feet from a building entrance that the bicycle rack is intended to serve. Long-term bike parking can also be provided interior to a building or parking structure, without the need for a distance requirement.
9. All landscaping for parking areas shall meet requirements set forth in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual, with the exception of any modifications, variances or alterations that are approved with the Special Use Permit.
10. Pods may be developed with parking to serve the uses within the pod in excess of the maximum given that the parking provided on the overall site does not exceed the maximum.
11. A minimum of 20% of the new, modified surface parking spaces provided will be pre-wired for electric charging stations. In all pods, modified parking lots with more than 50 spaces shall have electric vehicle charging stations provided on at least 3% of all new or modified parking spaces. The new parking along the internal main street driveway shall be exempt from this requirement.

NOTES

Drive-thru facilities shall be permitted in each pod, but must adhere to the following guidelines:

1. Drive-thru windows, lanes, menu boards, trash receptacles, ordering box, and other objects associated with the drive-thru, may not be placed along the public street facing façade of the associated building. This shall exclude any exit lanes from a drive-thru when necessary for adequate vehicular circulation so long as it does not conflict with primary pedestrian access points to and from the associated building.
2. Adequate stacking space must be made available on-site for any use having a drive-thru. No more than 2 drive-thru lanes are permitted for any single use with drive-thru, with the exception of a bank being allowed to have 3 drive-thru lanes maximum. Required stacking shall be as follows:
 - a. restaurant (including a coffee shop) with a drive-thru must provide a minimum of 5 spaces before the order board, with another 3 spaces provided between the order board and the transaction window
 - b. a bank with a drive-thru must provide a minimum of 3 spaces measured from the teller box
 - c. a pharmacy with a drive-thru must provide a minimum of 3 spaces measured from the order box
 - d. a dry cleaner with a drive-thru must provide a minimum of 3 spaces measured from the pick up door
 - e. stacking required for all other uses will be determined by the Town Manager. The number of required stacking spaces includes the space at the window or communication/ mechanical device (e.g., order board, pick up window). If a drive-thru has multiple order boxes, teller boxes or pick up windows, the number of required stacking spaces may be split between each order box, teller box or pick up window.
3. Vehicles may not encroach on or interfere with the public use of streets and sidewalks by vehicles, bicycles or pedestrians. Drive-thru lanes must be separated by striping or curbing from other parking areas. Individual lanes must be striped, marked or otherwise distinctly delineated.
4. Drive-thru windows and lanes must be screened from the public realm and any adjacent ground floor residential use for the entire length of the drive-thru lane, including but not limited to menu boards, stacking lanes, trash receptacles, ordering box, drive up windows, and other objects associated with the drive-thru must be screened. Screening must be a continuous compact evergreen hedge. At the time of installation, the screening must be at least 3 feet in height and reach a height of 4 feet within 3 years of planting. In lieu of the compact evergreen hedge, a screening wall with a minimum height of 4 feet may be installed. The wall must be compatible with the principal building in terms of texture, quality, material and color.
5. The above standards are not applicable to existing drive-thru facilities located at the property.

DESIGN CRITERIA

OUTDOOR AMENITY SPACE

Outdoor amenity space shall be provided and may include green space, courtyards, plazas, water features, amphitheaters, patios, rooftops, art, dog parks, playgrounds, pavilions, sport courts, community gardens and other public elements. It should be designed and furnished to encourage activity and create comfortable space for all to enjoy. The outdoor amenity space shall include trees for shade along with other landscaped areas that coordinate with the overall site development. When possible, use these green spaces or plazas to link adjoining buildings and provide clear connections to pedestrian circulation routes. Conceptual examples of those spaces are as follows:



