

TOWN OF CHAPEL HILL

Town Hall 405 Martin Luther King Jr. Boulevard Chapel Hill. NC 27514

Item Overview

Item #: 14., File #: [20-0867], Version: 1 Meeting Date: 12/9/2020

Close the Public Hearing to Consider a Request to Close a Portion of an Unmaintained and Unimproved Monroe Street Public Right-of-Way.

Staff: Department:

Lance Norris, Director Public Works

Chris Roberts, Manager of Engineering and Infrastructure

Overview: The developer of the Columbia Street Annex, CH Hotel Associates Limited Partnership, proposes to close approximately 150' of an unmaintained and unimproved portion of Monroe Street. This closure is needed to relocate the entrance of the development on South Columbia Street. The 30'-wide public right-of-way starts at its connection to the South Columbia Street right-of-way and extends west. This closure will disconnect the current Monroe Street right-of-way from South Columbia Street. The developer will dedicate a new easement providing reasonable access to the right-of-way at Columbia Street.

The requesting party must record and file a full public access easement per the submitted plan with the Orange County Register of Deeds before the subject right-of-way is removed from the plat of the Columbia Street Annex property.

This right-of-way public hearing will be presented as part of the <u>Columbia Street Annex</u> https://www.townofchapelhill.org/government/departments-services/planning/development-activity-report/columbia-street-annex-2017> development public hearing (Project Number #18-039).

This process followed the two requirements of North Carolina General Statute Sec. 160A-299:

- Council adopted a resolution declaring its intent to close the public right-of-way and to call a public hearing.
- All publication and posting requirements of this statute have been met.



Recommendation(s):

That the Council close the Public Hearing to consider closing the unmaintained and unpaved public right-of -way portion of Monroe Street. Under the recently enacted legislation authorizing public hearings at remote meetings, public comment may continue to be submitted into the hearing record for 24 hours after the December 9th public hearing. On January 13th, the Council may take action on the requested closure.

Fiscal Impact/Resources: The Town did not maintain this right-of-way, so there is no cost to the Town.

Key Issues:

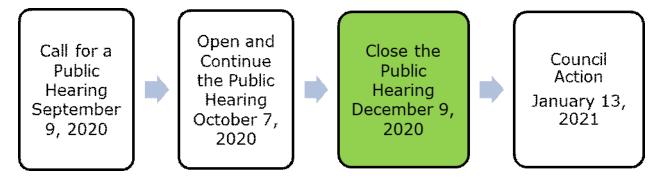
• The closure will isolate one property, which is currently accessed via a private driveway to the paved portion of Monroe Street. The developer has proposed a full public access easement, which will continue to provide reasonable legal access to this parcel. The requesting party must record and file a full public access easement per the submitted plan with the Orange County Register of Deeds before the subject right-of-way is removed from the plat of the Columbia Street Annex

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property.

Where is this item in its process?



Attachments:

- Draft Resolution
- Requested Monroe Street Public Right-of-Way Closure Plan
- NC General Statute 160A-299

Item #: 14., File #: [20-0867], Version: 1 Meeting Date: 12/9/2020

DRAFT RESOLUTION

A RESOLUTION TO APPROVE A REQUEST TO CLOSE A PORTION OF AN UNMAINTAINED AND UNIMPROVED PUBLIC RIGHT-OF-WAY ON MONROE STREET (2021-##-##/R-**)

WHEREAS, the Town of Chapel Hill has received a request from Wendi Ramsden, Coulter Jewell Thames, PA (representing CH Hotel Associates Limited Partnership, Owner) to close a portion of the unmaintained and unimproved Monroe Street public right-of-way; and

WHEREAS, one parcel will become isolated with this right-of-way closure; however, the right-of-way closure applicant will provide a legal reasonable means of access to this isolated property with a full public access easement; and

WHEREAS, dedication of the full public access easement will be required before the completion of the right -of-way closure process; and

WHEREAS, the Council finds, upon review of the facts and of information received at the Public Hearing, that closing this section of the Monroe Street right-of-way would not be contrary to the public interest and that no individual owning property in the vicinity of the proposed closures would be deprived of reasonable means of ingress and egress to his or her property by the closing of said right-of-way.

NOW, THEREFORE, BE IT RESOLVED by the Council of the Town of Chapel Hill that the Council hereby approves the closure of a portion of the unmaintained and unimproved public right-of-way of Monroe Street.

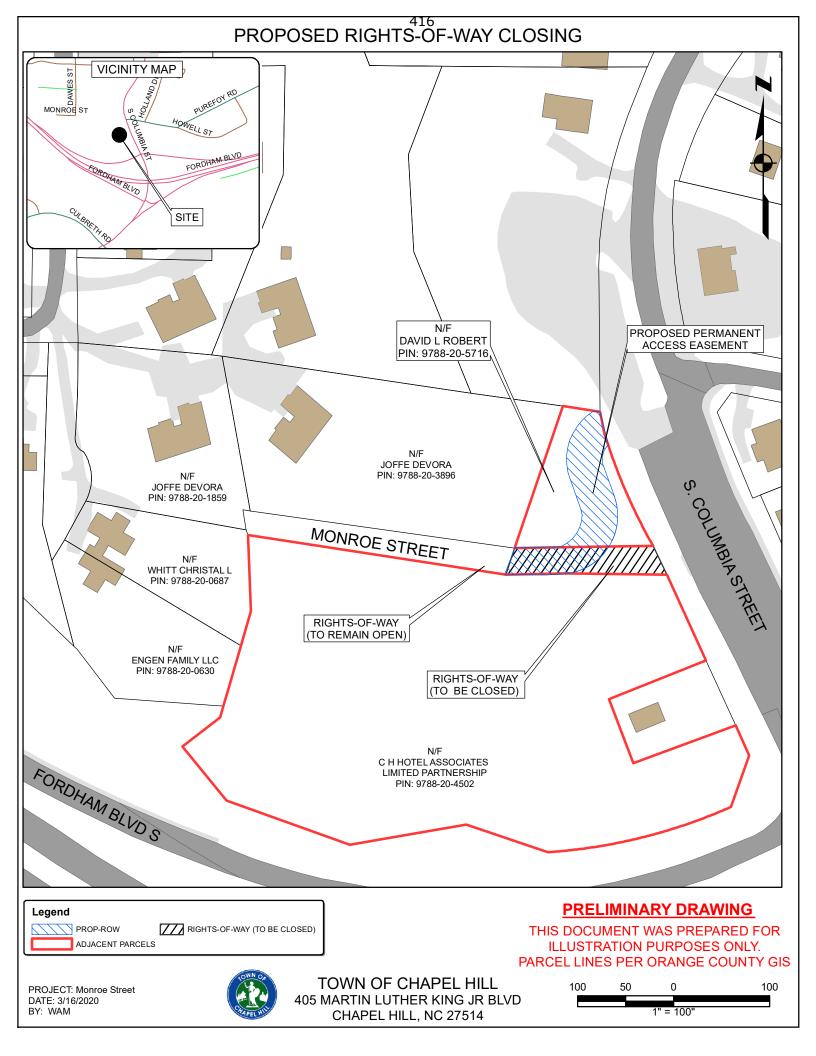
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The Agenda will reflect the text below and/or the motion text will be used during the meeting.

PRESENTER: Chris Roberts, Manager of Engineering and Infrastructure

- a. Introduction and recommendation
- b. Comments from the public
- c. Comments and questions from the Mayor and Town Council
- d. Motion to Close the Public Hearing and allow additional comments for 24-hours after discussing this item per recent Legislation
- e. Consider adopting the resolution to approve the right-of-way closure at the January 13, 2021 meeting.

RECOMMENDATION: That the Council continue and close a Public Hearing to consider closing the unmaintained and unpaved public right-of-way portion of Monroe Street. On January 13th, the Council may take action on the requested closure.



§ 160A-299. Procedure for permanently closing streets and alleys.

- When a city proposes to permanently close any street or public alley, the council shall first adopt a resolution declaring its intent to close the street or alley and calling a public hearing on the question. The resolution shall be published once a week for four successive weeks prior to the hearing, a copy thereof shall be sent by registered or certified mail to all owners of property adjoining the street or alley as shown on the county tax records, and a notice of the closing and public hearing shall be prominently posted in at least two places along the street or alley. If the street or alley is under the authority and control of the Department of Transportation, a copy of the resolution shall be mailed to the Department of Transportation. At the hearing, any person may be heard on the question of whether or not the closing would be detrimental to the public interest, or the property rights of any individual. If it appears to the satisfaction of the council after the hearing that closing the street or alley is not contrary to the public interest, and that no individual owning property in the vicinity of the street or alley or in the subdivision in which it is located would thereby be deprived of reasonable means of ingress and egress to his property, the council may adopt an order closing the street or alley. A certified copy of the order (or judgment of the court) shall be filed in the office of the register of deeds of the county in which the street, or any portion thereof, is located.
- (b) Any person aggrieved by the closing of any street or alley including the Department of Transportation if the street or alley is under its authority and control, may appeal the council's order to the General Court of Justice within 30 days after its adoption. In appeals of streets closed under this section, all facts and issues shall be heard and decided by a judge sitting without a jury. In addition to determining whether procedural requirements were complied with, the court shall determine whether, on the record as presented to the city council, the council's decision to close the street was in accordance with the statutory standards of subsection (a) of this section and any other applicable requirements of local law or ordinance.

No cause of action or defense founded upon the invalidity of any proceedings taken in closing any street or alley may be asserted, nor shall the validity of the order be open to question in any court upon any ground whatever, except in an action or proceeding begun within 30 days after the order is adopted. The failure to send notice by registered or certified mail shall not invalidate any ordinance adopted prior to January 1, 1989.

(c) Upon the closing of a street or alley in accordance with this section, subject to the provisions of subsection (f) of this section, all right, title, and interest in the right-of-way shall be conclusively presumed to be vested in those persons owning lots or parcels of land adjacent to the street or alley, and the title of such adjoining landowners, for the width of the abutting land owned by them, shall extend to the centerline of the street or alley.

The provisions of this subsection regarding division of right- of-way in street or alley closings may be altered as to a particular street or alley closing by the assent of all property owners taking title to a closed street or alley by the filing of a plat which shows the street or alley closing and the portion of the closed street or alley to be taken by each such owner. The plat shall be signed by each property owner who, under this section, has an ownership right in the closed street or alley.

- (d) This section shall apply to any street or public alley within a city or its extraterritorial jurisdiction that has been irrevocably dedicated to the public, without regard to whether it has actually been opened. This section also applies to unopened streets or public alleys that are shown on plats but that have not been accepted or maintained by the city, provided that this section shall not abrogate the rights of a dedicator, or those claiming under a dedicator, pursuant to G.S. 136-96.
- (e) No street or alley under the control of the Department of Transportation may be closed unless the Department of Transportation consents thereto.

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- (f) A city may reserve a right, title, and interest in any improvements or easements within a street closed pursuant to this section. An easement under this subsection shall include utility, drainage, pedestrian, landscaping, conservation, or other easements considered by the city to be in the public interest. The reservation of an easement under this subsection shall be stated in the order of closing. The reservation also extends to utility improvements or easements owned by private utilities which at the time of the street closing have a utility agreement or franchise with the city.
- (g) The city may retain utility easements, both public and private, in cases of streets withdrawn under G.S. 136-96. To retain such easements, the city council shall, after public hearing, approve a "declaration of retention of utility easements" specifically describing such easements. Notice by certified or registered mail shall be provided to the party withdrawing the street from dedication under G.S. 136-96 at least five days prior to the hearing. The declaration must be passed prior to filing of any plat or map or declaration of withdrawal with the register of deeds. Any property owner filing such plats, maps, or declarations shall include the city declaration with the declaration of withdrawal and shall show the utilities retained on any map or plat showing the withdrawal. (1971, c. 698, s. 1; 1973, c. 426, s. 47; c. 507, s. 5; 1977, c. 464, s. 34, 1981, c. 401; c. 402, ss. 1, 2; 1989, c. 254; 1993, c. 149, s. 1; 2015-103, s. 1.)

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TOWN OF CHAPEL HILL

Town Hall 405 Martin Luther King Jr. Boulevard Chapel Hill, NC 27514

Item Overview

Item #: 15., File #: [20-0868], Version: 1 Meeting Date: 12/9/2020

Continue the Public Hearing: Conditional Zoning Application for Columbia Street Annex, 1150 S. Columbia Street, from Residential-2 (R-2) to Mixed Use-Village-Conditional Zoning District (MU-V-CZD).

See Staff Report on next page.

The Agenda will reflect the text below and/or the motion text will be used during the meeting.

PRESENTER: Jake Lowman, Senior Planner

- a. Without objection, the revised report and any other materials submitted at the hearing for consideration by the Council will be entered into the record
- b. Introduction and revised recommendation
- c. Presentation by the applicant
- d. Comments from the public
- e. Comments and questions from the Mayor and Town Council
- f. Referral to the Manager and Attorney
- g. Motion to close the Public Hearing and receive public comment for 24-hours
- h. Consider enacting the ordinance at the January 13, 2021 Council meeting.

RECOMMENDATION: That the Council continue the public hearing, receive comment on the proposed Conditional Zoning and close the public hearing. That the Council then make a motion to close the public hearing and consider the proposed Conditional Rezoning application on January 13, 2021.



CONTINUE THE PUBLIC HEARING FOR CONDITIONAL ZONING AT COLUMBIA STREET ANNEX FROM RESIDENTIAL-2 (R-2) TO MIXED USE-VILLAGE-CONDITIONAL ZONING **DISTRICT (MU-V-CZD)**

STAFF REPORT

TOWN OF CHAPEL HILL PLANNING DEPARTMENT

Colleen Willger, Director

Judy Johnson, Operations Manager Jake Lowman, Senior Planner

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DATE

APPLICANT

December 9, 2020 1150 S. Columbia Street

Wendi Ramsden, Coulter Jewell Thames on behalf of David Robert and C. H. Hotel Associates

UPDATES SINCE THE NOVEMBER 18, 2020 PUBLIC HEARING

Council requested additional information regarding the encroachment into the Resource Conservation District, traffic impacts, and safety concerns, and stormwater impacts. Staff will distribute a revised staff report and a revised ordinance on Monday, December 7. These revisions include adding or changing stipulations.

STAFF RECOMMENDATION

That the Council close the Public Hearing on the proposed Zoning Atlas Amendment. (Under the state legislation authorizing public hearings at remote meetings, public comment may continue to be submitted into the hearing record for 24 hours after the December 9 public hearing).

That the Council then adopt a motion to schedule the proposed Zoning Atlas Amendment for possible decision on January 13, 2021.

ZONING

Current:

Residential-2 (R-2) (Approximately 4

Proposed: Mixed Use-Village-Conditional Zoning

District (MU-V-CZD)

PROCESS

Conditional Zoning is a legislative process. It allows the Council to review the rezoning application for consistency with the Land Use Plan in the Comprehensive Plan and establish standards that address any impacts on surrounding properties.

The applicant has submitted a Land Use Plan Amendment for Council review, to update the site's designation to high-density residential.

PROJECT OVERVIEW

The Columbia Street Annex project is located at the northwest corner of S. Columbia Street and the westbound on-ramp of NC 54. The proposed development includes:

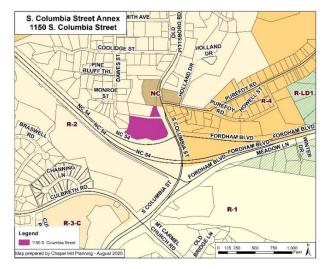
- a 6-story building set into the slope west of S. Columbia Street.
- up to 57,000 sq. ft. of residential floor area, for a maximum of 52 units including 8 affordable units (15%).
- a maximum of 4,000 sq. ft. of business/office
- a full-access driveway off S. Columbia Street with sidewalks and underground parking.

DECISION POINTS

The applicant requests the following modifications to regulations:

- Steep slope regulations
- Encroachment into the Resource Conservation District
- Reduced landscaping standards
- Mixed Use-Village land use mix standards

PROJECT LOCATION



ATTACHMENTS

- Technical Report and Project Fact Sheet
- Land Use Plan Amendment Technical Report
- **Draft Staff Presentation** 3.
- 4 Resolution A (Resolution of Consistency)
- Resolution B (Resolution Approving Land Use Plan Amendment) 5.
- 6. Revised Ordinance A (Enacting the Ordinance)
- Resolution C (Denying the Application) 7.
- 8. Advisory Board recommendations
- Applicant Materials (including Land Use Plan Amendment application materials)

10. Traffic Impact Study Executive Summary
11. Trip Generation Comparison from HNTB
12. Town of Chapel Hill Stormwater Vegetative Riparian Buffer and Stream Classification
13. State Stream Determination Letter dated June 19, 2018



TECHNICAL REPORT

KEY CONSIDERATIONS

<u>Planning Commission</u>: At the <u>October 20, 2020 meeting</u>, the Planning Commission recommended approval with the following modifications to Ordinance A:

• Bicycle parking should be significantly increased on-site.

Staff Response: The applicant has provided the minimum number of bicycle parking on site.

• If we want people to use transit they need to able to reach it as safely and efficiently as possible, then the existing pedestrian crosswalk by the bus stop should be retained- the southern cross walk is the more direct route to current bus stop and likely location of future BRT stop.

Staff Response: The current crosswalk will be moved to construct the left turn lane into the site, per NCDOT and Town. Staff recommends that a second crosswalk be installed at the NC 54 intersection with S. Columbia Street. The following stipulation is included in Revised Ordinance A:

<u>Pedestrian Improvements</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall submit plans for a pedestrian-activated signal with Rectangular Rapid Flashing Beacons for the crosswalk north of Purefoy Road on S. Columbia Street. The plans shall include ADA ramps aligned to serve the crosswalk location. The developer shall also provide a pedestrian crosswalk north of the 54 Bypass intersection with S. Columbia Street. The design of the crosswalks must be reviewed and approved by the Town Manager and NCDOT prior to issuance of a Zoning Compliance Permit.

• This is a terrible intersection - having both a southern and northern crossing signals to cars that they should slow down

Staff Response: Staff recommends the applicant work with NCDOT for appropriate signage along S. Columbia Street.

<u>Transportation and Connectivity Advisory Board</u>: At the <u>September 22, 2020 meeting</u>, the Transportation and Connectivity Advisory Board recommended denial, and asked that Council consider the following:

• 50 percent reduction of total parking spaces.

Staff Response: The Mixed Use-Village (MU-V) district allows for a 50 percent reduction in the minimum number of parking spaces, and the developer has identified that they may reduce the parking further than what is projected currently.

Dedicate transportation network company (TNC) parking spaces.

Staff Response: There are loading spaces at the back of the structure. Staff recommends that these parking spaces be considered for use by transportation network companies (such as Uber or Lyft).

• No Right on Red movements when the pedestrian signals are activated.

Staff Response: The following stipulation is included in Revised Ordinance A:

<u>Right Turn on Red Restrictions</u>: If the pedestrian signals are activated, the signal design include a restricted right-turn on red movement subject to review and approval by the Town Manager and NCDOT prior to issuance of a Zoning Compliance Permit.

 Install a crosswalk north of the S. Columbia Street and NC 54 Bypass intersection and install a crosswalk south of the S. Columbia Street and Purefoy Road intersection.

Staff Response: The following stipulation is included in Revised Ordinance A:

<u>Pedestrian Improvements</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall submit plans for a pedestrian-activated signal with Rectangular Rapid Flashing Beacons for the crosswalk north of Purefoy Road on S. Columbia Street. The plans shall include ADA ramps aligned to serve the crosswalk location. The developer shall also provide a pedestrian crosswalk north of the 54 Bypass intersection with S. Columbia Street. The design of the crosswalks must be reviewed and approved by the Town Manager and NCDOT prior to issuance of a Zoning Compliance Permit.

 Coordinate with Town Staff on the NS BRT station location to ensure safe and convenient access for pedestrians.

Staff Response: Additional traffic counts, to be completed prior to issuance of a Final Plan Zoning Compliance Permit, will include the impacts of the North-South Bus Rapid Transit (BRT). The following stipulation is included in Revised Ordinance A:

<u>Traffic Counts</u>: Prior to issuance of a Zoning Compliance Permit, additional traffic counts shall be conducted at locations to be review and approval by NCDOT and the Town. The report shall address the North-South Bus Rapid Transit project.

• Provide improved lighting at the crosswalks.

Staff Response: Staff recommends the following stipulation in Revised Ordinance A:

<u>Pedestrian Crossings Lighting</u>: Prior to issuance of a Zoning Inspection Final, adequate lighting shall be provided at both pedestrian crosswalks on S. Columbia Street. Designs shall be reviewed and approved by NCDOT and the Town Manager prior to issuance of a Zoning Compliance Permit.

Provide a current traffic impact analysis study.

Staff Response: Please see comment above.

<u>Environmental Stewardship Advisory Board</u>: At the <u>September 14, 2020 meeting</u>, the Environmental Stewardship Advisory Board recommended approval with the following modifications to Ordinance A:

 Request that the Stormwater Management Utility Advisory Board make recommendations on the applicant's proposal for stream restoration, including more details about flora and fauna.

Staff Response: The Council has not required the applicant to present to the Stormwater Management Utility Advisory Board, and the applicant has not volunteered to attend at this time.

• A traffic study with particular attention to carbon impacts from vehicle idling, as well as commuter traffic and congestion.

Staff Response: Additional traffic counts will be conducted prior to issuance of a Final Plan Zoning Compliance Permit. The additional analysis would not include an evaluation of these environmental impacts.

• Because an all-electric building is proposed, strong consideration must be given to installing a rooftop solar PV system

Staff Response: Staff recommend that Council consider the request, with applicant's acceptance of any requested changes.

 Comply with the Transportation and Connectivity Advisory Board's recommended guidance for EV stations

Staff Response: Staff recommend that Council consider the request, with applicant's acceptance of any requested changes. Transportation and Connectivity Advisory Board recommends at least 3 percent of all parking should provide EV stations and 20 percent of all total parking spaces should be made "EV Ready" which includes the dedicated installation of electrical circuits and underground conduit.

<u>Housing Advisory Board</u>: At the <u>September 8, 2020 meeting</u>, the Housing Advisory Board recommended approval with the following modifications to Ordinance A:

• That the applicant reach out to the Community Home Trust to confirm that its affordable housing plan aligns with their current and projected housing needs.

Staff Response: The applicant consulted with the Community Home Trust and we have received the following response from Kimberly Sanchez, Executive Director: "Developer Roland Gammon has shared his plans for the Columbia Street Annex with Community Home Trust. We have reviewed his plans and designs. We understand he has proposed to provide eight total units of his total 52 units in the building as Affordable--four at 65% AMI and four at 80% AMI. This satisfies the 15% TOCH Inclusionary Housing Ordinance. Of the eight, four would be 1BR of ~576 SF and four would be 2BR of ~864 SF. The eight units would be spread among floors 1-3 of the building.

Mr. Gammon has agreed that he is further willing to establish the HOA dues for the eight affordable units at a reduced rate relative to all other units. We have shared language that has been used successfully in other projects in Town that provides for enduring protection for the affordability of assessments. He has also agreed to establishing a Town Mandated

Transfer Fee in the ownership documents. To achieve these goals, he has agreed that he would work with the CHT team to determine (1) final unit pricing of the affordable units (2) reduced and affordable monthly HOA dues and (3) an appropriate Town Mandated Transfer Fee for the project.

We have experienced good demand for the types of units suggested as affordable and consider their proposed distribution within the building a sensible plan and are supportive of the project..."

PROJECT OVERVIEW

The application proposes a Mixed Use-Village-Conditional Zoning District (MU-V-CZD) in order to accommodate 52 residential dwelling units and approximately 4,000 square feet of commercial office space. The existing vacant site is located adjacent to NC 54 and S. Columbia Street. The Statement of Justification explains that the applicant is seeking a Conditional Zoning in order to develop the site to a greater intensity than what is allowed by the existing Residential-2 (R-2) zoning district. More details about the proposed development can be found in the applicant's narrative and Statement of Justification (Attachment 10).

Information about the site and proposed zoning district can be found below, as well as a list of proposed Modifications to Regulations, other important considerations that staff has identified, and an analysis of the project's consistency with the Comprehensive Plan and relevant Findings of Fact.

PROJECT HISTORY

1997	Special Use Permit and Zoning Atlas Amendment for 27 apartment units and ultimately denied in 1999.
2007	Concept Plan proposal for construction of 87,000 square feet of floor area in four buildings. A total of 32 dwelling units and 12,000 square feet of retail and office space with parking for 80 vehicles was proposed.
2013	Revised Concept Plan submitted with 44 dwelling units, 8,000 square feet of retail and office space with parking for 64 vehicles proposed.
2014	Special Use Permit and Zoning Atlas Amendment application submitted and subsequently withdrawn. The project was delayed while NCDOT reviewed options for improvements to the NC 54/US 15-501/S. Columbia Street interchange.
2017	Revised Concept Plan submitted with 39 dwelling units, 7,150 square feet of retail and office space with parking for 68 vehicles proposed.
2018	Special Use Permit and Zoning Atlas Amendment application submitted (converted to a Conditional Zoning application in 2020)

SITE CONTEXT

Staff have identified the following physical and regulatory characteristics of the land which are relevant to consideration of a Zoning Atlas Amendment:

- The site is adjacent to an area zoned Neighborhood Commercial (NC), which encompasses the parking lot to the north of the site and the Merritt's restaurant property on the other side of S. Columbia Street.
- Other property surrounding the site is zoned under a residential district, either Residential-2 (R-2) or Residential-4 (R-4).
- The elevation of the site drops considerably from S. Columbia Street to a stream running through the site. The applicant materials for the associated Conditional Zoning note topographical conditions would make it challenging to develop single-family homes on the site, as allowed under current zoning and suggested by the 2020 Land Use Plan designation.
- The applicant describes the location as a gateway into town, based on the prominence of S. Columbia Street and proximity to UNC's campus.
- The applicant's Statement of Justification also notes that because of the site's frontage on an interchange, "the site is not conducive to being developed for the single family home use for which it is zoned."

PROPOSED ZONING

The applicant proposes applying the Mixed Use-Village-Conditional Zoning District (MU-V-CZD) district in order to construct a proposed mixed use project with ground floor commercial and upper-story residences, as noted in the Statement of Justification: "The site is zoned Residential-2 (R-2) which permits only single family residential and duplexes, with a maximum FAR of 0.093."

Recent changes to the Land Use Management Ordinance allow Mixed Use-Village (MU-V) to be approved as a Conditional Zoning District. The conditions of the rezoning would become part of the site's zoning entitlement upon approval.

The Mixed Use-Village (MU-V-CZD) Zoning District can be characterized by the following standards:

- PURPOSE: Section 3.5.1 of the Land Use Management Ordinance states that "The
 mixed use districts are intended to provide for the coordinated development of office,
 commercial, and residential uses and their necessary support functions in the vicinity of
 key highway intersections and transit corridors in Chapel Hill. They are designed to
 facilitate stated public policies to encourage design which emphasizes lively, people
 oriented environments and compatible, visually interesting development."
- **USES:** Permitted uses for Mixed Use (MU) districts, as established in LUMO Table 3.7-1, include (but are not limited to) various types of retail, services and businesses; office; hotel/motel; single-family and multifamily residential; research activities; and public facilities. For the Mixed Use-Village (MU-V) district specifically, LUMO 3.5.1(b) further establishes that multiple uses must be either mixed horizontally with 25 percent floor area minimums for residential and nonresidential uses, or mixed vertically. The Conditional Zoning application proposes limiting uses to commercial, office, and multifamily residential. The existing Residential (R-2) zoning generally limits uses to single-family or two-family residential; or selected other uses include place of worship and schools.

- between different road types for the site's frontage. The subject site fronts on arterial roads. Standards for Mixed Use-Village (MU-V) arterial include a maximum residential density of 20.0 units/acre, maximum building height of 70 (setback) to 114 (core) feet, minimum setbacks of 0 feet (except where a solar setback applies), and a maximum Floor Area Ratio of 1.2. The existing Residential-2 (R-2) zoning has substantially larger required setbacks, lower permitted heights, lower allowable density (4.0 units/acre), and lower allowable Floor Area Ratio (0.093). Based on the comparison of dimensional standards, the proposed rezoning would allow a significant increase in the amount of development potential for the site.
- **DESIGN AND DEVELOPMENT STANDARDS:** Most of the standards are established in LUMO Article 5 and are the same for both the Mixed Use-Village (MU-V) and Residential-2 (R-2) districts. Parking is handled differently, with the Mixed Use-Village (MU-V) district requiring only 50 percent of the minimum parking ratios applied in other districts. Buffers are also not required internal to the district. The Mixed Use-Village (MU-V) district establishes additional development standards for: phasing plan review, outparcel design criteria, and bus stop provision.
- The associated Conditional Zoning application provides an opportunity to establish a
 narrower range of uses for the site, along with tailored dimensional standards and
 development standards, to address potential impacts on surrounding properties in
 support of a finding that the permitted development would maintain public health, safety
 and welfare.

PROPOSED MODIFICATIONS TO REGULATIONS

- 1) Section 3.6.3 Resource Conservation District (RCD): The Land Use Management Ordinance (LUMO) limits uses, land disturbance, and impervious surface area to specific percentages of the total land area in different zones of the RCD. The applicant is requesting three modifications to the RCD standards:
 - **Section 3.6.3(e) Permitted uses and activities in resource conservation district:** The LUMO restricts uses within the stream side, managed use, and upland zones of the RCD. The applicant is requesting to place parking and building structure within the managed use and upland zones of the RCD.

Staff Comment: Staff believes that the Council could find a public purpose for the placement of the parking and building structure within the managed use and upland zones of the RCD. The site has limited buildable area outside the RCD as well as a large tree save west of the stream. The applicant is providing riparian buffer restoration and/or enhancement to improve the current poor stream conditions to mitigate impacts proposed within the RCD buffer.

Section 3.6.3(f) Dimensional regulations: The LUMO limits land disturbance and impervious surfaces to specific percentages of the total land area by the different zones of the RCD. The table below illustrates the LUMO standards and the applicant's requested land disturbance and impervious surfaces by the RCD zones.

RCD Zone	Total Area	Proposed Disturbance	LUMO	Proposed	LUMO
		Disturbance	standard	Impervious	standard

Streamside	35,935 sf	2,210 sf 6.2%	7,187 sf 20%	0 sf	3,594 sf (10%)
Managed	38,618 sf	23,100 sf 59.8%	15,447 sf 40%	5,380 sf 13.9%	7,724 sf 20%
Upland	34,668 sf	18,300 sf 52.8%	13,867 sf 40%	13,650 sf 39.4%	6,934 sf 20%

Staff Comment: Staff believes that the Council could find a public purpose for the increased land disturbance and impervious surfaces, as the project will minimize disturbance in the streamside zone, and the applicant has offered to provide riparian buffer restoration or enhancement to improve the current poor conditions of the stream and to mitigate impacts proposed within the RCD buffer, including areas of channel degradation to be restored.

2) Section 5.3.2 Steep slopes: The applicant is requesting a modification to LUMO Section 5.3.2 to exceed the maximum 25 percent disturbance of the areas with slopes exceeding 25 percent. The applicant is proposing to disturb 100 percent of the slopes greater than 25 percent. The area of slope to be disturbed in 9,541 sq. ft.

Staff Comment: Staff believes that the Council could find a public purpose for the steep slope disturbance as the development proposal is minimizing impacts in the RCD and preserving a significant portion of the site west of the stream.

3) Section 5.6 Landscaping, screening and buffering: The applicant is requesting a reduction of the width and plantings of the landscape buffer along South Columbia Street. The required buffer is a 30 foot Type 'D' buffer. The proposed buffer as will vary in width between zero and 30 feet with an average width of 12 feet. The modification also requests a 70 percent reduction in plant material from the required 23 canopy trees, 47 understory trees and 156 shrubs to 7 canopy trees, 15 understory trees, and 50 shrubs within the project site.

Staff Comment: Staff believes that the Council could find a public purpose for the increased reduced width and plantings as the application includes a pedestrian plaza at the street level to activate the street frontage.

4) Section 3.5.1 Mixed Use-Village: The applicant is requesting a modification to required use mix percentages in the Mixed Use-Village (MU-V) zoning district. The LUMO specifies that the Mixed Use-Village (MU-V) zoning district have a mix of a minimum of 25 percent of the floor area devoted to residential uses and a minimum 25 percent of the floor area devoted to office/commercial uses. The applicant is requesting a modification to the minimum non-residential component to allow as little as 6 percent non-residential uses.

Staff Comment: Staff believes that the Council could find a public purpose for the reduction in the use intensities as the development is designed as a live/work space.

Council Findings and Public Purpose: The Council has the ability to modify the regulations according to Section 4.4.5 of the Land Use Management Ordinance. Staff believes that the Council could modify the regulations if it makes a finding in this particular case that public purposes are satisfied to an equivalent or greater degree. If the Council chooses to deny a request for modifications to regulations, the developer's alternative is to revise the proposal to comply with the regulations.

For additional information on the proposed modifications and other considerations on timing and flexibility, please refer to the applicant's attached materials.

CONSISTENCY WITH THE COMPREHENSIVE PLAN AND OTHER DOCUMENTS

Town staff has reviewed this application for compliance with the themes from the 2020 Comprehensive Plan¹, the standards of the Land Use Management Ordinance², and the Town of Chapel Hill, NC: Design Manual and Standard Details³ and believes the Columbia Street Annex proposal complies with several themes of the 2020 Comprehensive Plan:

Comprehensive Plan Themes: The following are themes from the 2020 Comprehensive Plan, adopted June 25, 2012:

\boxtimes		Create a Place for Everyone	\boxtimes		Develop Good Places, New Spaces
\boxtimes	9	Support Community Prosperity	\boxtimes	No.	Nurture Our Community
\boxtimes		Facilitate Getting Around	\boxtimes	15 p	Grow Town and Gown Collaboration

Land Use Plan: The 2020 Land Use Plan⁴, a component of the 2020 Comprehensive Plan, designates this site for low density residential. The applicant has submitted an accompanying Land Use Plan Amendment application, requesting to designate the site as high residential.

Staff Evaluation: North Carolina General Statute Section 160A-383 requires the Council to approve a statement describing whether its action is consistent with an adopted comprehensive plan and any other applicable officially adopted plan when adopting or rejecting any zoning amendment.

Staff provides the following evaluation of this application's consistency with the 2020 Comprehensive Plan and other adopted plans:

- The intent of the Mixed Use-Village (MU-Village) district notes coordinated development
 of office, commercial, and residential uses and their necessary support functions, which
 is not consistent with the Low Residential designation on the 2020 Land Use Plan. The
 developer has submitted a concurrent Land Use Plan Amendment to address the
 inconsistency.
- Considering the Conditional Zoning application, there is a proposed residential focus for the project. This is somewhat more consistent with the Land Use Plan in terms of residential use, but is still inconsistent in intensity. The Low Residential designation supports a density of 1-4 units/acre. The Mixed Use-Village (MU-Village) arterial district allows up to 20 units/acre and the Conditional Zoning application proposes up to 13.3 units/acre.

¹ http://www.townofchapelhill.org/home/showdocument?id=15001

https://www.municode.com/library/#!/nc/chapel hill/codes/code of ordinances?nodeId=CO APXALAUSMA

³ http://www.townofchapelhill.org/town-hall/departments-services/public-works/engineering/design-manual-and-standard-details

⁴ http://www.townofchapelhill.org/home/showdocument?id=1215

- The applicant's Statement of Justification notes that the project design retains existing forest and is energy efficient in building design and orientation, furthering Comprehensive Plan goals for a green community.
- The Statement of Justification also notes that the site's proximity to the university campus would allow the proposed housing to serve students and staff, furthering Comprehensive Plan goals for town & gown collaboration.
- Finding #3 in the Findings of Fact section notes additional purposes of the 2020 Comprehensive Plan which may be achieved through this application.

FINDINGS OF FACT

In order to establish and maintain sound, stable, and desirable development within the planning jurisdiction of the Town, it is intended that the Land Use Management Ordinance (as stated in Section 4.4) shall not be amended except:

- A. To correct a manifest error in the chapter; or
- B. Because of changed or changing conditions in a particular area or in the jurisdiction generally; or
- C. To achieve the purposes of the Comprehensive Plan.

Staff provides below an evaluation of this application based on the three findings. Further information may be presented for the Council's consideration as part of the public hearing process. All information submitted at the public hearing will be included in the record of the hearing.

Finding #1: The proposed zoning amendment is necessary to correct a manifest error.

Arguments in Support	To date, no arguments in support have been submitted or identified by staff.
Arguments in Opposition	To date, no arguments in opposition have been submitted or identified by staff.

Staff Response: We believe, based on the information entered into the record to date, that there is no manifest error in the Town's Zoning Atlas Amendment related to the project site.

Finding #2: The proposed zoning amendment is necessary because of changed or changing conditions in a particular area or in the jurisdiction generally.

Arguments in Support	Staff notes that the site is located at the intersection of two arterial roads and at a prominent gateway into Chapel Hill that could be considered a place where more intense development activity is appropriate. Transportation infrastructure that has been added to Columbia St, including sidewalks, bike lanes, and bus routes, could support more intense use.
	Staff also notes that while the proposed intensity exhibits inconsistency with the Comprehensive Plan, this site is part of a gateway area identified for study through the Town of Chapel Hill's Future Land Use Map update that is currently underway. This effort may result in a revised land use designation and/or identify additional changing

	conditions for the area. The current draft of the Future Land Use Map proposes character types that include Multifamily, shops and offices.
Arguments in Opposition	To date, no arguments in opposition have been submitted or identified by staff.

Staff Response: We believe, based on the information entered into the record to date, that the Council could make the finding that the proposed zoning amendment is necessary because of changing conditions in the S Columbia Gateway Focus Area.

Finding #3: The proposed zoning amendment is necessary to achieve the purposes of the comprehensive plan.

Arguments in	The applicant's Statement of Justification states that the proposed
Support	development would contribute to the following elements of the Comprehensive Plan:
	 A range of housing options for current and future residents (Goal- Place for Everyone.3)
	Balance and sustain finances by increasing revenues and decreasing expenses (Goal-Community Prosperity and Engagement.1) Promote a sefe with part and connected (physical and parent)
	Promote a safe, vibrant, and connected (physical and person) community (Goal-Community Prosperity and Engagement.3)
	• A connected community that links neighborhoods, businesses, and schools through the provision of greenways, sidewalks, bike facilities, and public transportation (Goal-Getting Around.2)
	 Make an adaptable transportation system to support both dense and suburban development (Goal-Getting Around.4)
	 A community that has a parking system based on strategies that support the overall goals of a holistic transportation system (Goal- Getting Around.8)
	 Low density, green Rural Buffers that exclude urban development and minimize sprawl (Goal-Good Places, New Spaces.1)
	• A vibrant, diverse, pedestrian-friendly, and accessible downtown with opportunities for growing office, retail, residential, and cultural development and activity (Goal-Good Places, New Spaces.2)
	 A community that welcomes and supports change and creativity (Goal-Good Places, New Spaces.6)
	 Future land use, form, and density that strengthen the community, social equity, economic prosperity, and natural environment (Goal- Good Places, New Spaces.8)
	 Maintain and improve air quality and water quality, and manage stormwater to heal local waterways and conserve biological
	ecosystems within the town boundaries (Goal-Nurturing Our Community.2)
	 Protect, acquire, and maintain natural/undeveloped open spaces and historic sites in order to protect wildlife corridors, provide recreation,
	and ensure safe pedestrian and bicycle connections. These spaces could include, among other things, Significant Natural Heritage Areas (SNHA) lands adjacent to and connecting various properties such as
	riparian lands, etc. (Goal-Nurturing Our Community.3) Reduce the carbon footprint of all Town-owned or managed services
	and properties; require that all new development meets standards; and support residents in minimizing their personal footprints (Goal-Nurturing Our Community.7)

	 Protect neighborhoods from the impact of development such as stormwater runoff, light and noise pollution, and traffic (Goal-Nurturing Our Community.8) Promote access for all residents to health-care centers, public services, and active lifestyle opportunities (Goal-Town and Gown Collaboration.6)
Arguments in Opposition	To date, no arguments in opposition have been submitted or identified by staff.

Staff Response: We believe, based on the information entered into the record to date, that the Council could make the finding that the proposed zoning amendment is necessary to achieve the purposes of the Comprehensive Plan.



PROJECT FACT SHEET

Overview

	Site Description		
Project Name	Columbia Street Annex		
Address	1150 S. Columbia Street		
Property Description	Approximately 4 Acres at the intersection of South Columbia and NC-54		
Existing	Vacant/Wooded; Existing Monroe Street Right-of-Way		
Orange County Parcel Identifier Numbers	9788204502;9788205716;9788206500		
Zoning	Residential-2 (R-2)		
Proposed Zoning	Mixed Use - Village - Conditional Zoning District (MU-V-CZD)		

Topic	Comment			Status		
Use/Density (Sec 3.7)	Applicant is proposing 5 stories of residential and office space with parking below. Density is approximately 13 units per acre, based on gross land area			\odot		
Dimensional Standards		Requi	Required Proposed			
	Building Height, Setback	70 feet		70 feet		
	Building Height, Core	114 feet		84 feet		
(Sec. 3.8)	Street Setback (min. ft.)	0 feet		26 feet		
(<u>366. 3.0</u>)	Interior Setback (min. ft.)	0 feet		112 feet		
	Solar Setback (min. ft.)	20 fee	et	170 feet		
Floor area (Sec. 3.8)	Maximum 104,817 sq. ft. Proposed 61,000 sq. ft.		\odot			
Modifications to Regulations (Sec. 4.5.6)	 Modifications: RCD encroachment use, land disturbance, and impervious surface) Steep slope disturbance of more than 25% Percent of floor area of residential and office/retail space Landscape buffers 			М		
Inclusionary Zoning (Sec. 3.10)	15% affordable units, will provide 8 units			\odot		
Landscape	Landscape					
Buffer - North (Sec. 5.6.2)	Required: 20' Type C Buffer Proposed: 20' Type C Buffer			\odot		
Buffer - East	Required: 30' Type D Buffer			М		
(Sec. 5.6.2)	Proposed: Variable width and planting quantity buffer					
Buffer – South (Sec. 5.6.2)	Required: 30' Type D Buffer Proposed: 30' Type D Buffer			\otimes		
Buffer - West (Sec. 5.6.2)	Required: 20' Type C Buffer Proposed: Type C- Alternate- Tree Save			\bigcirc		
Tree Canopy	Required: Proposed:					
	requiredi		opesca	•		

(<u>Sec. 5.7</u>)	40%	45%		\odot
Landscape Standards (Sec. 5.9.6)	Proposed 5' landscape buffer strip between building, landscaped interior islands, and other design elements to meet LUMO Standards		FP	
Environment				
	RCD Zone	Land Disturbance	Impervious Surface	
Resource	Streamside	2,210 sf 6.2%	0	
Conservation District (Sec. 3.6)	Managed	23,100 sf 59.8%	5,380 sf 13.9%	М
	Upland	18,300 sf 52.8%	13,650 sf 39.4%	
Erosion Control (Sec. 5.3.1)	Orange County Erosic	n Control permit re	quired	\odot
Steep Slopes (Sec. 5.3.2)	Required: Disturb < 2 Proposed: Disturb 100		•	М
Stormwater Management (Sec. 5.4)	Stormwater control measures proposed, including underground detention, and will be and designed to meet or exceed LUMO 5.4 standards.			FP
Land Disturbance	71,635 sq. ft. disturbed (1.6 acres)			\odot
Impervious Surface (Sec. 3.8)	48,950 sq. ft. (27.6% of gross land area)			⊗
Solid Waste & Recycling	Town services requested. Current proposal compliant with Orange County Solid Waste			\odot
Jordan Riparian Buffer (<u>Sec. 5.18</u>)	2,210 sq. ft. of disturbance proposed (6.1% of Jordan Buffer) for grading related to the driveway			М
Access and Circ	ulation			
Road Improvements (Sec. 5.8)	Addition of left turn lane on South Columbia Street			\odot
Vehicular Access (Sec. 5.8)	Abandonment of existing Monroe street right-of-way. Single full-access driveway proposed, with northbound left turn lane into the site			\odot
Bicycle Improvements (Sec. 5.8)	Bicycle lane crossing striping along site frontage			②
Pedestrian Improvements (Sec. 5.8)	Installation of ADA ramps and pedestrian activated LED signal at the future South Columbia Street crosswalk			②
Traffic Impact Analysis (Sec. 5.9)	TIA completed, revised traffic counts to be required at ZCP stage.			②
Parking (Sec. 5.9)	Minimum: 65 Maximum: 111			Θ

	MU-V allow a 50% reduction from the minimum parking standards	
Transit (Sec. 5.8)	Project location adjacent to potential BRT stop	\odot
Bicycle Parking (Sec. 5.9)	Required: 19 Proposed: 19	\odot
Parking Lot Standards (Sec. 5.9)	To be constructed to Town Standards	FP
Technical		
Fire	Meets Town Standards	FP
Recreation Area (Sec. 5.5)	Required: 3,070 sq ft Proposed:3,070 sq ft with 25% as a payment-in-lieu	FP
Lighting Plan (Sec. 5.11)	Maximum of 0.3 foot-candles at property line	FP
Homeowners Association (Sec. 4.6)	Not proposed at this time	②
Schools Adequate Public Facility (Sec. 5.16)	Required at Final Plans	FP

Project Summary Legend

Symbol	Meaning
\bigcirc	Meets Requirements
М	Seeking Modification
С	Requires Council Endorsement
FP	Required at Final Plan;
NA	Not Applicable

TECHNICAL REPORT

12/9/2020



ZONING AMENDMENT APPLICATION ANALYSIS: COLUMBIA STREET ANNEX

The Technical Report provides a staff analysis of the Zoning Atlas Amendment application based on long-range planning considerations, identifying arguments in favor and arguments opposed. The application would effect a change to the current zoning and permitted types and intensities of land uses.