EXAMPLE 1



EXAMPLE 2

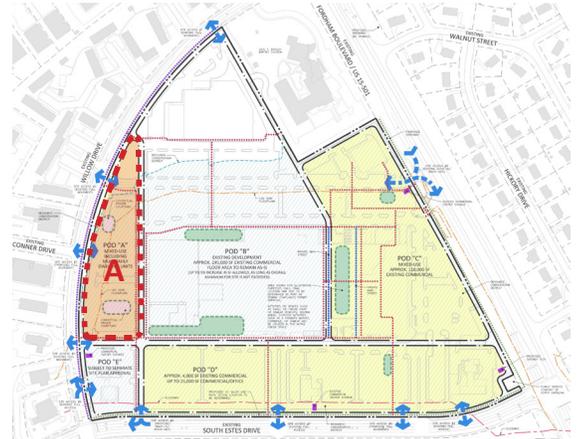
NOTES

1. Outdoor amenity space shall be provided for the entirety of the property at a minimum ratio of 4.6% of gross land area and shall be distributed throughout the site. Each Pod shall be required to provide outdoor amenity space at a minimum ratio of 3.6% of Pod area, so long as overall property minimum is met.
2. Outdoor amenity space shall be calculated as a function of gross land area of the development. Outdoor amenity space may be met in one contiguous open area or in multiple open areas on the lot and must meet minimum dimensions described as follows:
 - a. Where not located adjacent to a building, or where located adjacent to a building that is three stories in height or less, an outdoor amenity space must be at least 10 feet in width and length, with a minimum area of 100 square feet.
 - b. Where located adjacent to a building that is four stories in height or greater, the outdoor amenity space shall have greater dimensions, such that the space is in proportion to the associated building, provides a comfortable scale for pedestrians, and invites public use and enjoyment. In no case shall the area of a single outdoor amenity space be required to exceed the minimum outdoor amenity space ratio as specified for the property.
 - c. Where located in the build-to zone and used to create inviting space along a street facing façade, the width of the outdoor amenity space measured perpendicular to the right-of-way may be less than the dimension prescribed above, subject to approval of a design alternative.
3. Outdoor amenity space may be counted to meet the build-to-zone percentage requirements; however, only half the width of the applicable outdoor amenity space can be counted toward the required percentage.
4. Where pedestrian pass-throughs are provided, they may qualify as outdoor amenity space if they are unobstructed above by any building elements and meet all other requirements of this section. A building element used for shade purposes, such as a pergola or canopy, which allows partial views to the sky, may be considered as unobstructed above.
5. Outdoor amenity space cannot be parked or driven upon, except for emergency access and permitted temporary events.
6. Note that streetscape components and parking lot landscaping shall not be included in the outdoor amenity space calculations.
7. Outdoor amenity spaces may include but are not limited to facilities such as examples listed below:
 - a. green space
 - b. courtyards
 - c. seating area plazas
 - d. water features
 - e. amphitheaters
 - f. patios
 - g. parks
 - h. rooftops
 - i. public art
 - j. dog parks
 - k. playgrounds
 - l. pavilions
 - m. sport courts
 - n. community gardens
 - o. other outdoor public elements
8. Outdoor amenity space shall meet ADA accessibility standards.

IV. DEVELOPMENT OPTIONS

POD A

Pod A converts an existing surface parking area into a more dense, multi-use part of the site. A new parking structure is proposed with a residential wrap screen. Given the significant grade change along Willow Drive and the interior street being a service alley for trash and deliveries, no building pass-through shall be required on Pod A; however, a public outdoor amenity space will be provided in lieu of the building pass-through along Willow Drive.