PROPERTY ADDRESS	APPLICANT
1150 S Columbia St	Wendi Ramsden, Coulter Jewell Thames PA
CURRENT ZONING DISTRICT Residential-2 (R-2)	PROPOSED ZONING DISTRICT Mixed Use-Village-Conditional Zoning District (MU-V-CZD)
2020 LAND USE PLAN DESIGNATION Low Residential, 1-4 unit/acre	2020 FUTURE FOCUS AREA None

OTHER APPLICABLE ADOPTED PLANS

None identified

SUMMARY OF ARGUMENTS IN FAVOR

- While the proposed zoning allows more intense development, the concurrent Conditional Zoning
 process provides an opportunity to limit intensity and to establish standards that address any
 impacts on surrounding properties
- Topographical challenges may make it difficult to develop the uses allowed under current zoning
- Frontage on an interchange may make it difficult to develop the uses allowed under current zoning
- Project design would further Comprehensive Plan goals for environmental stewardship
- Based on proximity to university, proposed housing would further Comprehensive Plan goals for town and gown collaboration
- The site is located at the intersection of two arterial roads which are considered a gateway
- Transportation infrastructure along Columbia St. could support more intense use

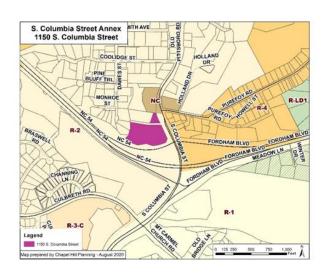
SUMMARY OF ARGUMENTS OPPOSED

- The proposed district allows significantly more intensity for the site compared to existing zoning
- A Land Use Plan Amendment is needed prior to consideration of a rezoning. The higher intensity
 allowed under the proposed district is inconsistent with the 2020 Land Use Plan designation for Low
 Density. The uses allowed under the proposed district are not entirely consistent with the 2020
 Land Use Plan designation (proposed residential is consistent, but not proposed commercial)





1150 S. Columbia Street – Columbia Street Annex Conditional Zoning



Town Council

December 9, 2020

1150 S. Columbia Street-Joint Presentation

- Continue and close the Public Hearing: Conditional Zoning and Land Use Plan Amendment for Columbia Street Annex
- Close the Public Hearing: Monroe Street Right of Way Closure
- Staff will jointly present the overall proposal

1150 S. Columbia Street and Montoe Right of Way Closure

- Continue the public hearing for Columbia Street Annex
- Receive comments for both Columbia Street Annex and the Monroe Street Right of way closure in this meeting and up to 24 hours after via email at:
 - mayorandcouncil@townofchapelhill.org
- Move to close both public hearings
- Enact the Ordinance A and adopt ROW closure resolution on January 13, 2021 (Two separate votes)

1150 S. Columbia Street- Conditional Zoning Process

Town
Evaluation of
Application
According to
Standards



Report
Presented to
Advisory
Boards and
Commissions



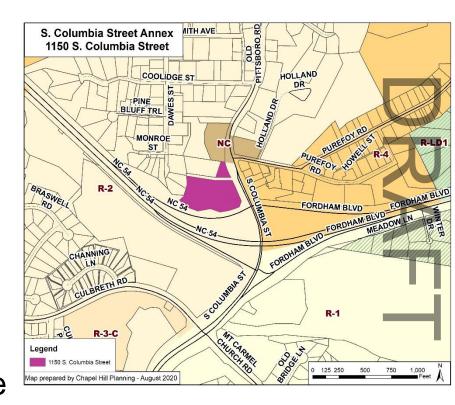
Open + Close
Public Hearing:
Report and
Recommendation
Presented to
Town Council



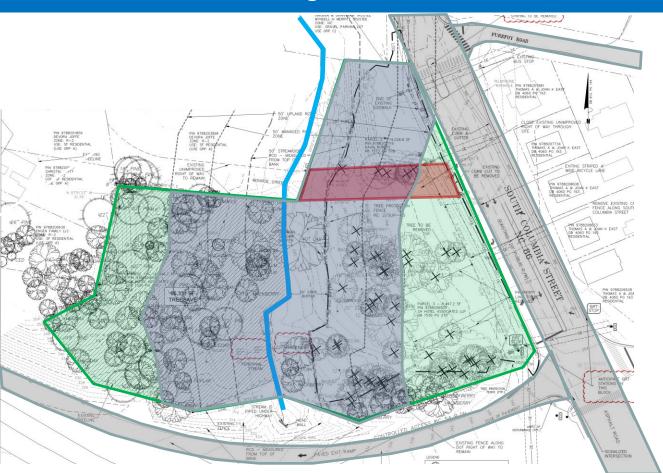


1150 S. Columbia Street- Project Summary

- 4 acre site
- Conditional Zoning
 - Currently R-2
 - Proposing MU-V-CZD
- Construct
 - 57,000 sq ft of residential
 - 4,000 sq ft of commercial
 - Max of 52 Units (8 affordable)
 - 6 stories set into steep slope
 - Close Monroe Street ROW

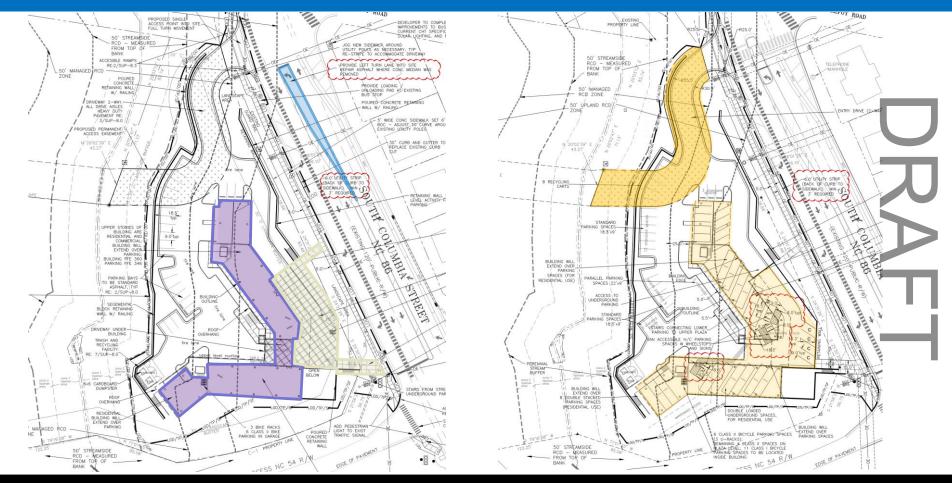


1150 S. Columbia Street – Existing Conditions

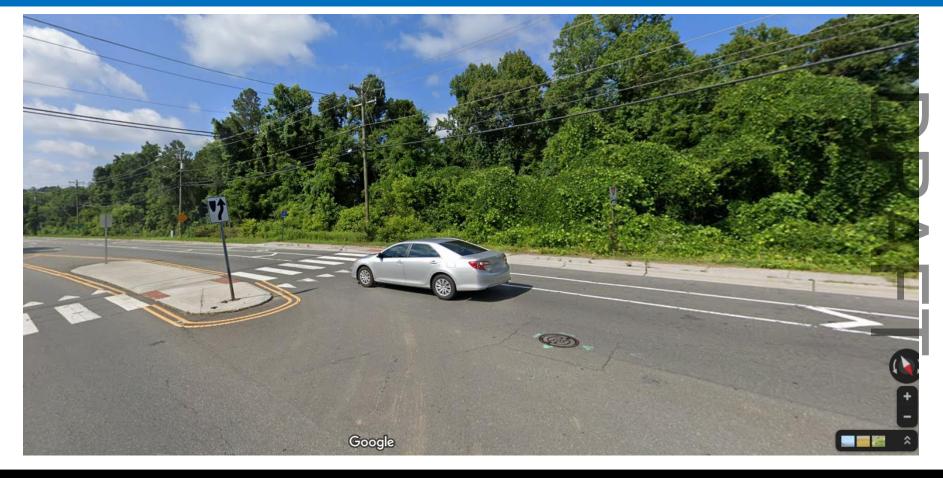


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1150 S. Columbia Street - Site Plan

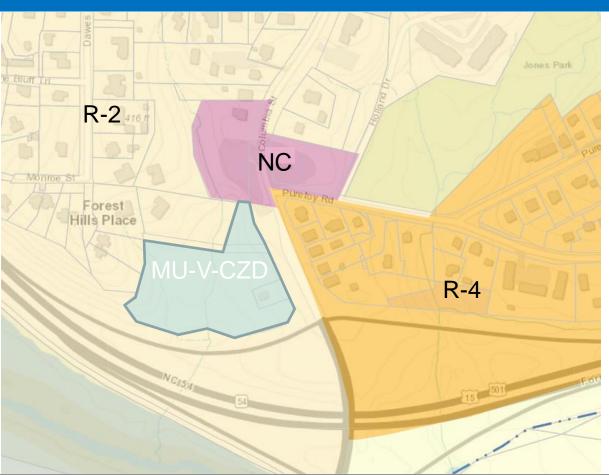


1150 S. Columbia Street – Site Plan



Town of Chapel Hill | 405 Martin Luther King Jr. Blvd. | www.townofchapelhill.org

1150 S. Columbia Street- Proposed Zoning



1150 S. Columbia Street- Proposed Zoning



Land Use Categories

- Rural Residential, 1 unit/5 ac.
- Rural Residential, 1 unit/2-5 ac.
- Low Residential, 1 unit/ac.
 - Low Residential, 1-4 units/ac.
- Medium Residential, 4-8 units/ac.
- High Residential, 8-15 units/ac.
- Commercial
- Mixed Use, Office/Commercial Emphasis
- Mixed Use, Office Emphasis
- Town/Village Center
- Institutional
- Office
- University
- Parks/Open Space
 - Landfill Activities

1150 S. Columbia Street- Proposed Conditions and Modifications

The applicant is requesting the following modifications to regulations:

- Steep slope disturbance regulations
- Encroachment into the Resource Conservation District
- Reduced landscaping standards
- MU-V land use percentage standards



1150 S. Columbia Street– Recommendations

Boards/Commissions	Recommendation
Community Design Commission	Approval as presented
Transportation and Connectivity Board	Denial with conditions
Housing Advisory Board	Approval with conditions
Environmental Stewardship	Approval with conditions
Planning Commission	Approval with conditions

1150 S. Columbia Street- Recommendation

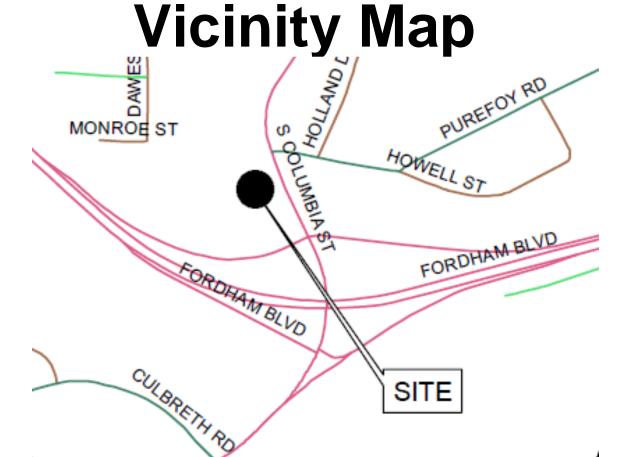
- Continue the public hearing
- Receive comments in this meeting and up to 24 hours after via email at: mayorandcouncil@townofchapelhill.org
- Move to close the public hearing at that point
- Enact the Ordinance on January 13, 2021



Right-Of-Way Closure -Unpaved and Unmaintained Portion of Monroe Street

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Chris Roberts, Manager of Engineering & Infrastructure

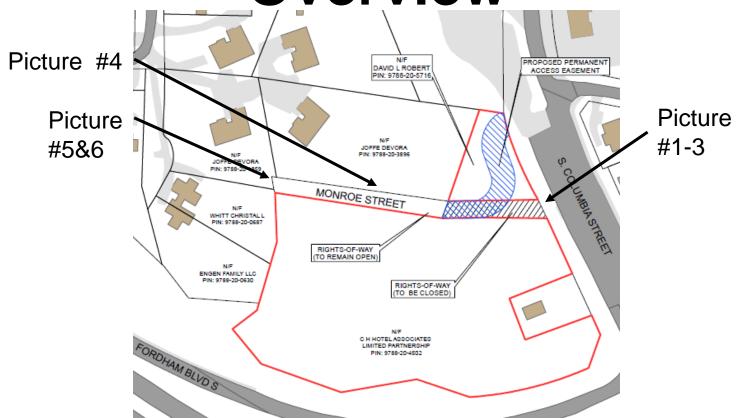


Where is this in the process?

Open the Close the Call for a Council Public Public **Public** Action Hearing Hearing Hearing January 13, September October 7, December 2021 9, 2020 2020 9, 2020

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Overview





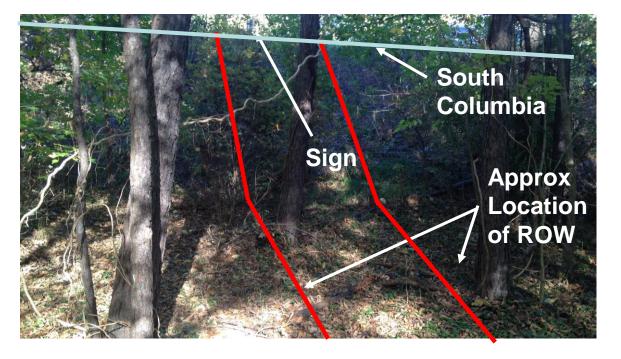
View South on South Columbia Street

Note the driveway curb-cut and the start of the un-used Monroe Street ROW



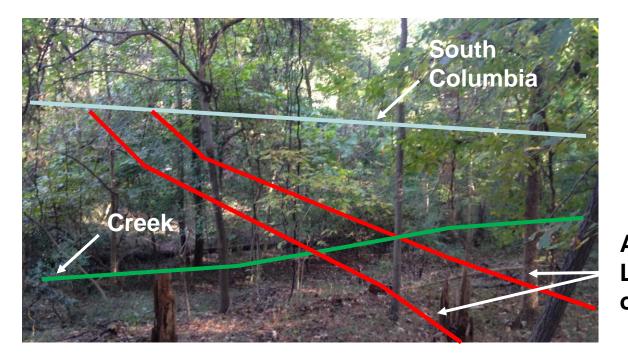
Monroe Street ROW Driveway Apron

Note the white sign in the next picture



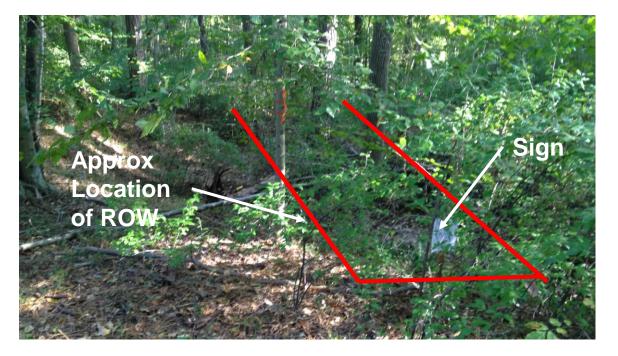
Monroe Street ROW Driveway Apron

Note the same sign on top of the hill and the steep slope of the ROW nest to South Columbia



Monroe Street ROW Approximate Mid-Point

Approx Location of ROW



Monroe Street ROW Opposite End

Note the white sign of surveyed end of Monroe Street ROW

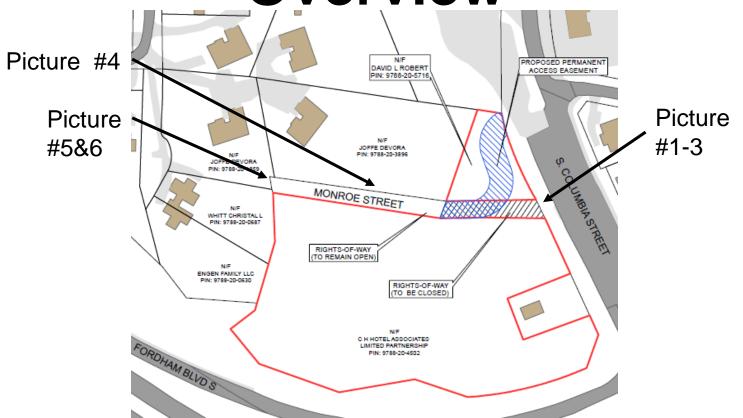


Monroe Street ROW Opposite End

Note the white sign of surveyed end of Monroe Street ROW

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Key Issues

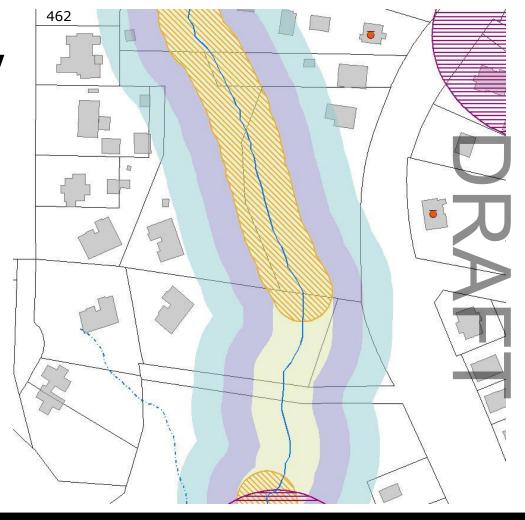
 The closure will isolate one property, which is currently accessed via a private driveway to the paved portion of Monroe Street.

 The Columbia Street Annex Developer has proposed a full public access easement to connect the isolated Monroe Street ROW which will continue to provide reasonable legal access to this parcel.

One adjacent property owner opposes this request.

Adjacent Property

- Impacts
- Future Development Opportunity
- RCD Encroachment
- Access



1150 S. Columbia Street and Montoe Right of Way Closure

- Continue the public hearing for Columbia Street Annex
- Receive comments for both Columbia Street Annex and the Monroe Street Right of way closure in this meeting and up to 24 hours after via email at: mayorandcouncil@townofchapelhill.org
- Move to close both public hearings
- Enact the Ordinance A and adopt ROW closure resolution on January 13, 2021 (Two separate votes)

RESOLUTION A

(Resolution of Consistency)

A RESOLUTION REGARDING THE APPLICATION FOR CONDITIONAL ZONING ATLAS AMENDMENT AT THE PROPERTY LOCATED AT 1150 SOUTH COLUMBIA STREET TO MIXED USE-VILLAGE-CONDITIONAL ZONING DISTRICT (MU-V-CZD) AND CONSISTENCY WITH THE COMPREHENSIVE PLAN (2021-XX-XX/R-X)

WHEREAS, Wendi Ramsden of Coulter Jewel Thames has filed an application for Conditional Zoning Atlas Amendment on behalf of both David L. Robert (Owner) and C. H. Hotel Associates, Limited Partnership (Owner and Contract Purchaser), to rezone 4.07 acres located at 1150 South Columbia on property identified as Orange County Property Identifier Numbers 9788-20-4502, 9788-20-5716, and 9788-20-6500, including a portion of Monroe Street Right of Way to be abandoned, to allow a mix of multifamily residential units and ground floor office/commercial space; and

WHEREAS, the Town staff have completed a review of the application for compliance with the Land Use Management Ordinance, Town Code, and for Consistency with the Comprehensive Plan; and

WHEREAS, the Planning Commission reviewed the application on October 20, 2020 and recommended that the Council enact the Zoning Atlas Amendment rezoning the property; and

WHEREAS, the Council of the Town of Chapel Hill has considered the application for Conditional Zoning Atlas Amendment on behalf of David L. Robert (Owner) and C. H. Hotel Associates, Limited Partnership (Owner and Contract Purchaser) to rezone and finds that the amendment if enacted, is reasonable and in the public's interest and is warranted to achieve the purposes of the Comprehensive Plan, as explained by, but not limited to, the following goals of the Comprehensive Plan:

- Diversified housing types (Goal A Place for Everyone.3)
- Business and Live/Work space (Community Prosperity and Engagement.1,.3)
- Accessible frontage, transit access, and underground car park (*Getting Around.2,.4,.8*)
- Minimize sprawl and preservation of natural areas (*Good Places, New Spaces.1,.2,.6,.8*)
- Natural area preservation and high quality Stormwater control measures (*Nurturing Our Community*.2,.3,.7,.8)
- Contribute to housing for UNC and UNC Health Care employees (*Goal Town and Gown Collaboration.4*)

NOW, THEREFORE, BE IT RESOLVED by the Council of the Town of Chapel Hill that the
Council hereby finds the proposed Conditional Zoning Atlas Amendment to be reasonable
and consistent with the Town Comprehensive Plan.

This	the	day	of		, 2021
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RESOLUTION B LAND USE PLAN AMENDMENT

(Amending the Land Use Plan from Low Residential to High Residential)

A RESOLUTION AMENDING THE LAND USE PLAN OF THE COMPREHENSIVE PLAN TO APPLY THE HIGH RESIDENTIAL LAND USE CATEGORY TO PROPERTY LOCATED AT 1150 S. COLUMBIA STREET (2021-XX-XX/R-X)

WHEREAS, the Land Use Plan, which is adopted as a part of the Comprehensive Plan and identifies the future land uses within the jurisdiction of the Town of Chapel Hill, may be amended at the discretion of the Council of the Town of Chapel Hill upon preparation of an amendment by the Planning Commission with assistance from the Town Manager; and

WHEREAS, on October 20, 2020 the Planning Commission recommended approval of an application to amend the Land Use Plan for 4.07 acres of property located at 1150 S. Columbia Street, identified as Orange County Property Identifier Numbers 9788-20-4502, 9788-20-5716, and 9788-20-6500, including a portion of Monroe Street Right-of-Way to be abandoned, with the land use category amended from Low Residential (1-4 units per acre) to High Residential (8-15 units per acre); and

WHEREAS, the Council of the Town of Chapel Hill has considered the application to amend the Land Use Plan and finds that the amendment, if enacted, is reasonable and in the public's interest and that it further meets the following criteria:

- Consistency with the goals and policies of Chapel Hill 2020, including Plans adopted as additions;
- Addressing significantly changed conditions since the last time the Land Use Plan was adopted and/or amended;
- Suitability of property for development in general conformance with adjacent land use and the existing surrounding development pattern or patterns as envisioned in adopted plans;
- Enhancing the public health, safety, and welfare of the town.

NOW, THEREFORE, BE IT RESOLVED by the Council of the Town of Chapel Hill that the
Council hereby amends the Land Use Plan of the Comprehensive Plan to apply the High
Residential land use category to property located at 1150 S. Columbia Street.