KEY MAP

DEVELOPMENT EXAMPLE

- Building A:
 - ±250 units Residential
 - ±4,000 SF Incubator retail
 - 2 private resident amenity courtyards
- Parking Structure: Parking Spaces for covered retail parking + multifamily residents/guests
- Outdoor Amenity Space



CONCEPT MASSING*

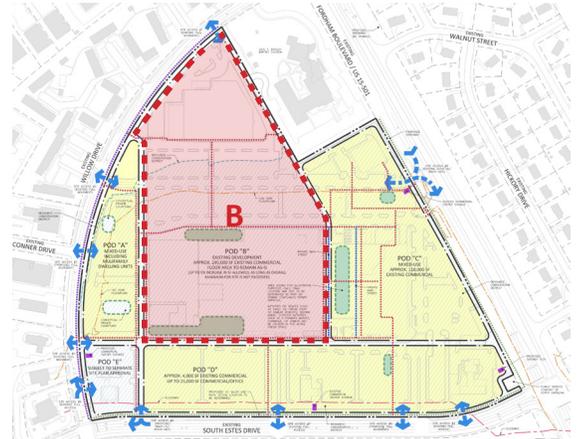
*This concept massing view is conceptual in nature and is provided for reference only. Building locations, access points, outdoor amenity space locations, and other design decisions shall be finalized during the Zoning Compliance Permit (ZCP) process.

DEVELOPMENT OPTIONS

PODS

POD B

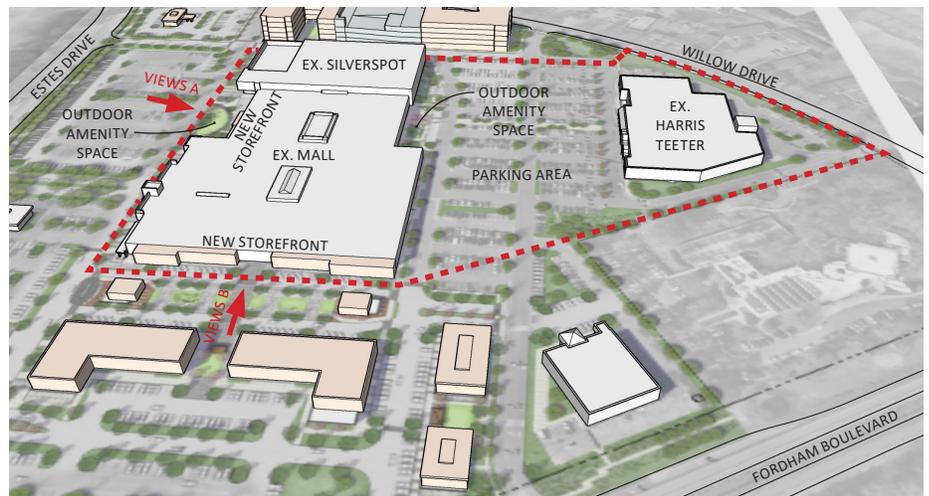
There is ±295,000 SF of existing commercial to remain and be renovated, with conversion of interior mall space into exterior space. Enhanced outdoor amenity and green spaces are to be provided throughout Pod B along with parking improvements. Examples of conceptual facade and outdoor amenity space improvements to occur in Pod B are shown in the storefront rendering views below.



KEY MAP

DEVELOPMENT EXAMPLE

- ±295,000 SF Existing Commercial to Remain
- New Storefronts along portions of existing mall (see views below)
- Outdoor Amenity Space



CONCEPT MASSING*

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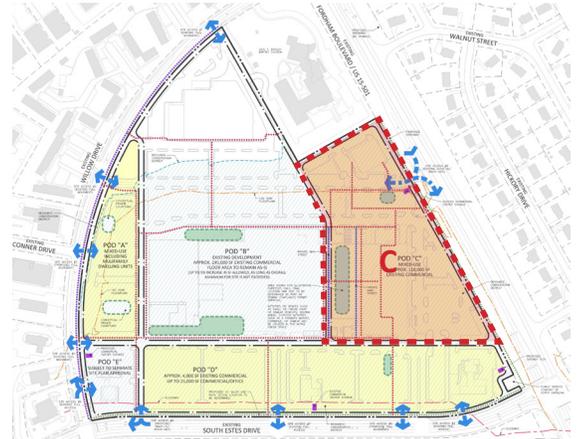