This	the	day	of	, 2021.

ORDINANCE A

(Approving the Conditional Zoning Application)

AN ORDINANCE AMENDING THE CHAPEL HILL ZONING ATLAS TO REZONE THE PROPERTY LOCATED AT 1150 SOUTH COLUMBIA STREET TO MIXED USE -VILLAGE-CONDITIONAL ZONING DISTRICT (MU-V-CZD) (PROJECT #18-039)

WHEREAS, the Council of the Town of Chapel Hill has considered the application for Conditional Zoning submitted by Wendi Ramsden, on behalf of both David L. Robert (Owner) and C. H. Hotel Associates, Limited Partnership (Owner and Contract Purchaser), to rezone 4.07 acres located at 1150 South Columbia on property identified as Orange County Property Identifier Numbers 9788-20-4502, 9788-20-5716, and 9788-20-6500, including a portion of Monroe Street Right-of-Way to be abandoned, to allow a mix of multifamily residential units and ground floor office/commercial space, and finds that the amendment if enacted, is reasonable and in the public's interest and is warranted to achieve the purposes of the Comprehensive Plan, as explained by, but not limited to, the following goals of the Comprehensive Plan:

- Diversified housing types (Goal A Place for Everyone.3)
- Business and Live/Work space (Community Prosperity and Engagement.1,.3)
- Accessible frontage, transit access, and underground car park (*Getting Around.2,.4,.8*)
- Minimize sprawl and preservation of natural areas (*Good Places, New Spaces.1,.2,.6,.8*)
- Natural area preservation and high quality Stormwater control measures (*Nurturing Our Community.2,.3,.7,.8*)
- Contribute to housing for UNC and UNC Health Care employees (*Goal Town and Gown Collaboration.4*)

WHEREAS, the application, if rezoned to Mixed Use–Village-Conditional Zoning District (MU-V-CZD) according to the rezoning plan dated April 26, 2018 and revised May 11, 2020, and the conditions listed below would:

- 1) Conform with the applicable provisions of the Land Use Management Ordinance and Town Code
- 2) Conform with the Comprehensive Plan
- 3) Be compatible with adjoining uses
- 4) Mitigate impacts on surrounding properties and the Town as a whole
- 5) Be harmonious with existing and proposed built systems including utility infrastructure, transportation facilities, police and fire coverage, and other public services and facilities
- 6) Be harmonious with natural systems such as hydrology, topography, and other environmental constraints

MODIFICATIONS TO REGULATIONS

WHEREAS, the Council of the Town of Chapel Hill finds, in this particular case, that the proposed development with the following requested modifications to regulations satisfies public purposes to an equivalent or greater degree:

1) Section 3.6.3 Resource Conservation District (RCD):

Section 3.6.3(e) Permitted uses and activities in resource conservation district: A Modification to Regulations is requested to allow a parking lot and building within the managed and upland zone of the resource conservation district which is not a permitted use within the resource conservation district according to Table 3.6.3-2.

Section 3.6.3(f) Dimensional regulations: Land disturbance is limited to specific percentages of the total land area in different zones of the RCD. The Upland and Managed Use zones are limited to 40 percent disturbance. The applicant is proposing the following land disturbance limits:

Existing Standards	Proposed Disturbed Area	Total land disturbed (SF)	Total Impervious Surface
Stream Side Zone	Less than 20%	2,210	0
Managed Use Zone	60%	23,100	5,380
Upland Use Zone	53%	18,300	13,650 sf (39%)

- 2) Section 5.6 Landscaping, screening and buffering: The developer is requesting a reduction of the width and plantings in the street side landscape buffer on South Columbia Street. The required buffer is 30 feet wide and planted to 'D' level opacity. The buffer as currently designed will vary in width between zero and 30 feet with an average width of 12 feet. The modification also requests a 70 percent reduction in plant material from the required 23 canopy trees, 47 understory trees and 156 shrubs to 7 canopy trees, 15 understory trees, and 50 shrubs within the project site.
- **3) Section 5.3.2 Steep slopes:** Section 5.3.2 specifies that the disturbance of steep slopes is to be minimized in order to protect water bodies from the effects of erosion on water quality, to protect plant and animal habitat, and to preserve the natural beauty and economic value of the Town's wooded hillsides. The applicant is requesting to disturb all steep slopes, equivalent to 9,541 SF, exceeding the stated disturbance limit of 25 percent of existing steep slopes on site.
- **4) Section 3.5.1 Mixed Use-Village:** Section 3.5.1 specifies that the Mixed Use-Village (MU-V) mixed use district have a mix of at least 25 percent floor area residential and a minimum 25 percent floor area in office/commercial uses. The developer is requesting a Modification to Regulation to a minimum of 6 percent of floor area for non-residential uses.

CONDITIONAL ZONING DISTRICT

WHEREAS, the Council of the Town of Chapel Hill that it finds, in this particular case, the proposed rezoning with the uses allowed in Mixed Use-Village (MU-V), subject to the conditions below, satisfies the purposes of Mixed Use-Village-Conditional Zoning District (MU-V-CZD).

NOW, THEREFORE, BE IT ORDAINED by the Council of the Town of Chapel Hill that the Chapel Hill Zoning Atlas be amended as follows:

SECTION I

The following Orange County parcels identified by Parcel Identifier Numbers (PINs) 9788-20-4502, 9788-20-5716, and 9788-20-6500, including 0.16 acres of the Monroe Street Right-of-way to be abandoned, shown on plans dated April 26, 2018 and revised May 11, 2020, shall be rezoned to Mixed Use-Village-Conditional Zoning District (MU-V-CZD):

LEGAL DESCRIPTION

(To be added)

SECTION II

BE IT FURTHER ORDAINED by the Council of the Town of Chapel Hill that the following conditions are hereby incorporated by reference:

- 1. <u>Expiration of Conditional Zoning Atlas Amendment</u>: An application for Zoning Compliance Permit must be filed by ______ (2 years from the date of this approval) or the land shall revert to its previous zoning designation. [LUMO 4.4.5(f)]
- 2. <u>Land Use Intensity</u>: This Conditional Zoning Atlas Amendment authorizes the following:

Land Use Intensity Mixed Use-Village				
Permitted Uses	 Business – General Business – Office-type Business - Convenience Dwelling Units, multifamily, over 7 dwelling units 			
Gross Land Area	177,090 sq. ft. (4.07 acres)			
Maximum Floor Area	61,000 sq. ft.			
Maximum Impervious Surface	48,950 sq. ft.			
Maximum Land Disturbance	71,635 sq. ft.			
Maximum Parking Spaces	106 spaces			
Minimum Parking Spaces	33 spaces (MU-V minimum is 50% of standard requirement)			
Minimum Bicycle Parking Spaces	19 spaces			

3. <u>Affordable Housing Plan/Performance Agreement</u>: Prior to the issuance of a Zoning Compliance Permit, the developer must submit an Affordable Housing Plan to be incorporated into an Affordable Housing Performance Agreement to be executed by the developer, the Town Manager (or designee), and the non-profit agency that will administer the affordable housing units that will contain the following information:

General information about the nature and scope of the covered development, including:

- a. 15% of the market rate units
- b. The developer will construct eight (8) affordable unit(s) on-site, four (4) of which will be available to households earning 65 percent of the AMI and four (4) of which will be available to households earning 80 percent of the AMI.
- c. The Plan will include information on:
 - i. The total number of market rate units and Affordable Dwelling Units in the development.
 - ii. The number of bedrooms and bathrooms in each Affordable Dwelling Unit.
 - iii. The approximate square footage of each Affordable Dwelling Unit.

- iv. The pricing for each Affordable Dwelling Unit. The pricing of each unit or lot shall be determined prior to issuing a Zoning Compliance Permit. At the time of sale, this price may be adjusted if there has been a change in the median income or a change in the formulas used in this ordinance.
- v. Documentation and plans regarding the exterior appearance, materials and finishes of the development for each of the Affordable Dwelling Units.
- d. Any and all other information that the Town Manager may require that is needed to demonstrate compliance with the Council's Affordable Housing Policies.

The Affordable Housing Plan and Performance Agreement shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.

- 4. Housing Plan: The developer shall provide the following:
 - e. 15 percent of the market rate homes (eight (8) proposed units) will be affordable town homes. Four of the units will be reserved for those making less than 65% of the median income and four will be made available to those making 80% of the median income.
 - f. Size of the affordable units will meet or exceed the minimum size requirements found in Table 3.10-2 of the Land Use Management Ordinance.
 - g. The affordable units to be placed within an organization ensuring their continued affordability for at least ninety-nine years.
 - h. The units be integrated into the development with preference to intersperse the affordable units throughout the development.
- 5. <u>Landscape Bufferyards</u>: The landscape bufferyards shall be provided as shown in the applicant's buffer plan sheet (SUP-7.0) and summarized in the following table:

Buffer Area	Buffer	Buffer Area	Buffer
South	Type D Buffers	North	Type C Buffer
East	Type D Modified Buffer	West	Type C Alternative Buffer

- 6. <u>NCDOT Improvements</u>: Prior to performing work in the NC Department of Transportation (NCDOT) Right-of-Way, the developer will need to obtain the following:
 - Approved NCDOT Driveway Permit for proposed access to South Columbia Street
 - Approved NCDOT Three-Party Encroachment Agreement with OWASA for any proposed water and sewer construction
 - Approved NCDOT Three-Party Encroachment Agreement with the Town of Chapel Hill for any proposed/stipulated sidewalk and appurtenances
- 7. <u>NCDOT Approval:</u> Prior to issuance of the above-mentioned permit and encroachments, the developer will need to submit complete and detailed plans per NCDOT requirements for approval.
- 8. <u>Pedestrian Improvements</u>: Prior to ZCP approval, the developer must show plans for a pedestrian-activated signal with Rectangular Rapid Flashing Beacons for the crosswalk north of Purefoy Road on S. Columbia Street. The plans shall include ADA ramps aligned to serve the crosswalk location. The developer shall also provide a pedestrian crosswalk north of the 54 Bypass intersection with S. Columbia Street.
- 9. <u>Stream Restoration</u>: Additional measures that demonstrate that this design goes above and beyond the requirements shall be required as a mitigating use for allowing modification to the RCD regulations. The developer shall provide riparian buffer restoration or enhancement to mitigate impacts proposed within the RCD buffer per the

- restoration or enhance requirement listed in Section 5.18.8 (e) (7) and Section 5.18.8 (e)(7)(F) of the Land Use Management Ordinance. Areas of channel degradation should be restored (degraded culverts in stream channel, fill placed in buffer, etc.).
- 10. Performance Guarantee: A performance and maintenance guarantee in an amount satisfiable to the Town Manager shall be provided to meet the requirement of Section 4.9.3 of the Land Use Management Ordinance prior to the approval of Construction plans. The performance quarantees and maintenance quarantees shall be satisfactory as to their form and manner of execution, and as to the sufficiency of their amount in securing the satisfactory construction, installation, or maintenance of the required stormwater control measure. The performance surety shall be an amount equal to one hundred and twenty-five percent (125%) of the total cost of uncompleted stormwater control measure(s) and conveyances prior to final plat recordation. The total cost of the storm water control measure(s) and conveyance(s) shall include the value of all materials, piping and other structures, seeding and soil stabilization, design and engineering, grading, excavation, fill, and other work. The developer shall submit unit cost information pertaining to all storm water control measure(s) and/or bids from the grading contractor hired to perform the work and any change orders related thereto as a method to determine the basis for cost of the work. The final cost determination shall be made by the Stormwater Management Division, taking into consideration any additional costs as deemed necessary for completion of the stormwater control measure(s) and conveyance(s).

Upon completion of the storm water control measures(s) and other improvements and acceptance by the Town after final site inspection, one hundred and twenty-five percent (125%) of the performance surety shall be released to the developer and a maintenance bond in an amount of twenty-five percent (25%) of the construction cost estimate shall submitted by the developer prior to the issuance of certificate of occupancy. No sooner than one year after the recording date of the deed(s), easements and maintenance agreement, the owner may request release of the remainder of the maintenance bond. Upon request by the owner, the Stormwater Management Division shall inspect the storm water control structure(s) to determine that the storm water measure(s) are performing as required by this Ordinance. The Stormwater Management Division, upon determining that the storm water control(s) are performing as required by this Ordinance, and after any repairs to the storm water control structure(s) are made by the owner, shall release the remaining maintenance bond. [LUMO 4.9.3]

TOWN OF CHAPEL HILL - CONDITIONAL ZONING STANDARD STIPULATIONS

Unless modified by the site-specific conditions noted above, these standards apply to all development permitted by a Conditional Zoning Atlas Amendment. The following standard stipulations are supplemental to site-specific conditions as set by Town Council.

<u>Access</u>

11. <u>Accessibility Requirements</u>: Prior to issuance of a Certificate of Occupancy, the developer shall provide the minimum required handicapped infrastructure according to the Americans with Disabilities Act and associated codes and standards.

<u>Transportation</u>

12. <u>Transportation Management Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall submit a Transportation Management Plan, subject to Town Manager

- approval. The Transportation Management Plan shall include monitoring of electric vehicle parking spaces usage. [LUMO 4.5.2]
- 13. <u>Bicycle Parking</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall provide dimensioned details that comply with the Town parking standards for required and/or proposed bicycle parking spaces. Bicycle parking spaces should be placed near building entrances. The spaces must comply with the Spring 2010 Association of Pedestrian and Bicycle Professionals Guidelines and the Class I and Class II bicycle parking standards required by the Town Design Manual. [LUMO 4.5.2]
- 14. <u>Parking Lot</u>: Any newly proposed parking lots, including additions to existing parking lots, shall be constructed to Town standards for dimensions and pavement design. [LUMO 5.9.5]
- 15. <u>Parking Lot Landscape and Screening:</u> The parking lot landscape design shall adhere to the standards of the Chapel Hill Land Use Management Ordinance. [LUMO 5.9.6]
- 16. <u>Lighting</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall design and install street lighting along the site frontage. Design and construction details must be approved by the Town Manager and the North Carolina Department of Transportation (NCDOT).
- 17. <u>Driveway Permit</u>: The developer must obtain an approved driveway permit and/or encroachment agreement(s) prior to beginning any proposed work within the NCDOT right-of-way. As a condition of the permit, the permittee shall be responsible for the design and construction of stipulated improvements in accordance with NCDOT requirements. An approved permit will be issued upon receipt of approved roadway and signal construction plans, inspection fees, and any necessary performance and indemnity bonds.
- 18. <u>Pavement Markings</u>: Any pavement markings proposed within the public street rights-ofway shall be long life thermoplastic. Pavement markers shall be installed if they previously existed on the roadways.
- 19. <u>Off-Site Construction Easements</u>: Prior to any development associated land disturbance on abutting properties, the developer shall provide documentation of approval from the affected property owner(s). [LUMO 5.8.1]
- 20. <u>Sight Distance Triangles</u>: Prior to issuance of a Certificate of Occupancy, the developer shall provide the Town of Chapel Hill with standard sight distance triangles at the proposed driveway locations. [Town Design Manual]
- 21. <u>Low Vision Design Features</u>: Any proposed pedestrian facilities should incorporate low vision design features as feasible. [LUMO 4.5.2]
- 22. <u>Repairs in Public Right-of-Way</u>: Prior to issuance of a Certificate of Occupancy, the developer shall repair all damage for work in the public right-of-way related to the construction of this project, which may include pavement milling and overlay. The design of such repairs must be reviewed and approved by the Town Manager and NCDOT prior to issuance of a Zoning Compliance Permit. [Town Code 17-40]

- 23. <u>Street Closure Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a street closure plan, subject to Town Manager and NCDOT approval, for any work requiring street, sidewalk, or lane closure(s). [Town Code 21-7.1]
- 24. <u>Work Zone Traffic Control Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a Work Zone Traffic Control Plan and a Construction Management Plan for approval by the Town Manager and NCDOT. The Work Zone Traffic Control Plan shall comply with the U.S. Department of Transportation Manual on Uniform Traffic Control Devices. The Construction Management Plan shall provide staging, construction worker parking, construction vehicle routes, and hours of construction. [Town Code 17-47]

Landscaping and Building Elevations

- 25. <u>Invasive Exotic Vegetation</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall identify on the planting plan any known invasive exotic species of vegetation, as defined by the Southeast Exotic Pest Plant Council (SE-EPPC), and provide notes indicating removal of these species from the landscape buffer areas prior to planting. [Town Design Manual]
- 26. <u>Alternate Buffer</u>: Prior to issuance of a Zoning Compliance Permit, approval shall be required from the Community Design Commission for any proposed alternate buffer. [LUMO 5.6.8]
- 27. <u>Landscape Protection</u>: Prior to issuance of a Zoning Compliance Permit, a detailed Landscape Protection Plan shall be approved. The plan shall include a complete and currently updated tree survey showing critical root zones of all rare and specimen trees and labeled according to size and species. The plan shall also indicate which trees will be removed and which will remain. The plan shall also include standard notes, fencing details, and location of fencing. [LUMO 5.7.3]
- 28. <u>Tree Protection Fencing:</u> Prior to issuance of a Zoning Compliance Permit, the developer shall provide a detail of a tree protection fence and a note on the Final Plans indicating that tree protection fencing will be installed prior to land-disturbing activity on the site. The plans shall include continuous tree protection fencing around construction limits and indicated construction parking and materials staging/storage areas, and Town standard landscaping protection notes, subject to Town Manager approval. [LUMO 5.7.3]
- 29. <u>Landscape Planting Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a detailed Landscape Planting Plan with a detailed planting list, subject to Town Manager approval. [LUMO 4.5.3]
- 30. <u>Tree Canopy</u>: A minimum of tree canopy coverage shall be provided through a combination of retained and replanted trees, unless a modification to regulations is approved. Calculations demonstrating compliance with Chapel Hill Land Use Management Ordinance Section 5.7.2 shall be included. [LUMO 5.7.2]
- 31. <u>Retaining Wall Construction</u>: If applicable, the final design and location of all retaining walls shall be approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
- 32. <u>Demolition Plan</u>: Prior to beginning any proposed demolition activity, the developer must obtain demolition permits from both the Planning and Inspections departments. While

the demolition component may be submitted to Planning in tandem with the Zoning Compliance Permit for new construction, a separate stand-alone demolition permit shall be issued prior to an Inspection's Demolition permit. Further, prior to the issuance of a demolition permit for all existing structures 500 square feet or larger, Orange County Solid Waste staff shall conduct a deconstruction assessment pursuant to the County's Regulated Recyclable Materials Ordinance (RRMO).

- 33. <u>Lighting Plan Approval</u>: Prior to issuance of a Zoning Compliance Permit, the Community Design Commission shall approve a lighting plan and shall take additional care during review to ensure that the proposed lighting plan will minimize upward light pollution and off-site spillage of light. [LUMO 8.5.5]
- 34. <u>Community Design Commission Approval</u>: The Community Design Commission shall review the building elevations, including the location and screening of all HVAC/Air Handling Units for the site, prior to issuance of a Zoning Compliance Permit. [LUMO 8.5.5] Within the Town's historic districts, the Historic District Commission will act in place of the Community Design Commission. [LUMO 8.4.6]

Environment

- 35. <u>Stormwater Management Plan</u>: That this project must comply with the Section 5.4 Stormwater Management of the Land Use Management Ordinance or whatever stormwater regulations are applicable at the time of issuance of Zoning Compliance Permit.
- 36. <u>Erosion Control Bond:</u> If one (1) acre or more is uncovered by land-disturbing activities for this project, then a performance guarantee in accordance with Section 5-97.1 Bonds of the Town Code of Ordinances shall be required prior to final authorization to begin land-disturbing activities. The financial guarantee is intended to cover the costs of restoration of failed or failing soil erosion and sedimentation controls, and/or to remedy damages resulting from land-disturbing activities, should the responsible party or parties fail to provide prompt and effective remedies acceptable to the Town. [Town Code 5-98]
- 37. <u>Silt Control</u>: The developer shall take appropriate measures to prevent and remove the deposit of wet or dry silt on adjacent roadways. [Town Code 5-86]
- 38. <u>Erosion Control</u>: The developer shall provide a copy of the approved erosion and sediment control permit from Orange County Erosion Control Division prior to receiving a Zoning Compliance Permit. During the construction phase, additional erosion and sediment controls may be required if the proposed measures do not contain the sediment. Sediment leaving the property is a violation of the Town's Erosion and Sediment Control Ordinance.
- 39. <u>Erosion Control Inspections</u>: In addition to the requirement during construction that an inspection occurs after every rainfall, the developer shall inspect the erosion and sediment control devices daily, make any necessary repairs or adjustments to the devices, and maintain inspection logs documenting the daily inspections and any necessary repairs. [Orange County Erosion Control]
- 40. <u>Stormwater Control Measure</u>: The proposed stormwater control measures for the site shall be designed to meet the current North Carolina Division of Environmental Quality Design Manual. The stormwater control measures must discharge outside of the 50 foot

- RCD Streamside Zone in a non-erosive manner.
- 41. <u>Curb Inlets</u>: The developer shall provide pre-cast curb inlet hoods and covers stating, "Dump No Waste! Drains to Jordan Lake", in accordance with the specifications of the Town Standard Detail SD-5A, for all new curb inlets for private, Town and State rights-of-way. [Town of Chapel Hill Design Manual]
- 42. <u>As-Built Plans</u>: That prior to the issuance of a Certificate of Occupancy, the developer shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces, and a tally of the constructed impervious area. The as-built plans should be in DXF binary format using State plane coordinates and NAVD 88.
- 43. <u>On-Site/Adjacent Stormwater Features</u>: The final plans must provide proper inlet protection for the stormwater drainage inlets on or adjacent to the site to ensure the stormwater drainage system will not be obstructed with construction debris.
- 44. <u>Repair/Replacement of Damaged Stormwater Infrastructure</u>: Existing stormwater infrastructure that is damaged as a result of the project demolition or construction must be repaired or replaced, as specified by the Stormwater Management Engineer, prior to requesting a Certificate of Occupancy.
- 45. <u>Jordan Riparian Buffer</u>: Any proposed impact in the Jordan Riparian Buffer require the submission of a Jordan Riparian Buffer Authorization application. The applicant shall demonstrate a NO PRACTICAL ALTERNATIVE for the proposed disturbance and meet the requirement in Section 5.18 of the Land Use Management Ordinance.
- 46. <u>Stream Restoration:</u> All stream crossings should be constructed perpendicular to the stream. No mechanical equipment should be brought into the streamside RCD buffer/Jordan Riparian Buffer without consulting with the Town stormwater staff beforehand.
- 47. <u>Homeowners' Association Responsibilities</u>: The Homeowners' Association shall be responsible for the maintenance, repair and operation of the Stormwater Control Measures, ditches and storm sewer conveyances located outside public right-of-way.
- 48. <u>P.E Certification</u>: The developer shall provide shall provide a certification, signed and sealed by North Carolina –licensed Professional Engineer, that the Stormwater Control Measure are constructed in accordance with the approved plans and specifications.
- 49. Energy Efficiency: Prior to issuance of a Zoning Compliance Permit, an energy efficiency plan shall incorporate a "20 percent more energy efficient" feature relative to the 90.1 energy efficiency standard of the American Society of Heating, Refrigeration, and Air Conditioning Engineers (ASHRAE), as amended and in effect at the time of Conditional Zoning approval. Comparable standards generally recognized as applicable to building energy consumption, as amended and in effect at the time of building permit issuance, may be used by the applicant when incorporating the "20 percent more energy efficient" feature into the final plans. An energy model should be used to demonstrate that the design will meet the aforementioned energy performance target. [Town Policy April 2007]
- 50. <u>Energy Management Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall submit an Energy Management Plan (EMP) for Town approval. The plan

shall: a) consider utilizing sustainable energy, currently defined as solar, wind, geothermal, biofuels, hydroelectric power; b) consider purchase of carbon offset credits and green power production through coordination with the NC GreenPower program; c) provide for 20 percent more efficiency that also ensures indoor air quality and adequate access to natural lighting, and allows for the proposed utilization of sustainable energy in the project; and (d) if requested, provide for the property owner to report to the Town of Chapel Hill the actual energy performance of the plan, as implemented, during the period ending one year after occupancy. [Town Policy April 2007]

Recreation

- 51. <u>Recreation Space (Multi-Family)</u>: A minimum of 25 percent of the required Recreation Space for the project shall be provided in the form of a payment in lieu. The payment in lieu shall be paid prior to the issuance of a Zoning Compliance Permit.
- 52. <u>Recreation Area (Subdivision)</u>: A minimum of 25 percent of the required Recreation Area for the project shall be provided in the form of a payment in lieu. The payment in lieu shall be paid prior to the issuance of a Zoning Compliance Permit.