CONCEPT STOREFRONT VIEWS A

CONCEPT STOREFRONT VIEWS B

POD C

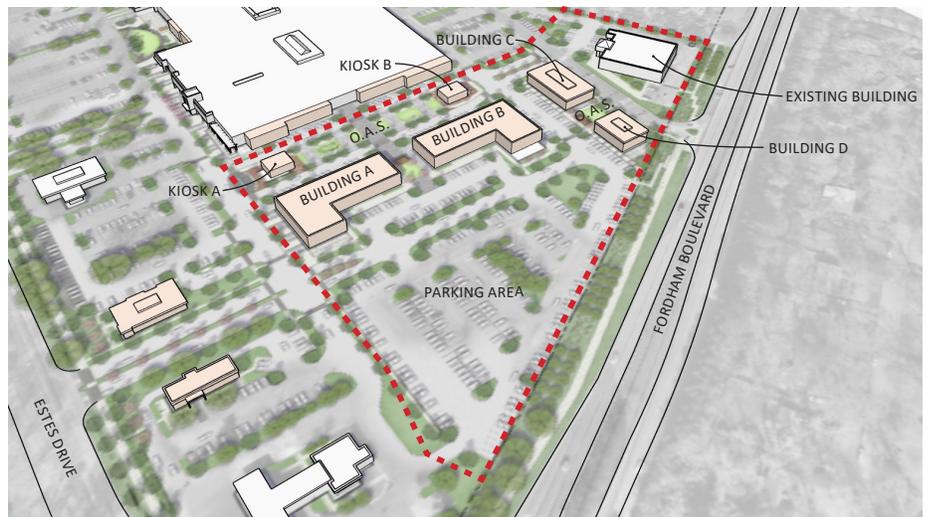
Pod C will be focused around a central park or green space that will be pedestrian focused and include programmed space for the farmers market and other recurring events. The central park area will be flanked by urban 'main streets' with a consistent streetscape experience that is designed to promote safety and minimize pedestrian-vehicle conflicts. This area will act as the heart of the project, with pathways busy during the day and into the evening in this vibrant urban environment.



KEY MAP

DEVELOPMENT EXAMPLE 1

- Demolition of ±105,000 SF of Existing Mall
- Existing Building: ±11,200 SF Commercial
- Building A: ±13,500 SF Commercial
- Building B: ±13,500 SF Commercial
- Building C: ±6,000 SF Commercial
- Building D: ±3,300 SF Commercial
- Kiosk A: ±1,200 SF Commercial
- Kiosk B: ±1,200 SF Commercial
- Outdoor Amenity Space (O.A.S.)



CONCEPT MASSING*

DEVELOPMENT EXAMPLE 2

- Demolition of ±105,000 SF of Existing Mall
- Existing Building: ±11,200 SF Commercial
- Building A: ±13,500 SF Commercial
- Building B: ±13,500 SF Commercial
- Building C: ±6,000 SF Commercial
- Building D: ±3,300 SF Commercial
- Building E: ± 45,000 SF Office
- Building F: ±125 room Hotel
- Kiosk A: ±1,200 SF Commercial
- Kiosk B: ±1,200 SF Commercial
- Outdoor Amenity Space (O.A.S.)

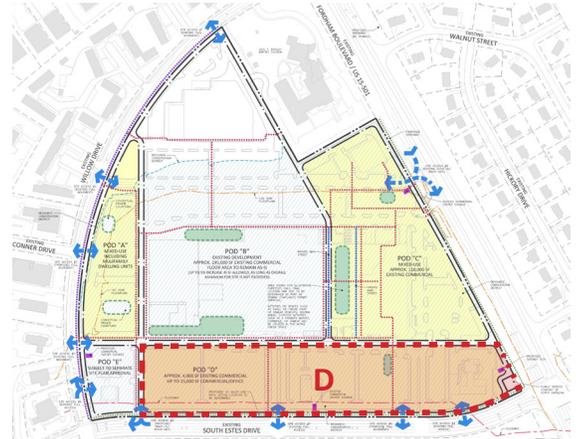


CONCEPT MASSING*

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POD D

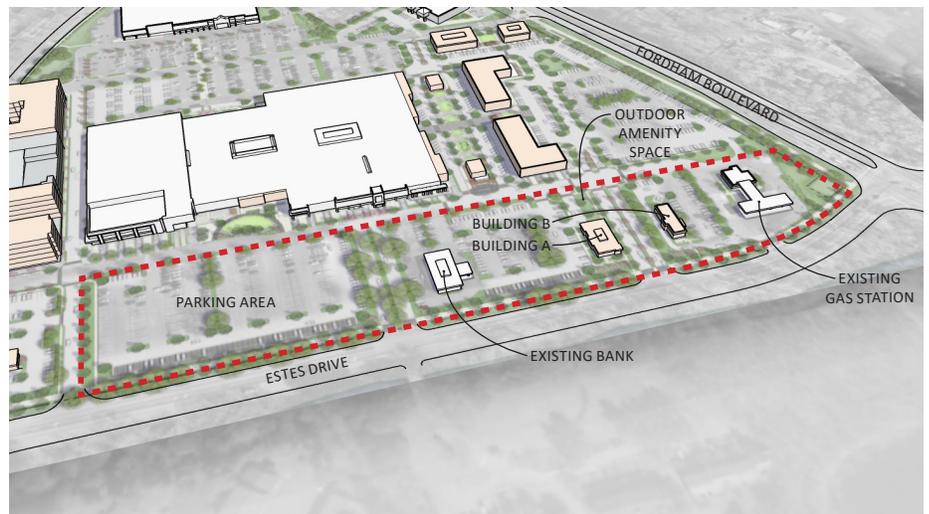
Pod D includes existing parking areas and buildings to remain, amongst 2 proposed commercial buildings lining a relocated driveway. Note that this pod faces limitations to major design changes because of sensitivity to being in a floodplain. The proposed development should assist with flooding concerns because it overall reduces the amount of impervious area on site.



KEY MAP

DEVELOPMENT EXAMPLE

- Existing Gas Station: ±2,000 SF
- Existing Bank: ±2,400 SF
- Building A: ±3,500 SF Bank Commercial
- Building B: ±2,800 SF Commercial
- Outdoor Amenity Space