Water, Sewer, and Other Utilities

- 53. <u>Utility/Lighting Plan Approval</u>: The final utility/lighting plan shall be approved by Orange Water and Sewer Authority (OWASA), Duke Energy Company, other applicable local utility service providers, and the Town Manager before issuance of a Zoning Compliance Permit. The developer shall be responsible for assuring that these utilities can continue to serve the development. In addition, detailed construction drawings shall be submitted to OWASA for review/approval prior to issuance of a Zoning Compliance Permit. [LUMO 4.5.3]
- 54. <u>Lighting Plan</u>: Prior to issuance of a Zoning Compliance Permit, the developer shall submit site plans, sealed by a Professional Engineer, for Town Manager approval, as well as other required documents to satisfy the lighting requirements of Section 5.11 of the Land Use Management Ordinance including: submission of a lighting plan; providing for adequate lighting on public sidewalks, including driveway crossings; and demonstrating compliance with Town standards. [LUMO 5.11]
- 55. <u>Relocation of Overhead Utilities Underground</u>: Prior to issuance of a Certificate of Occupancy, the developer will install underground all public utilities that are currently located overhead on the site except for 3 phase or greater electric lines. [LUMO 5.12.2]
- 56. <u>Water/Sewer Line Construction</u>: All public water and sewer plans shall be approved by and constructed according to OWASA standards. Where sewer lines are located beneath drive aisles and parking areas, construction methods approved by OWASA shall be employed to ensure that sewer lines will not be damaged by heavy service vehicles. [LUMO 5.12.1]
- 57. <u>OWASA Approval</u>: Prior to issuance of a Zoning Compliance Permit, any easement plats and documentation as required by OWASA and the Town Manager shall be recorded. [LUMO 5.12]
- 58. <u>Irrigation</u>: If permanent irrigation is proposed to support landscaping, an irrigation plan shall be submitted which includes the use of smart technologies to conserve water and energy.

Homeowner Association

- 1. <u>Homeowners' Association</u>: That a Homeowners' Association shall be created that has the capacity to place a lien on the property of a member who does not pay the annual charges for maintenance of all common areas, however designated. The Homeowners' Association documents shall be approved by the Town Manager prior to recordation at the Orange County Register of Deeds Office and shall be cross-referenced on the final plat. The Homeowners' Association documents shall comply with Section 4.6.7 of the Land Use Management Ordinance. That the Homeowners' Association covenants shall not exclude home occupation businesses as regulated by the Town of Chapel Hill.
- 2. <u>Homeowners' Association Responsibilities:</u> The Homeowners' Association shall be responsible for the maintenance, repair, and operation of required bufferyard(s), open space, recreation areas, paths, community garden, and shared stormwater management facilities.
- 3. <u>Dedication and Maintenance of Common Area to Homeowners' Association</u>: That the applicant provide for Town Manager review and approval, a deed conveying to the Homeowners' Association all common areas, however designated, including the community garden; recreation space; open space and common areas; the bufferyards; and stormwater management facilities. That the Homeowners' Association shall be responsible for the maintenance of the proposed internal subdivision roads until the NCDOT or the Town assumes ownership of the internal streets. These documents shall be reviewed and approved by the Town Manager prior to recordation at the Orange County Register of Deeds Office and cross-referenced on the final plat.
- 4. <u>Solar Collection Devices:</u> That the Homeowners' Association, or similar entity, shall not include covenants or other conditions of sale that restrict or prohibit the use, installation, or maintenance of solar collection devices, including clotheslines.

Fire Safety

- 59. <u>Fire Department Access</u>: All turns, radii, bridges, and depressions within roadways shall be designed and constructed to be accessible by the largest fire apparatus operated by the Town of Chapel Hill. Technical information on this equipment is available from the Towns Fire Marshal. 2018 NCFC Section 503, Appendix D.
- 60. <u>Fire Department Access/Construction</u>: Fencing around projects shall include access gates with a 20 foot swing or slide motion. Any areas which will be inaccessible for firefighting or rescue operations shall be noted. Emergency access designation for apparatus shall be provided. 2018 NCFC Section 503, Appendix D.
- 61. Obstruction of Fire Apparatus Access Roads: Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths and clearances established in Sections 503.2.1 and 503.2.2 shall be maintained at all times. 2018 NCFC 503.4
- 62. <u>Traffic Calming Devices</u>: Traffic calming devices shall be prohibited unless approved by the fire code official. 2018 NCFC 503.4.1

- 63. <u>Water Supply for Fire Protection</u>: When required. An approved water supply for fire protection, either temporary or permanent, shall be made available as soon as combustible material arrives on the site. 2018 NCFC 3312
- 64. <u>Fire Sprinklers</u>: The developer shall install sprinklers under the North Carolina Fire Protection Code (NC FPC) prior to issuance of a Certificate of Occupancy. Prior to issuance of a Zoning Compliance Permit, the plans shall show all proposed fire department connections to such systems. [TOWN CODE 7-56]
- 65. <u>Gates and Barricades</u>: Where required or authorized by the fire code official and permanent or temporary (construction), any gates across fire apparatus access roads shall be a minimum width of 20 feet, be of swinging or sliding type, have an emergency means of operation, shall be openable by either forcible entry or keyed, capable of being operated by one person, and shall be installed and maintained according to UL 325 and ASTM F 2200. [NC FPC 2018, 503.5, 503.6, D103.5]
- 66. <u>Grade and Approach</u>: Fire apparatus access roads shall not exceed 10 percent in grade unless approved by the fire chief, and all approach and departure angles shall be within the limits established based on the Fire Department's apparatus. [NC FPC -2018, 503.2.7, 503.2.8 and D103.2]
- 67. <u>Fire Protection and Utility Plan</u>: A fire flow report for hydrants within 500 feet of each building shall be provided and demonstrate the calculated gallons per minute with a residual pressure of 20 pounds per square inch. The calculations should be sealed by a professional engineer licensed in the State of North Carolina and accompanied by a water supply flow test conducted within one year of the submittal. Refer to the Town Design Manual for required gallons per minute.
- 68. <u>Fire Department Connections and Standpipes</u>: When the building being constructed requires standpipes, a temporary standpipe connection will be constructed with ready Fire Department Access when the building is not more than 40 feet in height. Such standpipes shall provide usable connections adjacent to the stairs and shall continue with building progression always being not more than one floor below the highest floor of the building. [NC FPC 912 & NC FPC 2018 3313]
- 69. <u>Fire Command Center</u>: Where required in the North Carolina Fire Protection Code and in all high rise buildings, a fire command center must be constructed in accordance with Section 508, NC FPC 2018.
- 70. <u>Aerials</u>: Where a building exceeds 30 feet in height OR 3 stories above the lowest level of Fire Department Access, overhead power and utility lines shall not be allowed within the aerial apparatus access roadway and the roadway shall have an unobstructed width of 26 feet exclusive of the shoulders. At least one of the apparatus access roadways shall be located within a minimum of 15 feet and maximum of 30 feet from one complete side of the building. [NC FPC 2018 D105.1, D105.2, D105.3, D105.4]
- 71. Fire Apparatus Access Road: Any fire apparatus access roads (any public/private street, parking lot access, fire lanes and access roadways) used for fire department access shall be all-weather and designed to carry the imposed load of fire apparatus weighing at least 80,000 lbs. Fire apparatus access roads shall have a minimum width of 20 feet exclusive of shoulders with an overhead clearance of at least 13 feet 6 inches for structures not exceeding 30 feet in height and shall provide access to within 150 feet of all exterior portions of the building. Structures exceeding 30 feet in height shall be

provided with an aerial apparatus access road 26 feet in width in the immediate vicinity of the building or portion thereof and shall provide at least one of the required access roads to be located not less than 15 feet and not more than 30 feet from the structure parallel to one entire side of the structure. [NC FPC 2018 502.1,503.1.1, 503.2.1, D102.1 SECOND ACCESS DEPENDENT UPON NORTH CAROLINA DEPARTMENT OF TRANSPORTATION DOT APPROVAL]

- 72. <u>Dead End Access Roads</u>: Dead end fire apparatus access roads exceeding 150 feet shall have a designated turn around. The turnaround shall meet one of the design standards of NC FPC 2018, Appendix D table D 103.4.
- 73. <u>Building Height</u>: Buildings exceeding 30 feet or three stories in height must have at least two means of fire apparatus access separated by at least one half the diagonal distance of the building. [NC FPC 2018, D104.1, D104.3 DEPENDENT UPON NORTH CAROLINA DEPARTMENT OF TRANSPORTATION DOT APPROVAL]
- 74. <u>Fire Access</u>: Prior to issuance of a Certificate of Occupancy, fire access shall be reviewed and approved by the Town of Chapel Hill.
- 75. <u>Fire Apparatus Access Road Authority</u>: The fire code official shall have the authority to increase the minimum access widths where they are deemed inadequate for fire and rescue operations. [NC FPC 2018 503.2.2]
- 76. <u>Hydrants Active</u>: The developer shall provide active fire hydrant coverage, acceptable to the Fire Department, for any areas where combustible construction materials will be stored or installed, prior to having such materials delivered to the site. All required fire hydrants must be installed, active, and accessible for the Fire Department use prior to the arrival of combustible materials on site. Fire protection systems shall be installed according to Town Ordinance, the NC Fire Protection Code, and National Fire Protection Association Standard #13. [NC Fire Protection Code 2018 Section 501.1 & 3312]
- 77. Fire Hydrant and FDC Locations: The Final Plans shall indicate the locations of existing and proposed fire hydrants and Fire Department Connections (FDC). Fire Department Connections shall be located on the street side of the building within 100 feet of a hydrant. Hydrant spacing shall comply with the Town Design Manual. Design shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit. [NC FPC 2018 Section 501.5.1.1]
- 78. <u>Firefighting Access During Construction</u>: Vehicle access for firefighting shall be provided to all construction or demolition sites including vehicle access to within 100 feet of temporary or permanent fire department connections and hydrants. Vehicle access shall be provided by either temporary or permanent roads capable of supporting vehicle loading under all weather conditions. [NC FPC 2018, Section 3310.1]
- 79. <u>Premise Identification</u>: Approved building address numbers, placed in a position acceptable to the fire code official, shall be required on all new buildings. [NC FPC 2018, 505.1]
- 80. <u>Key Boxes</u>: Where required by the fire code official, a secure key box, mounted on the address side of the building, near the main entrance, shall be provided to ensure adequate access to the building based on life safety and/or fire protection needs. [NC FPC 2018, 506]

- 81. <u>Automatic Fire Sprinkler System Required</u>: An automatic fire sprinkler system meeting the requirements of NFPA Standard #13 and Town Code 7-56 is required to be installed in non-residential construction.
- 82. <u>Fire Department Connections, Locations</u>: Any required FDCs for any buildings shall meet the design and installation requirements for the current, approved edition of NFPA 13, 13D, 13R, or 14 of the NC FPC 2018 and Town Code 7-38 for location. FDCs shall be installed within 100 feet of a hydrant or unless otherwise approved by the fire code official and shall not be obstructed or hindered by parking or landscaping. FDCs shall be equipped with National Standard Thread (NST) and be a 2.5" siamese.
- 83. <u>Fire Department Connections, Installation</u>: A working space of not less than 36 inches in width and depth and a working space of 78 inches in height shall be provided on all sides with the exception of wall mounted FDCs unless otherwise approved by the fire code official. The FDCs where required must be physically protected from impacts by an approved barrier. [NC FPC 2018, 912.1, 912.2 912.2.1, 312]
- 84. <u>Fire Department Connections, Standpipes:</u> When the building being constructed requires standpipes, a temporary standpipe connection will be constructed with ready Fire Department Access when the building is not more than 40' in height. Such standpipes shall provide usable connections adjacent to the stairs and shall continue with building progression always being not more than one floor below the highest floor of the building. NC FPC 2012 Section 1413.
- 85. <u>Fire Apparatus Access for Chapel Hill Fire Department</u>: All fire department access determinations shall be based upon Chapel Hill Fire Department apparatus specifications (data specifications provided by Office of the Fire Marshal/Life Safety Division) and field verification. All proposed fire department access designs shall be reviewed and shall also pass field inspection.
- 86. <u>Fire Flow Report</u>: The Final Plan application shall include a fire flow report sealed by an Engineer registered in the State of North Carolina. An OWASA flow test must be provided with the report. Fire flow shall meet the 20 psi or exceed the requirements set forth in the Town Design Manual. The Fire Flow Report shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit. [Town Design Manual]
- 87. <u>Fire Lane</u>: Prior to issuance of a Certificate of Occupancy, any fire lane shall be marked and signed in accordance with Town standards, with the associated plans approved by the Town Manager prior to issuance of a Zoning Compliance Permit. [NC FPC, Sections 2018 503.3, D103.6, D103.6.1, D103.2]
- 88. <u>Emergency Responder Radio Coverage in New Buildings</u>: All new buildings shall have approved radio coverage for emergency responders within the building based upon the existing coverage levels of the public safety communication systems of the jurisdiction at the exterior of the building. This section shall not require improvement of the existing public safety communication systems. [NC FPC 2018 Section 510.1]
- 89. <u>Construction/Demolition</u>: All Construction and demolition conducted shall be in compliance of the current edition of Chapter 14 of the NC FPC.

Solid Waste Management and Recycling

- 90. Solid Waste Management Plan: Prior to issuance of a Zoning Compliance Permit, a detailed Solid Waste Management Plan, including a recycling plan and a plan for managing and minimizing construction debris, shall be approved by the Town Manager and Orange County Solid Waste (OCSW). The plan shall include dimensioned, scaled details of any proposed refuse/recycling collection areas, associated screening, and protective bollards, if applicable. Each bulk waste container shall be labeled as to type of material to be collected. If a refuse compactor is proposed or if the collection enclosure is not accessible by Town vehicles, the developer shall provide documentation of an agreement for solid waste collection by a private provider prior to issuance of a Zoning Compliance Permit. [Orange County Solid Waste]
- 91. <u>Construction Waste</u>: Clean wood waste, scrap metal and corrugated cardboard (Regulated Recyclable Materials), all present in construction waste, must be recycled. All haulers of construction waste containing Regulated Recyclable Materials must be properly licensed with Orange County Solid Waste. The developer shall provide the name of the permitted waste disposal facility to which any land clearing or demolition waste will be delivered. [Orange County Solid Waste]
- 92. <u>Deconstruction Assessment</u>: For any existing structure 500 square feet or larger a deconstruction assessment shall be conducted by OCSW staff prior to the issuance of a demolition permit pursuant to the County's Regulated Recyclable Materials Ordinance (RRMO). Prior to any demolition or construction activity on the site, the developer shall hold a pre-demolition/pre-construction conference with Solid Waste staff. This may be held at the same pre-construction meeting held with other development/enforcement officials.

State and Federal Approvals

- 93. <u>State or Federal Approvals</u>: Any required State or federal permits or encroachment agreements (e.g., 401 water quality certification, 404 permit) shall be approved and copies of the approved permits and agreements be submitted to the Town of Chapel Hill prior to the issuance of a Zoning Compliance Permit. [NC State; Federal Permits]
- 94. North Carolina Department of Transportation Approvals: Prior to issuance of a Zoning Compliance Permit, plans for any improvements to State-maintained roads or in associated rights-of-way shall be approved by NCDOT. [NC Department of Transportation]

Miscellaneous

- 95. Construction Management Plan: A Construction Management Plan shall be approved by the Town Manager prior to issuance of a Zoning Compliance Permit. The construction management plan shall: 1) indicate how construction vehicle traffic will be managed, 2) identify parking areas for on-site construction workers including plans to prohibit parking in residential neighborhoods, 3) indicate construction staging and material storage areas, 4) identify construction trailers and other associated temporary construction management structures, and 5) indicate how the project construction will comply with the Town's Noise Ordinance. [Town Design Manual Chapter 10]
- 96. <u>Traffic and Pedestrian Control Plan</u>: The developer shall provide a Work Zone Traffic Control Plan for movement of motorized and non-motorized vehicles on any public street

that will be disrupted during construction. The plan must include a pedestrian management plan indicating how pedestrian movements will be safely maintained. The plan must be reviewed and approved by the Town Manager prior to the issuance of a Zoning Compliance Permit. At least 5 working days prior to any proposed lane or street closure the developer must apply to the Town Manager for a lane or street closure permit. [Town Code 17-42]

- 97. Construction Sign Required: The developer shall post a construction sign at the development site that lists the property owner's representative and telephone number, the contractor's representative and telephone number, and a telephone number for regulatory information at the time of issuance of a Building Permit, prior to the commencement of any land disturbing activities. The construction sign may have a maximum of 32 square feet of display area and maximum height of 8 feet. The sign shall be non-illuminated, and shall consist of light letters on a dark background. Prior to the issuance of a Zoning Compliance Permit, a detail of the sign shall be reviewed and approved by the Town Manager. [LUMO 5.14.4]
- 98. <u>Schools Adequate Public Facilities Ordinance</u>: If applicable, the developer shall provide the necessary Certificates of Adequacy of Public Schools or an exemption prior to issuance of a Zoning Compliance Permit. [LUMO 5.16]
- 99. <u>Open Burning</u>: The open burning of trees, limbs, stumps, and construction debris associated with site development is prohibited. [Town Code 7-7]
- 100. <u>Detailed Plans</u>: Prior to the issuance of a Zoning Compliance Permit, final detailed site plans, grading plans, utility/lighting plans, stormwater management plans (with hydrologic calculations), landscape plans, and landscape maintenance plans shall be approved by the Town Manager. Such plans shall conform to plans approved by this application and demonstrate compliance with all applicable regulations and the design standards of the Chapel Hill Land Use Management Ordinance and the Design Manual. [LUMO 4.5.3]
- 101. <u>Certificates of Occupancy</u>: No Certificates of Occupancy shall be issued until all required public improvements are complete. A note to this effect shall be placed on the final plats.
 - If the Town Manager approves a phasing plan, no Certificates of Occupancy shall be issued for a phase until all required public improvements for that phase are complete, and no Building Permits for any phase shall be issued until all public improvements required in previous phases are completed to a point adjacent to the new phase. A note to this effect shall be placed on the final plats.
- 102. <u>Traffic Signs</u>: The developer shall be responsible for placement and maintenance of temporary regulatory signs before issuance of any Certificates of Occupancy.
- 103. <u>New Street Names and Numbers</u>: The name of the development and its streets and house/building numbers shall be approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
- 104. <u>As-Built Plans</u>: Prior to the issuance of a Certificate of Occupancy, the developer shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces, and a tally of the constructed impervious area. The as-

- built plans should be in DXF binary format using State plane coordinates and NAVD 88. [Town Design Manual Chapter 10]
- 105. <u>Vested Right</u>: Approval of a Conditional Zoning District and the associated district-specific plan constitutes a site specific development plan establishing a vested right. During the period of vesting this permit may be subject to subsequent changes to Town regulations to the extent such regulations have been enacted under authority other than the Town's zoning authority.
- 106. <u>Continued Validity</u>: Continued validity and effectiveness of this approval shall be expressly conditioned on the continued compliance with the plans and conditions listed above.
- 107. Non-Severability: If any of the above conditions is held to be invalid, approval in its entirety shall be void.
- 108. <u>Not-Comprehensive</u>: The listing of these standard stipulations, and the specific stipulations applicable to this Permit, is not intended to be comprehensive and does not exclude other state and local laws and regulations which may be applicable to this Permit and development project.

BE IT FURTI	HER RESOLV	'ED that the C	ouncil hereby a	approves the a	pplication for
Conditional	Zoning for S	South Columbi	a Street Annex	at 1150 South	Columbia Street.
	3				
This the	day of	, 2020.			

RESOLUTION C

(Denying the Conditional Zoning Application)

A RESOLUTION DENYING AN AMENDMENT OF THE CHAPEL HILL ZONING ATLAS TO REZONE THE PROPERTY LOCATED AT 1150 SOUTH COLUMBIA STREET TO MIXED USE-VILLAGE-CONDITIONAL ZONING DISTRICT (MU-V-CZD) (2021-XX-XX/ R-X)

BE IT RESOLVED by the Council of the Town of Chapel Hill that it finds that a Conditional Zoning application, proposed by Wendi Ramsden of Coulter Jewel Thames on behalf of both David L. Robert (Owner) and C. H. Hotel Associates, Limited Partnership (Owner and Contract Purchaser), to rezone a 4.07 acres located at 1150 South Columbia on property identified as Orange County Property Identifier Numbers 9788-20-4502, 9788-20-5716, and 9788-20-6500, including a portion of Monroe Street Right-of-Way to be abandoned, if rezoned to Mixed Use–Village-Conditional Zoning District (MU-V CZD) according to the rezoning plan dated April 26, 2018 and revised May 11, 2020, with the conditions proposed would not:

- 1) Conform with the applicable provisions of the Land Use Management Ordinance and Town Code
- 2) Conform with the Comprehensive Plan
- 3) Be compatible with adjoining uses
- 4) Mitigate impacts on surrounding properties and the Town as a whole
- 5) Be harmonious with existing and proposed built systems including utility infrastructure, transportation facilities, police and fire coverage, and other public services and facilities
- Be harmonious with natural systems such as hydrology, topography, and other environmental constraints

BE IT FURTHER RESOLVED that the Council hereby denies the application for an amendment of the Chapel Hill Zoning Atlas to rezone the property located at 1150 South Columbia Street to Mixed Use – Village Conditional Zoning District (MU-V CZD).

|--|

PLANNING COMMISSION

The charge of the Planning Commission is to assist the Council in achieving the Town's Comprehensive Plan for orderly growth and development by analyzing, evaluating, and recommending responsible town policies, ordinances, and planning standards that manage land use and involving the community in long-range planning.