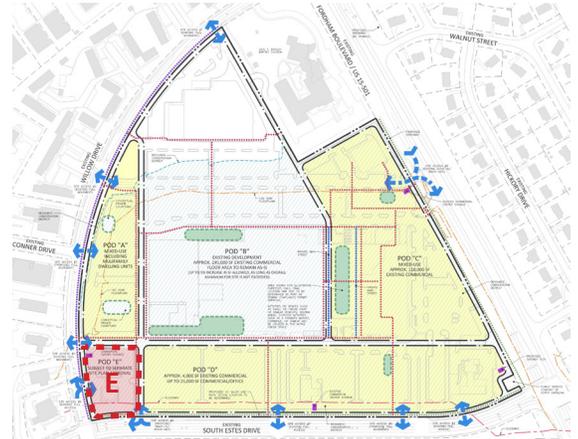


CONCEPT MASSING*

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POD E

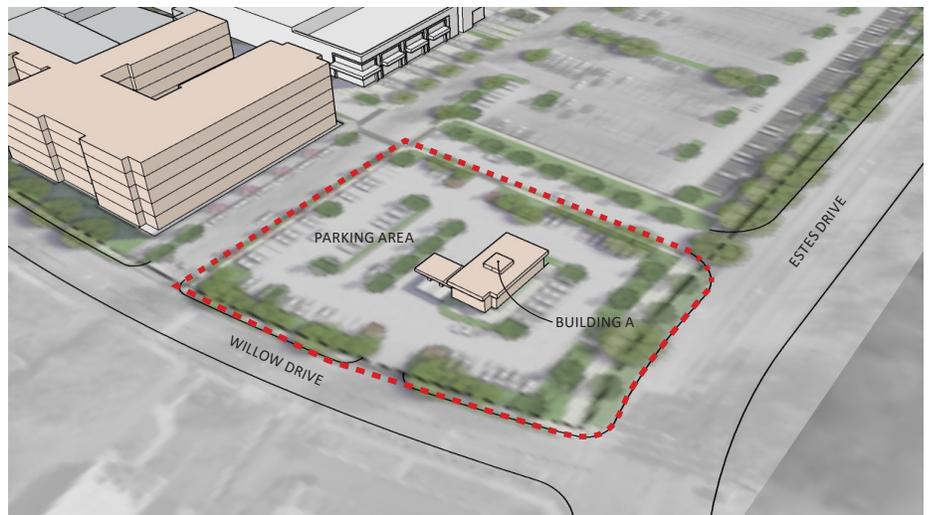
In Pod E, the Zoning Compliance Permit (ZCP) is under review for minor modifications to the existing SUP. The bank use is to remain and only parking modifications and potential driveway relocation are to occur in Pod E.



KEY MAP

DEVELOPMENT EXAMPLE

- Building A: ±2,000 SF Bank



CONCEPT MASSING*

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V. ADMINISTRATION

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS:

This list is provided for convenience only and is not meant to be exhaustive. Refer to the balance of the document for more information.

1. Where the street frontage standards listed herein conflict due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Planning Staff.
2. Building pass-throughs shall be a minimum height equal to the first floor ceiling height of the adjacent building except when life safety service is required. For buildings 4-stories or greater, a building pass-through shall be a minimum width of 20'. For building 4-stories or less, the building pass-through shall be a minimum width of 12'. Building pass-throughs shall be adequately lit. Building pass-throughs shall serve as a publicly accessible passage between or through buildings that allows pedestrians to move from one side of a building to another, through a privately owned or publicly dedicated area.
 - a. The maximum building pass-through spacing may be increased by five percent (5%) through a minor modification where one or more of the following applies:
 - i. proposed to protect sensitive natural areas or save healthy existing trees
 - ii. required to protect natural conditions, such as watercourses, riparian buffers, natural rock formations or topography
 - iii. required based on some unusual aspect of the development site or the proposed development that is not shared by landowners in the surrounding area (e.g., unusual lot size or configuration)
 - iv. required due to the presence of existing utilities or other easements
 - v. proposed because there are no other options for ingress and egress.
 - b. Where the Planning Staff makes a finding that a proposed design alternative for building pass-through will provide access that at a minimum meets the purpose or intent the pass through requirement and where one or more of the site constraints listed below applies, the Planning Staff may approve an alternatively designed building pass-through:
 - i. proposed to protect sensitive natural areas or save healthy existing
 - ii. required to protect natural conditions, such as watercourses, riparian buffers, natural rock formations or topography
 - iii. required based on some unusual aspect of the development site or the proposed development that is not shared by landowners generally in the surrounding area (e.g., unusual lot size or configuration)
 - iv. required due to the presence of existing utilities or other easements.
 - c. Modifications to existing structures shall not trigger any requirement for a building pass-through. Only new buildings shall be subject to the pass-through requirement.
 - d. Where building pass-throughs cannot be implemented because of limitations caused by existing conditions, a public amenity must be provided in its place to continue a piece of the pedestrian experience in an alternative way. Given the significant grade change and the interior street being a service alley for trash and deliveries, no building pass-through shall be required on Pod A; however, a public outdoor amenity space will be provided in lieu of the building pass-through.
3. Willow Drive street frontage section modified from current streetscape requirements in the Town's mobility plan to preserve existing mature trees along Willow Drive frontage. A 4.5' foot bike lane and 5' foot sidewalk shall be provided along the Willow Drive frontage.
4. Estes Drive street frontage section modified from current streetscape requirements in the Town's mobility plan due to environmental constraints and to preserve existing mature trees along Willow Drive. A 10' foot multi-use path shall be provided with 2' foot clear zone along Estes Drive in lieu of a 5' foot bike lane and 5' foot sidewalk.