RECOMMENDATION FOR CONDITIONAL ZONING APPLICATION: COLUMBIA STREET ANNEX

October 21, 2020

Recommendati	on:	Approval ☑	Approval with Conditions \Box	Denial \square
		•	d Whit Rummel seconded a motion to and Use Plan Amendment.	recommend that
Vote:	9 – 0			
	Benc	•	r), Louie Rivers (Vice-Chair), James E t, Melissa McCullough, Whit Rumme	*
	Nays	::		
Recommendati	on:	Approval ☑	Approval with Conditions \Box	Denial □
Motion: Neal B Council approve			McCullough seconded a motion to rec	commend that the
Vote:	9 – 0			
Yeas: John Rees (Chair), Louie Rivers (Vice-Chair), James Baxter, Nea Bench, Michael Everhart, Melissa McCullough, Whit Rummel, Buffie W Stephen Whitlow				*
	Nays	::		
Recommendati	on:	Approval □	Approval with Conditions ☑	Denial □
-			elissa McCullough seconded a motion ith the following conditions:	to recommend

- Bicycle parking should be significantly increased on-site.
- If we want people to use transit they need to able to reach it as safely and efficiently as possible, then the existing pedestrian crosswalk by the bus stop should be retained- the

southern cross walk is the more direct route to current bus stop and likely location of future BRT stop.

• This is a terrible intersection - having both a southern and northern crossing signals to cars that they should slow down.

Vote: 9 - 0

Yeas: John Rees (Chair), Louie Rivers (Vice-Chair), James Baxter, Neal Bench, Michael Everhart, Melissa McCullough, Whit Rummel, Buffie Webber,

Stephen Whitlow

Nays:

Prepared by: John Rees, Chair

Michael Sudol, Planner II

COMMUNITY DESIGN COMMISSION

The charge of the Community Design Commission is to assist the Council in guiding the Town's vision on aesthetics, character, and function to focus community growth through advice, advocacy and implementation of the Council's policies and review of proposed development in key areas of the community.

RECOMMENDATION FOR CONDITIONAL ZONING PERMIT AT 1150 SOUTH COLUMBIA STREET

September 22, 2020

Recommend	ation: A	Approval ☑	Approval with Conditions \Box	Denial □
		oved and Megan Patr by the applicant.	naik seconded a motion to recommend	l project
Vote:	6 – 1			
	Yeas:	Susana Dancy Susan Lyons Ted Hoskins Megan Patnaik Polly van de Velde John Weis	Nays: Christine Berndt	

HOUSING ADVISORY BOARD September 8, 2020

The charge of the Housing Advisory Board is to assist the Chapel Hill Town Council in promoting and developing a full spectrum of housing opportunities that meet the needs of the Chapel Hill community.

RECOMMENDATION 1150 SOUTH COLUMBIA STREET DEVELOPMENT APPLICATION 1150 SOUTH COLUMBIA STREET

Recommenda	ation: Approval □ Approval with Conditions ■ Denial □
Columbia Stre with the condi affordable hou	otion was made by Anne Hoole, seconded by Letitia Davison, that the 1150 South set Development Application be recommended for approval by the Town Council, tion that the applicant reach out to the Community Home Trust to confirm that its using plan aligns with their current and projected housing needs. The motion nanimous vote.
Vote:	5-0
	Ayes: Dawna Jones (Chair), Mary Jean Seyda (Vice-Chair), Laura Cowen, Letitia Davison, Anne Hoole
	Nays:
Prepared by:	Dawna Jones, Housing Advisory Board Emily Holt, Staff

ENVIRONMENTAL STEWARDSHIP ADVISORY BOARD

The charge of the environmental stewardship advisory board will be to assist the Chapel Hill Town Council in strengthening environmentally responsible practices that protect, promote and nurture our community and the natural world through advice and program support.

RECOMMENDATION FOR CONDITIONAL ZONING FOR S COLUMBIA ST ANNEX LOCATED AT 1150 S COLUMBIA ST

September 14, 2020

Recommendation to Council: Approval \square Approval with Conditions \square Denial \square

Motion: Tom Henkel moved and Julie McClintock seconded a motion to recommend that, <u>if the project receives the exemption requested for encroachment into the RCD</u>, the Council approve the conditional zoning for South Columbia Street Annex, located at 1150 S Columbia St, with the following special considerations.

Vote: 7-0

Yeas: Adrienne Tucker (Chair), Maripat Metcalf (Vice-Chair), Bruce Sinclair,

Gregory Ames, John Wallace, Julie McClintock, Tom Henkel

Nays:

Special Considerations:

- Request that the Stormwater Utility Advisory Board make recommendations on the applicant's proposal for stream restoration, including more details about flora and fauna
- A traffic study with particular attention to carbon impacts from vehicle idling, as well as commuter traffic and congestion
- Because an all-electric building is proposed, strong consideration must be given to installing a rooftop solar PV system
- Comply with the Transportation and Connectivity Advisory Board's recommended guidance for EV stations

Prepared by: Adrienne Tucker, Chair, Environmental Stewardship Advisory Board

John Richardson, Community Resilience Officer, Staff Liaison to ESAB

TRANSPORTATION AND CONNECTIVITY ADVISORY BOARD

To assist the Chapel Hill Town Council in creating an inclusive connected community by recommending, advocating and planning for comprehensive, safe, effective and sustainable multi-modal transportation and connectivity

RECOMMENDATION S. Columbia Street Annex (Project #18-039) September 22, 2020

Recon	nmendation:	Approved		Appro	val with Con	ditions		Denied
Motio	develo	-	e subjec	ct property	site unless T			denial of future ake into
b) c) d) e) f)	50% reduction Dedicate trans No Right on F Install a cross Install a cross Coordinate with convenient act Provide impro-	sportation net Red movement walk north of walk south of th Town Staf cess for pedes oved lighting	work conts when S. the S. of the S. of on the strians. at the contract	ompany (Tompany)	strian signals a Street and NC Street and Pu- station location	are active 54 Byprefoy Ro	oass into	ersection.
	Vote	e: 7-0						
	Ayes		an, Da		sanne Kjemtr n, Rudy Julia	_		(Vice-Chair), geman and
	Nay	s:						

Prepared by: Jason Merrill, Chair, Transportation and Connectivity Advisory Board Jomar Pastorelle, Transportation Planner I

CONDITIONAL ZONING APPLICATION



TOWN OF CHAPEL HILL
Planning Department
405 Martin Luther King Jr. Blvd.
(919) 968-2728 fax (919) 969-2014
www.townofchapelhill.org

06.08.2020

Parcel Identifier Number (PIN): 9788206500/ -4502/ -5716 Date: 8-26-2020								
Section A: Project Inform	Section A: Project Information							
Project Name:	Columbia Street Annex							
Property Address:	1150 South Columbia Street	Zip Code:	27514					
Use Groups (A, B, and/or C):	A, C	Existing Zoning District:	R-2					
Project Description:	Multi-use 52-unit residential deve	lopment, underground parking, 4,000	sf general business space.					
Section B: Applicant, Owner, and/or Contract Purchaser Information								
Applicant Information	(to whom correspondence will I	pe mailed):						
Name: Coulter Jewell Th	ames PA, Attn: Wendi Ramsden							
Address: 111 West Main S	treet							
City: Durham	State:	NC Zip Co	de: 27701					
Phone: 919-682-0368 Email: wramsden@cjtpa.com								
The undersigned applicant hereby certifies that, to the best of their knowledge and belief, all information supplied with this application and accurate. Signature: Date: 8.27.2020								
Owner/Contract Purch	_							
○ Owner		ontract Purchaser						
Name: CN Hotel Associa	ites Limited Partnership (978820450	02/-6500						
Address: 3008 Anderson Drive, Suite 120								
City: Raleigh	State	NC Zip Co	de: 27609					
Phone: 919-821-4665	Email	: roland@whiteoakinc.com	15.					
The undersigned appli supplied with this app Signature:	fication and accurate.	Date:	all information $427/w$					

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CONDITIONAL ZONING APPLICATION



TOWN OF CHAPEL HILL Planning Department

405 Martin Luther King Jr. Blvd. (919) 968-2728 fax (919) 969-2014 www.townofchapelhill.org

Parcel Identifier Number (PIN): 9788206500/ -4502/ -5716	Da	ate: 8-26-2020				
Section A: Project Inform	ation		Well-ker of				
Project Name:	Columbia Street Annex						
Property Address:	1150 South Columbia Street	Zip Code:	27514				
Use Groups (A, B, and/or C):	oups (A, B, and/or C): A, C Existing Zoning Dist						
7	Multi-use 52-unit residential development,	— underground parking, 4,000 s	of general business space.				
Project Description:							
Section B: Applicant, Own	ner, and/or Contract Purchaser Inforn	nation					
150,50	(to whom correspondence will be mailed):					
Name: Coulter Jewell Th	ames PA, Attn: Wendi Ramsden						
Address: 111 West Main St	Sec. of Comment						
City: Durham	State: NC	Zip Cod	le: 27701				
Phone: 919-682-0368	Email: wramso	den@cjtpa.com					
Signature:	The undersigned applicant hereby certifies that, to the best of their knowledge and belief, all information supplied with this application and accurate. Signature: Date: 8 · 27 · 20 20 Owner/Contract Purchaser Information:						
Owner	Contract Pu	rchaser					
Name: CN Hotel Associa	tes Limited Partnership (9788205716)						
Address: 3008 Anderson D	rive, Suite 120						
City: Raleigh	State: NC	Zip Cod	de: 27609				
Phone: 919-821-4665	Email: roland@	@whiteoakinc.com					
The undersigned applic supplied with this appl Signature:	cant hereby certifies that, to the best of the ication and accurate. Click here for application submit	Date:	all information 8/27/20				

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06.08.2020

TOWN OF

CONDITIONAL ZONING

TOWN OF CHAPEL HILL Planning Department

Conditional Rezoning applications are reviewed by staff, Planning Commission, and Town Council. The application is part of an open public process that enables Town Council to discuss and decide on the key issues of a rezoning proposal. If a rezoning is approved, the applicant may then submit a detailed final plan application to staff for compliance review with the technical development standards and with the Council rezoning approval.

The establishment of a Conditional Zoning District shall be consistent with the Land Use Plan in the Comprehensive Plan. A proposed Conditional Zoning District is deemed consistent if the proposed District will be located in conformance with an adopted small area plan and/or in one of the following Land Use Categories:

- Medium Residential
- High Residential
- Commercial
- Mixed Use, Office/Commercial Emphasis
- Mixed Use, Office Emphasis
- Town/Village Center
- Institutional
- Office
- University
- Development Opportunity Area
- Light Industrial Opportunity Area

If the proposed conditional zoning districts is located in a Low Residential or a Rural Residential Land Use Category, the Town Council must approve a Land Use Plan amendment prior to proceeding.

SIGNED CONDITIONS: All conditions shall be in writing, prepared by the owner of the property or an attorney and must be signed by all property owners and contract purchasers, if applicable. The Town Attorney may require additional signatures if necessary and will determine whether or not the conditions statement is legally sufficient. Within thirty (30) days after receipt of the conditions the Planning Division Manager will notify the applicant of any deficiencies in the conditions statement or if any additional information is needed. The applicant may make changes to the written conditions statement provided it is submitted at least thirty (30) prior to Planning Commission meeting or thirty (30) days prior to Town Council public hearing.

RECORDATION OF CONDITIONS: After a rezoning has been approved by the Town Council, the conditions statement shall be recorded with the Register of Deeds Office. After a rezoning has been approved by Town Council and recorded by the Register of Deeds Office, the conditions may not be amended except through a new rezoning application.



PROJECT FACT SHEET TOWN OF CHAPEL HILL

Planning Department

Section A: Project Information							
Use Type: (check/list all that apply)							
Office/Institutional Residential	Mixed-Use	Other:					
Overlay District: (check all that apply)							
Historic District Neighborhoo	od Conservation Distric	ct Airport Hazaı	rd Zone				
Section B: Land Area							
Net Land Area (NLA): Area within zoning lot bo	undaries			NLA=	160,997	sq. ft.	
Choose one, or both, of a) Credited Street		ontage) x ½ width of p	ublic right-	CSA=		sq. ft.	
the following (a or h) not		al adjacent frontage) x	½ public or	COS=		sq. ft.	
TOTAL: NLA + CSA and/or COS = Gross Land Are	ea (not to exceed NLA	+ 10%)		GLA=	177,090	sq. ft.	
Special Protection Areas, Lan Special Protection Areas: (check all those to Discourse Conserting C	hat apply)	d Impervious Area	☐ Wate	rshed Pro	etection Dist	rict	
Land Disturbance					Total (sq. f	ft.)	
Area of Land Disturbance (Includes: Footprint of proposed activity plus work area envelope, staging area for materials, access/equipment paths, and all grading, including off-site clearing)							
Area of Land Disturbance within RCD							
Area of Land Disturbance within Jordan Buffer					2,210 sf		
Impervious Areas Existing (sq. ft.) Demolition (sq. ft.) Proposed (sq. ft.)						q. ft.)	
Impervious Surface Area (ISA) 676 676 48,950					48,950		
Impervious Surface Ratio: Percent Impervious 38% 27.64%					27.64%		
Surface Area of Gross Land Area (ISA/GLA)% If located in Watershed Protection District, % of impervious surface on 7/1/1993	.38%	.38%					
						_	



PROJECT FACT SHEET TOWN OF CHAPEL HILL

Planning Department

Section D: Dimensions

Dimensional Unit (sq. ft.)	Existing (sq. ft.)	Demolition (sq. ft.)	Proposed (sq. ft.)	Total (sq. ft.)
Number of Buildings	0	0	1(61,000 sf)	1(61,000 sf)
Number of Floors	0	0	6	6
Recreational Space	0	0	3,070 sf and/or PIL	3,070sf &/orPIL

Residential Space							
Dimensional Unit (sq. ft.)	Existing (sq. ft.) Demolition (sq. ft.)		Proposed (sq. ft.)	Total (sq. ft.)			
Floor Area (all floors – heated and unheated)	0	0	57,000 sf	57,000 sf			
Total Square Footage of All Units							
Total Square Footage of Affordable Units							
Total Residential Density							
Number of Dwelling Units	0	0	52	52			
Number of Affordable Dwelling Units	0	0	8	8			
Number of Single Bedroom Units							
Number of Two Bedroom Units							
Number of Three Bedroom Units							

Non-Residential Space (Gross Floor Area in Square Feet)						
Use Type	Existing	Proposed	Uses	Existing	Proposed	
Commercial	0	0				
Restaurant			# of Seats			
Government						
Institutional						
Medical						
Office	0	4,000 sf				
Hotel			# of Rooms			
Industrial						
Place of Worship			# of Seats			
Other						

Dimensional Requirements		Required by Ordinance	Existing	Proposed
Catharda	Street	0	n/a	26'
Setbacks (minimum)	Interior (neighboring property lines)	0	n/a	112'
(IIIIIIIIIIII)	Solar (northern property line)	20'	n/a	170'
Height	Primary	70'	n/a	70'
(maximum)	Secondary	114'	n/a	84.5'
Stupete	Frontages	80'	n/a	390'
Streets	Widths	62'	n/a	260'



PROJECT FACT SHEET TOWN OF CHAPEL HILL Planning Department

Section F: Ad	inining or	Connecting	Streets an	d Sidewalks
Jection I. Au	IJUIIIIIIIII UI	COILLECTILL	z Juiccus an	u jiucwaiks

Note: For approval of proposed street names, contact the Engineering Department.

Street Name	Right-of-Way Width	Pavement Width	Number of Lanes	Existing Sidewalk*	Existing Curb/Gutter
S Columbia Street	var	76'	5	Yes	⊠ Yes
NC 54 ramp	var	20' (on ramp)	1	☐ Yes	☐ Yes

List Proposed Points of Access ((Fx: Number, Street Name)	1

*If existing sidewalks do not exist and the applicant is adding sidewalks, please provide the following information:

Sidewalk Information				
Street Names	Dimensions	Surface	Handicapped Ramps	
S Columbia Street	5' wide	concrete	⊠ Yes □ No □ N/A	
			Yes No N/A	

Section G: Parking Information

Parking Spaces	Minimum	Maximum	Proposed
Regular Spaces	62	106	66
Handicap Spaces	3	5	3
Total Spaces	65	111	69
Loading Spaces	0		0
Bicycle Spaces	19	19 will meet LUMO requi	
Surface Type	Concrete, asphalt, pavers		

Section H: Landscape Buffers

Location (North, South, Street, Etc.)	Minimum Width	Proposed Width	Alternate Buffer	Modify Buffer
North	20'	20'	☐ Yes	☐ Yes
West	20'	20'	☐ Yes	☐ Yes
South	30'	30'	☐ Yes	☐ Yes
East (street)	30'	0'-30'	☐ Yes	

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PROJECT FACT SHEET TOWN OF CHAPEL HILL Planning Department

Section	l: Land	Use	Intensity
JCCLIOII	i. Laiic	1 036	IIICCIISICY

Existing Zoning District: Proposed Zoning Change (if any):

Section J: Utility Service

Telephone

Solid Waste

z	oning – Area – Ra	ea – Ratio Impervious Surface Thresholds		Minimum and Maximum Limitations			
Zoning District(s)	Floor Area Ratio (FAR)	Recreation Space Ratio (RSR)	Low Density Residential (0.24)	High Density Residential (0.50)	Non- Residential (0.70)	Maximum Floor Area (MFA) = FAR x GLA	Minimum Recreation Space (MSR) = RSR x GLA
MU-V Art'l	1.2					103,724	
streamside	.01					359	
managed	.019					734	
TOTAL						104,817 sf	3,070 sf
RCD Streamside	.01	0.01					
RCD Managed	.019	0.019					
RCD Upland	1.2						

Check all that apply:				
Water		☐ Individual Well	Community Well	Other
Sewer		☐ Individual Septic Tank	Community Package Plant	Other
Electrical	□ Underground	Above Ground		

Above Ground

Private

Underground

□ Town

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TOWN OF CHAPEL HILL Planning Department

The following must accompany your application. Failure to do so will result in your application being considered incomplete. For assistance with this application, please contact the Chapel Hill Planning Department (Planning) at (919) 968-2728 or at planning@townofchapelhill.org.

Х	Application fee (including Engineering Review fee) (refer to fee schedule) Amount Paid \$			
Х	Pre-application meeting –with appropriate staff			
Х	Digital Files – provide digital files of all plans and documents			
Х	Recorded Plat or Deed of Property			
X	Project Fact Sheet			
	Traffic Impact Statement – completed by Town's consultant (or exemption)			
Х	Description of Public Art Proposal, if applicable			
X	Statement of Justification			
Х	Response to Community Design Commission and Town Council Concept Plan comments, if applicable			
Х	Affordable Housing Proposal, if applicable			
Х	Statement of Consistency with Comprehensive Plan or request to amend Comprehensive Plan			
Х	Mailing list of owners of property within 1,000 feet perimeter of subject property (see GIS notification tool)			
Х	Mailing fee for above mailing list (mailing fee is double due to 2 mailings) Amount Paid \$			
Х	Written Narrative describing the proposal, including proposed land uses and proposed conditions			
Х	Resource Conservation District, Floodplain, & Jordan Buffers Determination – necessary for all submittals			
n.a	Jurisdictional Wetland Determination – if applicable			
Х	Resource Conservation District Encroachment Exemption or Variance (determined by Planning)			
n/a	Jordan Buffer Authorization Certificate or Mitigation Plan Approval (determined by Planning)			
X	Reduced Site Plan Set (reduced to 8.5" x 11")			

Stormwater Impact Statement (1 copy to be submitted)

- a) Written narrative describing existing & proposed conditions, anticipated stormwater impacts and management structures and strategies to mitigate impacts
- b) Description of land uses and area (in square footage)
- c) Existing and proposed impervious surface area in square feet for all subareas and project area
- d) Ground cover and uses information
- e) Soil information (classification, infiltration rates, depth to groundwater and bedrock)
- f) Time of concentration calculations and assumptions
- g) Topography (2-foot contours)
- h) Pertinent on-site and off-site drainage conditions
- i) Upstream and/or downstream volumes
- j) Discharges and velocities
- k) Backwater elevations and effects on existing drainage conveyance facilities
- I) Location of jurisdictional wetlands and regulatory FEMA Special Flood Hazard Areas
- m) Water quality volume calculations
- n) Drainage areas and sub-areas delineated
- o) Peak discharge calculations and rates (1, 2, and 25-year storms)
- p) Hydrographs for pre- & post-development without mitigation, post-development with mitigation
- q) Volume calculations and documentation of retention for 2-year storm

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TOWN OF CHAPEL HILL

Planning and Development Services

- r) 85% TSS removal for post-development stormwater runoff
- s) Nutrient loading calculations
- t) BMP sizing calculations
- u) Pipe sizing calculations and schedule (include HGL & EGL calculations and profiles)

Plan Sets (10 copies to be submitted no larger than 24" x 36")

Plans should be legible and clearly drawn. All plan set sheets should include the following:

- Project Name
- Legend
- Labels
- North Arrow (North oriented toward top of page)
- Property boundaries with bearing and distances
- Scale (Engineering), denoted graphically and numerically
- Setbacks
- Streams, RCD Boundary, Jordan Riparian Buffer Boundary, Floodplain, and Wetlands Boundary, where applicable
- Revision dates and professional seals and signatures, as applicable

Cover Sheet

a) Include Project Name, Project fact information, PIN, and Design Team

Area Map

- a) Project name, applicant, contact information, location, PIN, & legend
- b) Dedicated open space, parks, greenways
- c) Overlay Districts, if applicable
- d) Property lines, zoning district boundaries, land uses, project names of site and surrounding properties, significant buildings, corporate limit lines
- e) Existing roads (public & private), rights-of-way, sidewalks, driveways, vehicular parking areas, bicycle parking, handicapped parking, street names
- f) 1,000' notification boundary

Existing Conditions Plan

- a) Slopes, soils, environmental constraints, existing vegetation, and any existing land features
- b) Location of all existing structures and uses
- c) Existing property line and right-of-way lines
- d) Existing utilities & easements including location & sizes of water, sewer, electrical, & drainage lines
- e) Nearest fire hydrants
- f) Nearest bus shelters and transit facilities
- g) Existing topography at minimum 2-foot intervals and finished grade
- h) Natural drainage features & water bodies, floodways, floodplain, RCD, Jordan Buffers & Watershed boundaries