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS (CONTINUED...):

5. No build-to-zone shall be required on Estes Drive due to the flood sensitivity and proximity to the FEMA floodway where buildings may not encroach.
6. As of the date of these Design Standards, a ZCP and minor modification to the current SUP is under review and shall not be subject to these Design Standards unless future modifications occur following completion of the pending minor modification and subsequent construction related to the minor modification.
7. The alley between Pod A and Pod B currently functions as a service area for commercial tenant's trash, grease disposal, loading/unloading and to access parking. Given that the function of that alley is not intended to change, certain modifications to the Design Standards have been requested herein to specifically address how new development interacts with the alley versus how it would otherwise interact with a street (public or internal).
8. The property contains several mature trees around the perimeter, which are intended to remain. As such, all tree requirements shall allow flexibility to address those mature trees, which may require modifications from the required standards contained herein.
9. Outdoor amenity space shall be provided for the entirety of the property at a minimum ratio of 4.6% of gross land area and shall be distributed throughout the site. Each Pod shall be required to provide outdoor amenity space at a minimum ratio of 3.6% of Pod area, so long as overall property minimum is met.
10. For any NCDOT roads, streetscape & landscaping requirements are subject to NCDOT approval and may be modified as needed so long as Planning Staff agrees with the requested changes.
11. Note that these Design Standard street frontage requirements shall take the place of perimeter buffer requirements that are outlined in LUMO Section 5.6.
12. Note that if a building fronts the Main Street it is exempt from other frontage/BTZ requirements.
13. The required percentage of building façade in the BTZ is measured by the width of the building along the street frontage divided by the buildable width of the lot. Since the project may occur in phases, the buildable width of the lot shall only include the portion of the lot included and being disturbed in any such phase of construction. If any such phasing creates a non-conforming frontage request until a future phase is built, a design alternative may be reviewed and approved by the Town Manager.
14. Co-working space shall be permitted under Business, Office-Type.
15. The internal main street driveway section shown below is mainly shown for design intent only. There may be a need for flexibility in this layout to accommodate specific urban amenities and groups that will utilize this space such a farmer's market, events lawn, and other events such as hosting larger community gatherings. Note that if a design alternative is proposed for this example streetscape section, Planning Staff can approve the alternative so long as it meets the overall intent.
16. Retail kiosks and other structures may be placed in the outdoor amenity space along the internal main street driveway.
17. A tree shall be provided at least every 40' measured linearly along each side of the internal main street driveway. These trees may be placed within the outdoor amenity space, streetscape amenity zone or in the parking islands; however, only the trees located on each side of the centerline may count towards that half of the streetscape's tree requirement.

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS (CONTINUED...):

18. Minor modifications to parking areas shall include parking areas that are generally remaining the same and undergoing modifications to the layout of the spaces only. Major modifications include adding parking area somewhere where there was previously no parking area. If minor modifications are made to existing parking areas, the owner will not be required to bring those entire parking areas into full compliance so long as the result of the minor modifications are closer to compliance than the existing conditions. If major modifications are made, the parking area must be brought into full compliance per requirements of the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual. If the applicant and Planning Staff disagree, the Town Manager shall ultimately determine whether or not changes to the parking fields are considered major or minor modifications.
19. The minimum parking requirement (vehicle or bicycle) may be reduced by up to 30% for shared parking between a mix of uses. Any reduction to the minimum parking shall be reviewed and approved by the Town Manager.
20. All landscaping for parking areas shall meet requirements set forth in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual, with the exception of any modifications, variances or alterations that are approved with the Special Use Permit.
21. When outdoor amenity space is located in the build-to zone and used to create inviting space along a street facing façade, the width of the outdoor amenity space measured perpendicular to the right-of-way may be less than the dimension prescribed in those criteria notes, subject to approval of a design alternative.