TOWN OF CHAPEL HILL

Planning and Development Services

Detailed Site Plan

- a) Existing and proposed building locations
- b) Description & analysis of adjacent land uses, roads, topography, soils, drainage patterns, environmental constraints, features, existing vegetation, vistas (on and off-site)
- c) Location, arrangement, & dimension of vehicular parking, width of aisles and bays, angle of parking, number of spaces, handicapped parking, bicycle parking. Typical pavement sections & surface type.
- d) Location of existing and proposed fire hydrants
- e) Location and dimension of all vehicle entrances, exits, and drives
- f) Dimensioned street cross-sections and rights-of-way widths
- g) Pavement and curb & gutter construction details
- h) Dimensioned sidewalk and tree lawn cross sections
- i) Proposed transit improvements including bus pull-off and/or bus shelter
- j) Required landscape buffers (or proposed alternate/modified buffers)
- k) Required recreation area/space (including written statement of recreation plans)
- I) Refuse collection facilities (existing and proposed) or shared dumpster agreement
- m) Construction parking, staging, storage area, and construction trailer location
- n) Sight distance triangles at intersections
- o) Proposed location of street lights and underground utility lines and/or conduit lines to be installed
- p) Easements
- q) Clearing and construction limits
- r) Traffic Calming Plan detailed construction designs of devices proposed & associated sign & marking plan

Stormwater Management Plan

- a) Topography (2-foot contours)
- b) Existing drainage conditions
- c) RCD and Jordan Riparian Buffer delineation and boundary (perennial & intermittent streams; note ephemeral streams on site)
- d) Proposed drainage and stormwater conditions
- e) Drainage conveyance system (piping)
- f) Roof drains
- g) Easements
- h) BMP plans, dimensions, details, and cross-sections
- i) Planting and stabilization plans and specifications

Landscape Protection Plan

- a) Rare, specimen, and significant tree survey within 50 feet of construction area
- b) Rare and specimen tree critical root zones
- c) Rare and specimen trees proposed to be removed
- d) Certified arborist tree evaluation, if applicable
- e) Significant tree stand survey
- f) Clearing limit line
- g) Proposed tree protection/silt fence location
- h) Pre-construction/demolition conference note
- i) Landscape protection supervisor note
- j) Existing and proposed tree canopy calculations, if applicable

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TOWN OF CHAPEL HILL Planning and Development Services

Planting Plan

- a) Dimensioned and labeled perimeter buffers
- b) Off-site buffer easement, if applicable
- c) Landscape buffer and parking lot planting plan (including planting strip between parking and building, entryway planting, and 35% shading requirement

Steep Slope Plan

- a) Classify and quantify slopes 0-10%, 10-15%, 15-25%, and 25% and greater
- b) Show and quantify areas of disturbance in each slope category
- c) Provide/show specialized site design and construction techniques

Grading and Erosion Control Plan

- a) Topography (2-foot contours)
- b) Limits of Disturbance
- c) Pertinent off-site drainage features
- d) Existing and proposed impervious surface tallies

Streetscape Plan, if applicable

- a) Public right-of-way existing conditions plan
- b) Streetscape demolition plan
- c) Streetscape proposed improvement plan
- d) Streetscape proposed utility plan and details
- e) Streetscape proposed pavement/sidewalk details
- f) Streetscape proposed furnishing details
- g) Streetscape proposed lighting detail

Solid Waste Plan

- a) Preliminary Solid Waste Management Plan
- b) Existing and proposed dumpster pads
- c) Proposed dumpster pad layout design
- d) Proposed heavy duty pavement locations and pavement construction detail
- e) Preliminary shared dumpster agreement, if applicable



TOWN OF CHAPEL HILL Planning and Development Services

Construction Management Plan

- a) Construction trailer location
- b) Location of construction personnel parking and construction equipment parking
- c) Location and size of staging and materials storage area
- d) Description of emergency vehicle access to and around project site during construction
- e) Delivery truck routes shown or noted on plan sheets

Energy Management Plan

- a) Description of how project will be 20% more energy efficient than ASHRAE standards
- b) Description of utilization of sustainable forms of energy (Solar, Wind, Hydroelectric, and Biofuels)
- c) Participation in NC GreenPower program
- d) Description of how project will ensure indoor air quality, adequate access to natural lighting, and allow for proposed utilization of sustainable energy
- e) Description of how project will maintain commitment to energy efficiency and reduced carbon footprint over time
- f) Description of how the project's Transportation Management Plan will support efforts to reduce energy consumption as it affects the community

Exterior Elevations

a) An outline of each elevation of the building, including the finished grade line along the foundation (height of building measured from mean natural grade)



MAIN OFFICE 111 WEST MAIN STREET DURHAM, NC 27701 p919.682.0368 f919.688.5646

Planning for the Future

May 11, 2020

Mr Jake Lowman Town of Chapel Hill Planning Department 405 Martin Luther King Jr Blvd Chapel Hill, NC 27514

RE: SPECIAL USE PERMIT APPLICATION – <u>PROJECT NARRATIVE - REVISED</u> COLUMBIA STREET ANNEX, 1150 South Columbia Street

Jake:

Enclosed is the resubmittal for the Special Use Permit application for the Columbia Street Annex multi-use project.

The project site is located at the northwest corner of South Columbia Street and the NC 54 westbound onramp at the south end of Chapel Hill. The site is currently wooded / vacant with a jurisdictional stream buffer running north to south approximately 200-300 feet west of the street right of way.

The proposed development will include 6-story buildings set into the steep drop between the Columbia Street roadway and the much lower stream area to the west, and connected by a pedestrian plaza at street level. The lowest level will be underground parking and walk up entries to some residential units facing the stream area. The main floor at street level will be residential and office/general business space. This level will sit above and overlook the stream area to the west, but will be at street level with plaza pedestrian access on the Columbia Street side. This space is intended to provide offices and shared commercial space for the residential tenants to create a live/work environment for the project. The top four levels will be residential units; mainly 1 bedroom, with a few 2 bedroom units.

Total square footage for the building will not exceed 61,000 square feet broken down as approximately 4,000 sf of general business space, and 57,000 sf for 52 residential units. There will be 69 parking spaces in an underground garage and surface spaces on the west side of the building, not visible from the street. Some surface parking located near Columbia Street right of way will sit 16' below street level but will be separated from the street view by a retaining wall and plant screening. Service and trash will be located at the south end of the project and not visible from the road. The vehicular layout allows service and emergency vehicle access. Bicycle parking will be accommodated in the residential units, in the

underground garage, and a few short term spaces located in the street level plaza accessible from Columbia Street.

Site vehicular access will be located at the northernmost part of the site, across from Purefoy Road to remove it as much as possible from the 15/501/54 / Columbia Street intersection. This configuration will allow full turn circulation as requested by NC DOT. A new sidewalk would be built in the right of way to connect to existing sidewalk from the north and continuing down to the NC54 ramp intersection. There is a requirement for 40% tree coverage on site that will be satisfied entirely with existing trees on the west side of the stream buffer. This project will include only minimal land disturbance west of the stream where an existing culvert will be removed at the request of the Town's stormwater group. Landscape buffers will be planted in disturbed areas on the northern, northwestern and eastern property lines, and a modified buffer will be provided on the street frontage.

The applicant is requesting a landscape buffer modification on South Columbia Street. The required buffer would be a 30' wide buffer planted to 'D' level opacity. But we are requesting a buffer ranging in width from 0 to 30' and with 30% of the total required plantings on the project property. This frontage will be opening to a pedestrian plaza to allow this awkward site to be accessible by pedestrians from South Columbia Street. The Owner would like to also plant some street trees in the DOT right of way and will work with DOT toward this condition. Those would be additional to the 30% plantings. The reduction will provide some buffering between the street and the building and shade for pedestrians along that sidewalk, while still providing view of the building and extensive access to the plaza area.

The applicant is requesting a reduction in parking to a minimum of 80% of the minimum requirement. The unit mix and quantity within the building have not been finalized by the owner. Project minimum parking is 65 spaces, and if the unit count is kept to 52 and the non-residential space to the low end, the 69 spaces that can be provided on site will satisfy the requirement. But the project is accessible to multiple alternate forms of transit, and if at the time of final design the market does not demand as much parking and the building program is at the higher end of the approved range, the total parking count may fall below the current LUMO requirement. The 69 spaces shown on the current plan are expected to satisfy the parking demand at this location. There are 28 double stacked spaces in the underground garage, suiting parking needs for 14 of the largest residential units and leaving 41 spaces to satisfy the demands of the other units and the non-residential space. With the existing sidewalk system in place, the bike lanes, public transit service, and the location of this project close to campus and downtown Chapel Hill, it is expected that 80% of the parking requirement will fulfil the user demands and encourage alternative forms of transportation. The owner is requesting a modification to allow a reduction to 80% of the required spaces as the minimum standard, though would have the right to provide as many as 69 spaces total.

Stormwater management will be handled through a BMP located under the surface parking west of the building. Stormwater management will meet State regulations. The developer is arguing that during the early application review in 2014 the stream was determined to be intermittent and therefore needs only a 50' RCD and buffer area. This is the information shown on the plans. The site is steeply sloped and is not in a flood plain. The 50' stream buffer will be protected and the majority of all construction and disturbance will remain outside of that zone. The stream itself does not always have water in it, and is

only 1-2 feet wide, and any visible banks are less than a foot above the bottom of the channel. The new project will have a retaining wall as the divider between the buffer and the parking lot. The development sits a minimum of 11 feet higher than the bottom of the stream. A modification to allow development in the managed and upland RCD zones, and a modification to disturb all steep slopes east of the stream area also being requested. This modification also includes a request to disturb more than stated ordinance amount, and to increase the impervious more than the stated ordinance amount. The project will be in conformance with the disturbance and impervious limits in the streamside zone. In the managed zone the disturbance will be approximately 60% - the ordinance restricts disturbance in this area to 40%. The project will be in conformance with the impervious limits in the managed zone.

In the upland zone, the disturbance will be approximately 53% while the ordinance restricts it to 40%. And the final impervious will be approximately 40% - double the ordinance restriction of 20%. A separate RCD Exemption Application has been submitted.

The final modification being requested is for a us mix percentage well below the LUMO mandated 25% per use. This is to reduce the parking needs while still providing general business space to encourage a live-work opportunity.

Trash and recycling will be handled in a dumpster facility at the bottom on the entry drive, accessible for Town and County pickup. Both garbage truck and fire truck access will meet Town standards.

A traffic impact study has been completed for this project, which fronts two NC DOT streets and no Town streets. The Owner has spoken with Chuck Edwards at DOT and will work with DOT to provide the turn lanes and safety measures required by DOT. The Town has requested an updated traffic impact study which the Owner is proposing to do during the ZCP phase of approvals when the counts and flows would be more accurate to the final project.

A rezoning application to change this site from R-2 to MU-V is being submitted concurrently with this SUP application to allow for a development with more building height and density.

The project has gone through the Town's Concept review process. It was presented to the Community Design Commission in October 2007 and there is a separate document responding to the board members' comments. The project also was reviewed by Town Council in February 2008, and a separate document addresses and responds to their concerns.

Sincerely,

Coulter Jewell Thames, PA

Wendi Ramsden RLA

Phil Szostak - Architect, Szostak Design CC.

Roland Gammon - Owner, CH Properties



MAIN OFFICE 111 WEST MAIN STREET DURHAM, NC 27701 p919.682.0368 f919.688.5646

Planning for the Future

COLUMBIA STREET ANNEX

1150 South Columbia Street

SPECIAL USE PERMIT APPLICATION - STATEMENT OF JUSTIFICATION

26 April 2018

REVISED 11 May 2020

The project site is located at the northwest corner of South Columbia Street and the NC54 westbound entry ramp. The proposed use will be a mix of 52 residential units and some office/general business space in a 61,000 sf building. Stormwater control and tree coverage will meet Town standards. We will be asking for 5 modifications: 1- Reduction of the width and plantings in the landscape buffer on South Columbia Street 2- Potential reduction of parking from the requirement 3- Permission to build in the managed use and upland RCD zones and to exceed ordinance disturbance and impervious limits, 4-Permission to disturb steep slopes, and 5-modification to the required use mix percentages.

We believe the project satisfies all the required findings as stated in section 4.5.2 of the Town's Land Use Management Ordinance. These findings and our responses to how we address each finding are submitted as follows:

1. The use is located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare.

The project will be designed with open public space on the South Columbia frontage and more private space for residents on the west facing part of the building and site. Parking and plaza areas will be well lighted to meet the Town's lighting codes. Public sidewalk and handicap accessibility will be provided. The new building will comply with all current building and safety codes.

A traffic impact study has been completed for this project, and the owner will work with Town Transportation and NC DOT to update counts as relevant during the final plan application phase. The proposed access to the site has been located at the north end of the whole parcel, across from Purefoy Road and as far from the highway overpass as possible. The drive would be located 420 feet away from

the 15/501/54 / Columbia intersection. There is no other possible frontage for vehicular access to or from the site. The south end of the property fronts the NC 54 entry ramp. The Monroe Street right of way is not improved, topographically doesn't meet the South Columbia Street pavement elevation, and to the west crosses a jurisdictional stream buffer into a quiet residential street. The closing of this right of way will be taken to Council prior to the final SUP and Rezoning presentations. When the right of way closing is recorded, an access easement will be recorded at the same time with boundaries to align with the project's proposed driveway and the remaining Monroe Street right of way at the site's western property line.

The project will have minimal impact on utility demands. The site is already serviced with water, sanitary sewer, power, and natural gas. There will be no need for public upgrade of utilities to service this site, though the project will be installing a pump force main to connect sanitary sewer service to an existing manhole in S Columbia Street at Purefoy. The project will add a fire hydrant on South Columbia Street which will directly serve the new facility but which will also be an appropriate additional streetside and publicly accessible hydrant for the area.

The use complies with all required regulations and standards of the LUMO including all
applicable provisions of articles 3 and 5, the applicable specific standards contained in the
supplemental use regulations (article 6) and with all other applicable regulations.

We are requesting a concurrent zoning amendment to allow for more built square footage and a taller building than would be allowed in the existing zoning. All dimensional, design, and development standards are in conformance with applicable LUMO standards for the proposed zone except for a request for five modifications outlined above: streetside landscape buffer, potential parking reduction, building, disturbance, and impervious surfaces in the managed and upland RCD zones, disturbance of steep slopes, and a reduced non-residential use percentage.

3. The use is located, designed and proposed to be operated so as to maintain or enhance the value of contiguous property, or the use is a public necessity.

It is expected that the new project will maintain or enhance the value of contiguous property. The site is currently forested but is quite overgrown at the street edges. The streets have been built up over time in order to allow enough elevation for an overpass at the NC 54 intersection. Because of this, the property sits as much as 25' below street level. It would be difficult to fill the site to make it usable for single family homes as there is a jurisdictional stream buffer 200-300' inside the site which runs the entire length north to south. The proposed design will create a building which takes advantage of the elevation changes by putting underground parking on the lowest level (away from street view), and creating a pedestrian connection at street level to a plaza area and to the buildings. Because the parking and lowest floor will sit below the level of S Columbia Street, the structure will appear less than 6 stories. The upper floors and the space facing west toward the stream and lower slopes would be used for residential purposes. In this way the building itself acts as beacon at the gateway into Town, and provides a transition between the busy arterial street on the east facing side, and the residential

neighborhood to the west. Additionally because the space is constructed in a 6 story building, an economically efficient density can be reached while leaving more than half the site undisturbed in existing forest. The concurrent rezoning would support the taller structure of mixed residential and non-residential uses that are being proposed.

4. The use conforms with the general plans for the physical development of the town as embodied in the appendix and in the comprehensive plan.

The project site is located at the northwest corner of South Columbia Street and NC 54. Though not in an area specifically identified for future development, the area has definitely changed and traffic increased since the residential zoning was instated. The topographic constraints and the location of the parcel facing two arterial roads make it less attractive for single family home development than for a mix of other uses.

2020 Comprehensive Plan

The proposed project complies with all six goals as specified in the 2020 Plan.

Theme 1: A Place For Everyone - The proposed development will add to the diversified housing types in town. (PFE.3)

Theme 2: Community Prosperity and Engagement - The project site is a vacant lot in a gateway location at the south end of town. The proposed development will increase the value of the land in a place where infrastructure is already completely in place. Because the site is so far below the street (25') and is limited by a stream and associated buffer only 200-300 feet inside the site, there is a limit to the R-2 potential building in this location. By recombining the parcels and creating access to the whole piece from the most northerly point, the lot will become more valuable by being fully developed, but only if the increased density and building height can be built. (CPE.1) The small size general business space available within the building will foster small / start-up businesses and live-work space, and the project provides housing and small scale work space within walking distance of the hospital and science buildings on campus. (CPE.3)

Theme 3: Getting Around - The project site has accessible frontage on one public street and is already connected to the rest of the community by sidewalks, bike lanes, and local bus service. (GA.2) There is an existing Chapel Hill Transit stop at the north end of the site which is serviced by 4 local bus routes. (GA.4) The developer proposes to install a shelter, solar lighting, and a passenger information sign to improve the existing bus stop at the Purefoy / Columbia intersection. Potentially a future BRT station would also be located in this block. The project is located very close to schools, the University, and the Morgan Creek Greenway Trail, allowing users to access the site on foot or by bicycle as well as public transit or private car. The project will provide underground and open car parking, and covered bicycle parking. (GA.8)

Theme 4: Good Places, New Spaces - The proposed development increases density in a well-serviced area, which helps minimize sprawl. (GPNS.1) Development of these lots was made difficult years ago when the raised South Columbia / NC 54 intersection was installed, creating a site located as much as 25' below street level at points, and with no possible access from the south end. Additionally, more stringent stormwater and stream protection requirements adopted during the past 15 years have severely limited the building envelope on site. The proposed development works within these limitations. The underground parking takes advantage of land below street level and the building itself acts as a transition providing a public street front on the east side and a more private space overlooking the natural environment of stream and forest toward the west. (GPNS.2 and GPNX.6) This development will be a signature building visible to everyone entering Town at this location. At the same time, the project is protecting the stream and buffer area, and by increasing the density on the east portion of the site is able to leave a large treed area protected in the western half of the site, thereby fully utilizing the site while protecting environmental elements. (GPNS.8)

Theme 5: Nurturing Our Community - The proposed development will provide tree coverage and stormwater management on site to meet or exceed the Town's standard requirements. (NOC.2) The steep difference between the street elevation and the stream elevation only 200-300 feet away creates a challenge to development that would require either fill or very high retaining walls to create a level building envelope. The building design takes advantage of this elevation difference by setting the building into the slope and allowing the building to become the transition between the street and stream as fixed elements. The site allows for building access on at least two levels and the development takes advantage of that by tucking over half of the parking under the building, thereby reducing impervious area and screening the parking from the street view. There will be a pedestrian plaza at street level providing an urban feel for the pedestrian and creating a gracious visual and pedestrian entry into the building. The west facing side of the building will have views of the stream and forest and allow the building to create a noise and visual buffer from the busy street. This is the direction many of the residential units will face. (NOC.7) The high density of the building itself allows the footprint to remain small and therefore allows a large portion of the site to remain undisturbed in existing forest and stream buffer. (NOC.3) The siting of the building also creates a transition between the noise and commotion of Columbia Street and the quieter residential neighborhood to the west. (NOC.8)

Theme 6: Town and Gown Collaboration - Though the project is not being built in collaboration with the University, it's location within easy walking distance of the south end of campus will make it an attractive support facility for University employees and students and people whose business is connected with the University. (TGC.4 and TGC.6)

Requested Modifications

The applicant is requesting 5 modifications.

1. Reduction of the width and plantings in the streetside landscape buffer on South Columbia Street. The required buffer would be 30' wide and planted to 'D' level opacity. The applicant is

requesting a reduction to 70% of the plant material in an area with an average width of 12' against the proposed new street sidewalk.

Landscape Buffer Reduction Justification

The Owner is creating a pedestrian plaza at the street level which is being partially built on top of underground parking and that roof system will not support the full landscape buffer. Additionally the design intent is to create an open space for pedestrians to transition between the public sidewalk and the streetside plaza and for the space to have an urban atmosphere rather than a suburban one. Parking near the street will be located 16' below street level and retaining walls and landscaping will be used to screen that parking which will not be visible from the street. We will work with NC DOT and are requesting permission from them to plant some trees the Columbia Street DOT right of way. The buffer as currently designed will vary in width between zero and 30' with an average width of 12'. The modification also requests a 70% reduction in plant material from the required 23 canopy trees, 47 understory trees and 156 shrubs to 7 canopy trees, 15 understory trees, and 50 shrubs within the project site.

2. Potential Parking Reduction

The unit mix and quantity within the building has not been finalized by the owner. Project minimum parking is 65 spaces, and if the unit count is kept to this, the 69 spaces that can be provided on site will satisfy the requirement.

Parking Reduction Justification

The project is accessible to multiple alternate forms of transit, and if at the time of final permitting the market does not demand as much parking, the total count may fall below the current LUMO requirement. The 69 spaces shown on the current plan are expected to satisfy the parking demand at this location. There are 28 double stacked or tandem spaces in the underground garage, suiting parking needs for 14 of the larger residential units. With the existing sidewalk system in place, the bike lanes, public transit service, and the location of this project close to campus and downtown Chapel Hill, it is expected that 80% of the parking requirement will fulfil the user demands and encourage alternative forms of transportation. The owner also intends to offer a discount on some units if they are taken without associated parking spaces. The owner is requesting a modification to allow a reduction to 80% of the required spaces as the minimum standard if needed at final plan.

3. Modification to allow structures and parking in the managed and upland RCD zones and to exceed disturbance and impervious limits within the managed and upland RCD zones

There is only 120-320 feet between the right of way and the existing stream on site. There is no associated flood plain on this property or within 300' of the parcel. The developer is requesting permission to construction the building and parking in the outer RCD zones and to concentrate

development on the east side of the stream, thereby leaving over half the site in undisturbed forest.

RCD Buffer Impact Justification

The Special Use Permit Application for this project was first submitted in April, 2014. At that time, the subject stream was identified by the Town as being *Intermittent* in nature, and the resulting 50' RCD line was incorporated into the project design. The advancement of the SUP was then stalled by delays created over indecision by NCDOT and the Town about potential future right of way taking needed for a S. Columbia/Fordham Blvd. Interchange improvement precipitated by the Obey Creek Project south on NC 15/501. The Applicant subsequently withdrew the application and then re-submitted in order to try and force the issue on the right of way acquisition.

In the interval between the initial submittal and the recent submittal, Town Staff re-evaluated the stream and subsequently determined that it is now a *perennial* stream, which by LUMO requires a 150' RCD from top of bank. The developer is puzzled by this, as the stream and surrounding environment have not changed since the initial application. This determination now essentially renders the land area between the eastern side of the stream and South Columbia unusable, absent a consideration of an RCD Encroachment.

Further, the location of the stream channel bisecting the site north to south, along with the constraint along Fordham Blvd on-ramp means that the land on the west side of the stream is challenging at best to access. This is further exacerbated by the steep slopes to the east and west of the channel. The site is for all intents and purposes rendered un-buildable given a 300' plus wide swath of RCD through the property.

We would suggest that proposed project has too many positive benefits to the goals of the Town to remain undeveloped. Those include creation of a mixed-use building that has the inherent efficiencies of live/work opportunities; the millions of dollars in tax base created for the Town and County; and not least the construction of a significant well-designed building that can further enhance the southern gateway into Town.

We would also suggest that there are negligible if any adverse environmental impacts that would be created by granting our RCD encroachment request. The lowest floor elevation will still be well above the above the stream channel. There is no flood plain on the site that would be impacted. Stormwater will be treated in an underground detention facility to meet State and Town standards. This facility will be reviewed and approved by the Town Stormwater Department prior to construction and will mitigate increased runoff conditions caused by the increase in impervious surface on the site. The project development will removed some stream obstructions currently in place, and will help remove some of the non-native invasive vegetation on site.

For these and other reasons we are therefore requesting that the project as proposed be allowed to be constructed in the location shown on the SUP Plans.

4. Modification to steep slope disturbance

LUMO section 5.3.2 specifies that the disturbance of steep slopes is to be minimized in order to protect water bodies from the effects of erosion on water quality, to protect plant and animal habitat, and to preserve the natural beauty and economic value of the Town's wooded hillsides. And the stated limit to disturbance of steep slopes is one quarter of existing steep slopes on site.

Steep Slope Disturbance Justification

Basically all the land between the stream buffer and the street will be disturbed to install the project and that land is very steep in its entirety. There is no flat space in the parcel that would be better suited to development. The majority of steep slope on the site is located in this area and was man-made during the South Columbia Street/15/501/NC54 interchange construction over 20 years ago.

The steep slopes on site are not related to water bodies or wetlands. And the terracing of the site and stormwater mitigation of all impervious area will protect the stream area. When the project is complete there will still be significant evidence of steep slopes and elevation change throughout the site.

5. Modification to required use mix percentages in the MU-V zone

LUMO section 3.5.1 specifies that the MU-V mixed use district have a mix of at least 25% floor area residential and a minimum 25% floor area in office/commercial uses. The developer is requesting an exemption to the minimum non-residential component to allow as little as 6% non-residential uses.

Office/Commercial Percentage Reduction Justification

The building is not located in an area with other businesses, but is on a busy street not suitable for the single family residential use it is currently zoned for. The existing topography of the site is conducive to a tall building (a high square footage) to bridge the elevation changes between the stream buffer area below and the existing streets above. But there is no on-street parking nearby or public parking available, so any uses on site will need to have parking provided on site. The parking requirements for commercial or office space are much higher per square foot than residential use, both the ordinance and market driven requirements. The non-residential space in this project will target consultants or businesses that do not have a lot of visitors, and will target users who live in the building so that the project is geared for a live/work facility. So the developer is asking for a reduction in the commercial component to be able to provide a live/work space, but not a full office/restaurant/retail experience as the area is not currently conducive to retail or restaurant uses.



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Planning for the Future

RESPONSE TO CONCEPT PLAN REVIEW, COMMUNITY DESIGN COMMISSION COLUMBIA STREET ANNEX

1150 South Columbia Street Town File 9788-20-4502 April 26, 2018 REVISED May 11, 2020

The concept plan was original presented to the COMMUNITY DESIGN COMMISSION on WEDNESDAY, OCTOBER 24, 2007, 6:00 P.M.

The project included 87,000 sf floor area in four buildings proposed as a mixed use development on a 4.6 acre site located in an R-2 zoning district. There is RCD on the site. The uses included 75,000 sf residential in 32 dwelling units with 15% proposed as affordable housing. The remaining 12,000 sf would be retail and office on the ground floor. Parking for 80 cars was proposed. The mixed use proposal was described as a carbon zero development.

The scope has changed slightly since 2007. As the design is developed, the proposed design for the 4.6 acre project now includes 52 residential units, 4,000 sf of non-residential general business space, and a parking reduction to approximately 69 spaces. Total building square footage will be reduced to 61,000 sf.

At the hearing: Chairperson Jonathan Whitney called the meeting to order at 6:00 p.m. Commission members present were Mark Broadwell, George Cianciolo, Kathryn James, Gretchen MacNair, Laura King Moore, Scott Nilsen, Amy Ryan, Jonathan Whitney (Chair), and Robin Whitsell (Vice-Chair). Staff members present were Kay Pearlstein, Senior Planner and Kay Tapp, Senior Planning Technician.

CITIZEN COMMENTS

1. Joe Capowski, a resident of Coolidge Street, stated that the biggest bottle-neck to the project is S. Columbia Street traffic. He showed a photo taken the previous day at 5:00 p.m. of traffic on S. Columbia Street to illustrate the traffic problem. He believed that if would be difficult if not impossible to make a left turn out the proposed development at certain times of the day during peak traffic. He

supported bus use, but stated that it would be dangerous for people to cross S. Columbia Street to catch the bus to the University. He believed that the traffic problem has to be solved or the project will not be viable.

RESPONSE: Since the project was first proposed in 2007, DOT has completed road improvements on South Columbia Street. There is a stream and buffer running through the site from north to south which limits the opportunity for access/egress point anywhere but on the same side of the site as the development is proposed – which takes traffic onto South Columbia Street. Road improvements now completed include bike lanes and turn lanes and pedestrian crosswalks. Additionally this building as a mixed use space with sidewalks and bike lanes connecting it to other parts of town will promote a clustering of activities and alternate forms of transportation.

 Kimberly Brewer, a resident of Purefoy Road, thought that it is a wonderful project but should be on a different site; too intensive; presence of Resource Conservation District; and heavy traffic on S. Columbia Street. She believed that the 80 spaces proposed with the project will create additional traffic hazards.

Ms. Brewer stated that the Entranceway Plan for this location identifies green areas and tree canopy rather than a 2-story building. She wanted to see a project more compatible with Residential-2 zoning and better bicycle and pedestrian circulation on S. Columbia Street before major projects are built.

RESPONSE: The on-site stream has been determined to be perennial, which creates a 50' stream buffer and 150' resource conservation district zone on each side from the top of bank. There will be little disturbance of the 50' stream buffer, and much of that disturbance will involve stream improvement including removal of old culverts that are creation erosion problems.

The developer is proposing 69 parking spaces on site, but it is expected that a higher than average number of trips to and from this site will be by foot or bicycle or public transit. Ms Brewer was concerned about bicycle and pedestrian circulation on S. Columbia Street, but DOT has finished road improvements on this stretch of road to accommodate bicycles and pedestrians, including bike lanes, sidewalk, and crosswalks. This project will add skip striping as a further visual indicator of multiple users on the street. As for the intensity of use on this site, major thoroughfares are exactly where higher intensity uses should be concentrated. And the higher density residential mix in the proposed project provides an appropriate buffer between South Columbia Street and the residential uses to the west.

3. Scott Radway, a resident of Chapel Hill, stated that he had presented a proposal on this property previously and that the site is an isolated piece leftover when NC 54 was constructed. He stated that the on-site Resource Conservation District is a man-made corridor of drainage pipes and should be reconstructed.

He believed that the traffic issues are significant but that the proposed driveway is a good solution. He thought that the project should be viewed as a significant RCD reclamation project.

RESPONSE: The developer proposes a single right-in/right-out access point across from Purefoy Road. The design team has started discussions with Town of Chapel Hill stormwater staff, and also with NC DWQ staff. The stream is under the jurisdiction of Waters of the United States, and therefore the developer would need to get a US 404 permit and a NC 401 permit to do any restoration work within the stream. The design team has been advised that there will be no stormwater credit given for restoration work done on site. Given the time and expense involved in the permits and the expense of stream restoration without reduction of other stormwater requirements on site, the Owner feels it is unlikely that stream restoration will be part of this project. Little disturbance of the 50' stream buffer is proposed, and will include some minor stream restoration where an old culvert is creating erosion problems.

COMMUNITY DESIGN COMMISSION COMMENTS

1. Commissioner George Cianciolo was interested in hearing more about reclamation of the Resource Conservation District. He wants this location to be used for significant project and architecture. He encouraged the Town to be innovative with the use of the site.

Commissioner Cianciolo believed that traffic was a significant problem and encouraged the applicant to talk to Town and NCDOT traffic engineers. He supports the use of geothermal heat and wants to know that it is workable from the beginning of the project.

He asked the applicant about an Affordable Housing component. The applicant replied that Robert Dowling had suggested a payment-in-lieu. The applicant replied that it was crucial to have a non-segregated social base. The applicant also proposed live-work building design to cut down on the need for car travel.

RESPONSE: The Commission comments reference many issues.

- The developer has investigated stream restoration work within the RCD and stream buffer. Work within that stream would require multiple permits from state and federal agencies and would not reduce stormwater mitigation facility requirements on site. Therefore, given the time and monetary costs of stream restoration, the owner feels it is not feasible for this size project though some small amount of stream restoration will occur where a misplace culvert is creating erosion problems.
- The architecture on site will be distinctive, and will be intensive on the portion of the site that is developable. The architect and owner expect the building development to act as a gateway of sorts as people exit the highway and head toward the center of town.
- Traffic in this area is an issue but NCDOT has recently completed road improvements. The

scale of this project is small enough that the owner will be requesting a TIA exemption but will work with the Town Traffic Engineer to determine if counts are relevant at the final plan stage. The design team has started discussions with the Town Traffic Engineer and will also be coordinating with the DOT regional office.

- The use of geothermal heat has been further investigated and is not going to be feasible on this project.
- AFFORDABLE HOUSING The developer has met with Robert Downing and is proposing 8 affordable units in the project.
- The project will be mixed use residential / general business and because of its proximity to UNC campus and to public transit, the owner expects lower than average vehicular traffic and higher than average pedestrian, bicycle, and public transit use.
- 2. Commissioner Scott Nilsen thought the design very innovative but the intensity was too great. He stated that in order to make the live-work concept effective ideal tenants would need to be found. He wanted to see further development of the project.
- RESPONSE: The proposed development results in a .312 FAR which is not overly intense for development along a thoroughfare, and is within the 1.2 FAR allowed by the proposed MU-V zoning. During the SUP process the design will be developed with much the same scope as was presented during the Concept Plan phase.
- 3. Commissioner Mark Broadwell considered the presence or Resource Conservation District on the property an opportunity and not an obstacle. He suggested that the western portion of the site remain undeveloped and additional compact parking spaces be included. In so doing, the western parking area could be unnecessary. He also suggested a traffic light at Purefoy Road to help with traffic.
- RESPONSE: The current project scope proposes to use only the eastern portion of the site between the stream buffer and South Columbia Street. The design proposes a single vehicular access point across from Purefoy Road, with a full turn configuration and will work with DOT to ensure a safe design. It is unlikely that DOT would approve an additional traffic light so close to the existing lights at the bypass on-ramp, which are located only 450 feet to the south.
- 4. Commissioner Robin Whitsell thought there was a lot of pavement on the western side of the site for parking area. She liked the environmental aspects and supported the scale of the project. She also wanted to see a bike-friendly design.
- RESPONSE: All the pavement for parking is on the eastern portion of the site, and more than half will be under the buildings. That pavement not only serves the surface parking spaces, but in fact all of the aisles are required for fire access. The project will be bicycle friendly indoor

- bicycle storage will be provided, and general proximity to downtown, UNC campus, and greenway trails should provide incentive for bicycle use.
- 5. Commissioner Laura Moore liked the unique aspects of the project. She particularly liked the proposed stream restoration but is not comfortable with the encroachments into the RCD. She wondered why the RCD had to be crossed at all. Instead, she recommended focusing the development on the eastern portion of the site and leaving the reminder of the site undeveloped. She wanted the applicant to concentrate the development and retain tree stands. She is excited about the project but cautioned the applicant about traffic and bicycle circulation problems.
- RESPONSE: See response on p.2 regarding the extent to which stream restoration which is likely to occur. The project will focus on the eastern portion of the site and is not expected to cross the stream and buffer, but the owner will be asking for permission to encroach into the managed and upland RCD zones.
- 6. Commissioner Amy Ryan supported leaving the western portion of the site undeveloped. She thought reclamation of the RCD was acceptable but concerned about precedents for encroaching into the RCD.
 - She thought the siting of the project across from Merritt's Pasture is significant and did not want to see a typical office building with parking lots on this site. Instead, she wanted to see something with a subtle impact on the land and recommended a green site with suitable architecture. She was excited about a zero carbon development.
- RESPONSE: The current project proposes development on the eastern portion. The design proposes an intensive use of the eastern portion but for now leaves the western portion untouched. The majority of the parking will take advantage of the slopes on site and will be provided under the building. In fact the whole development takes advantage of the slopes and multiple stories will be located below the street level and therefore the project will appear much less intensive than the numbers imply.
- 7. Commissioner Kathryn James hopes that the project goal of zero carbon works. She recommended intensity on the street and at the same time retaining the green canopy along S. Columbia Street.
 - She is concerned about the building mass but supports careful integration with nature. Keeping the building transparent is important.
- RESPONSE: It is unclear at this stage if a zero carbon footprint is feasible. The use of the eastern portion of the site will be intensive, but in a pedestrian friendly way, with plazas and new plantings at the street level overtop of underground parking. It is not anticipated that any existing vegetation along South Columbia will be retained.
- 8. Commissioner Gretchen MacNair hopes the buildings are wonderful and will be visible from Fordham Blvd. and S. Columbia Street.

RESPONSE: The buildings will certainly rise above and be visible from S. Columbia Street.

SUMMARY

The Commission was excited about the project, especially the proposal for a zero carbon design. They like the innovative design and architecture. The biggest concern for the Commissioners was traffic on S. Columbia Street – vehicular, pedestrian, and bicycle. A traffic light, easy bus access, unique tenants for the live-work units, and better circulation for bicycles to campus and pedestrians across S. Columbia St. were recommended.

RESPONSE: NC DOT has finished improvements on South Columbia Street with turn lanes, bicycle lanes, and pedestrian routes. A traffic light at the Purefoy / S. Columbia / project driveway location is unlikely to be allowed given the close proximity of the intersection with existing traffic light configuration at the by-pass junction just 450 feet to the south, and given that DOT recently completed extensive road improvements in this stretch and did not include a new light.

Several members were concerned about the proposed intensity and suggested leaving the western portion of the site undeveloped and not crossing the RCD. They wanted to keep the site green and concentrate the buildings close to the road. They encouraged the applicant to pursue reclamation of the RCD but concerned with precedent setting and minimizing encroachment into the RCD.

RESPONSE: The current proposal is for development to be concentrated in a high density use between the stream buffer and South Columbia Street, and to leave the western portion of the site untouched. The owner will be fully encroaching into the managed and upland RCD zones on the east side of the stream as there is no non-RCD land accessible on this site. Extensive stream restoration is not anticipated. Stormwater management will be built to manage any increase in runoff caused by the project.

Some Commissioners wanted the architecture to be seen from the highway and street and others were interested in maintaining tree canopies and reducing tree removal. A relationship to Merritt's Pasture and the Entranceway Plan was recommended that includes careful architectural design.

RESPONSE: The new development is proposed to extend multiple stories above South Columbia Street and will certainly be visible from that thoroughfare. The proposed architecture will be unique and the building design will incorporate some kind of street level plaza and plantings. It is unlikely that any existing tree canopy will be retained along South Columbia Street, but new plantings will be installed.



Coulter Jewell Thames, PA

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Planning for the Future

RESPONSE TO CONCEPT PLAN REVIEW, PUBLIC HEARING AT TOWN COUNCIL COLUMBIA STREET ANNEX

1150 South Columbia Street Town File 9788-20-4502 April 26, 2018 REVISED MAY 11, 2020

The concept plan was original presented to the TOWN COUNCIL on MONDAY, FEBRUARY 18, 2008, 7.00 pm.

Present were Mayor Kevin Foy, Mayor pro tem Jim Ward, Council Member Matt Czajkowski, Council Member Laurin Easthom, Council Member Sally Greene, Council Member Ed Harrison, Council Member Mark Kleinschmidt, Council Member Bill Strom, and Council Member Bill Thorpe.

Staff members present were Town Manager Roger Stancil, Deputy Town Manager Florentine Miller, Assistant Town Manager Bruce Heflin, Town Attorney Ralph Karpinos, Planning Director J.B. Culpepper, Development Coordinator Gene Poveromo, Planner Dana Sridham, and Acting Town Clerk Amy Harvey.

3. Concept Plan - Columbia Street Annex at 1150 S. Columbia Street.

Ms. Culpepper presented the concept plan for the Columbia Street Annex Development and displayed a vicinity map of the location. She said the staff hadn't reviewed the proposal and they hadn't received a formal zoning or special use permit application. She said the proposal included construction of an 87,000 square foot development with residential, office, and retail space and 32 dwelling units. The ground floor would have approximately 12,000 square feet for retail and office use, parking for 80 vehicles and the property was 4.6 acres in size, she said. She recommended that the Council review the concept plan and following the discussion adopt a resolution transmitting the comments to the applicant. The CDC had reviewed the proposal and their comments were included in the materials provided.

Since 2008 the project has been refined and now includes an increase in residential units and a decrease in retail and commercial space and a decrease in parking. The current scope includes:

- 52 residential units
- 4,000 sf general business space
- 69 parking spaces

Phil Szostak stated that his project wasn't required to present to the Council because it was less than five acres but said he really needed their input on the issues related to the RCD. He said they named the project Carbon Zero because the power generated was made on site and it was one of the country's first projects of its kind. He said they were looking for a site in Chapel Hill to build the project that had good solar exposure in a high traffic area and they had reduced the size from 80,000 square feet. He added that the project would be looked at globally, regionally and locally and they talked the about the socioeconomic environmental impacts on the community.

At this point a carbon zero project is not feasible.

Mr. Szostak said from the environmental sensitivity side they were using the project like a research building because they were looking at new systems, new ways to build and new financial dynamics. He stated that the social equity proposed affordable housing and the economic vitality was difficult because of the RCD issue. He said their project with a 150-foot buffer cut off every bit of the project except for one corner which was for a single family house, which was an unbuildable lot.

The Town and State have determined the stream is perennial, with a 50' stream buffer from top of bank on both sides, and 150' RCD zone from top of bank. The majority of the land between the buffer and the street is in the managed and upland RCD zones and therefore a request is being made to concentrate building in these zones, otherwise the site is practically unusable.

Mr. Szostak said that the ordinance, the way it was written, had a lot of specific tools used to determine creeks but they could only have a creek determination done once every five years and the last determination was in 2004. He said he had two scientists complete the creek evaluation and it hadn't been determined to be a perennial stream, but if someone had tampered with the waterway they would have to look downstream or upstream and use that classification. He stated they also looked at the entire watershed and how much water was generated that ran through their property. He said the 15-inch pipe was enough needed to transmit the water, but it had the same buffer requirement as Morgan Creek, Booker Creek, and the creek that ran through the Eastgate Shopping Center. He displayed photos of the creek and the 48-inch pipe that went under the bypass and said that as soon as the water got to a DOT piece of property it was unprotected and went into Morgan Creek. He said that a 300-foot RCD buffer was required on his property and they proposed to re-channel the creek and restore it as an ecosystem. He said the RCD had been a great tool for the community in preserving water quality and he asked if water quality was really the issue then how did they address protecting the trash found in it. He said they were planning to stabilize the channel by using stormwater management with low impact development techniques and to increase the public access.

The RCD remains from 150' from top of bank on each side. The majority of the land between the buffer and the street is in the managed and upland RCD zones and therefore a request is being made to concentrate building in these zones, otherwise the site is practically unusable.

Mr. Szostak said their building would include combined heating and power strategies, wind generator, photovaltaic, solar thermal, passive solar gain, occupant load, biofuels, and geothermal. He said their building was thin enough with a long face facing south to allow them to use natural ventilation throughout, rainwater collection to reuse for irrigation and flushing toilets, and biofuels for combined heat with solar at the top. He stated that they mapped out the site and the road system that would go around all of the trees without taking any of them down. He said there were comments made at the CDC meeting about the construction over the creek and they decided to take the road out of the plan so the building would remain on the east side of the creek. He said they looked at four stories above the road and the solar rays would be on top of the building. He stated the project met all of the design guidelines for the Town, the LUMO compliance standards, and all the other local initiatives. He said the issue was that Chapel Hill had never attempted a project like it before and never went against the RCD ordinance to modify a particular project. He said that he was trying to get some direction from the Council to see if the project was worth proceeding with.

Mayor Foy asked him to go back to the slide that showed the placement of the building on the land and asked where the issue was with the RCD. Mr. Szostak said the site map was showing a 50-foot RCD for an intermittent stream. Mayor Foy clarified that they planned to make the water corridor more functional and that it would leave the property cleaner than what it was currently. Mr. Szostak said it would definitely have cleaner water because of the project, even though it was going into the DOT pipe. He said they talked to DOT because they wanted to get the pipe included in their project and they weren't interested because it would mean work in their right of way. He said that opening the area to pedestrians and opening the water course would make it a greater asset to the community. Mayor Foy said that the purposes of the RCD were listed on a previous slide and his reaction was that they were trying to achieve those purposes and not a rigid adherence to the rule. He said that he felt the staff was right to let the Council have some input because he didn't object to the proposal.

Mayor pro tem Ward asked how could they have a higher flowing stream turn into one that was less than or how could it go from perennial down to intermittent. Mr. Szostak stated he felt it was inaccurate and that a perennial stream should remain as such. He added that as it had been urbanized over many years it had changed. Mayor pro tem Ward agreed with the logic of Mayor Foy and that he looked for the outcomes related to the RCD. He said he hoped the Council and the community could see the value in being flexible within the framework of maintaining the goals they set out to achieve in the first place. Mr. Szostak replied that the way the LUMO was written it gave a little flexibility.

Kimberly Brewer said that she and most of their neighbors applauded most of what Mr. Szostak was trying to do on the site. She said the neighbors had traffic and practical safety concerns of getting into and out of Purefoy Road because it was already dangerous and would be more so with the proposal. She suggested at minimum they needed a traffic light at the intersection of Purefoy and Columbia. She said that Mr. Szostak made a good argument for looking at the outcomes regarding the RCD and that they

could achieve a lot of water quality benefits with a narrower buffer given the right design. She said it was also important to think about the scale and the design of the project and how it related to the adjacent homes and neighborhoods with the entranceway plan and the DOT plan for improving South Columbia Street. She said that Mr. Szostak was trying to make lemonade out of lemons on the difficult site but there was a lot of work to do particularly on the traffic issue. She spoke on behalf of Joe Capowski and said he could live with the project if a traffic light was installed at Purefoy and South Columbia Streets, otherwise he believed the project was a non-starter.

There is a stream and buffer running through the site from north to south which limits the opportunity for access/egress point anywhere but on the same side of the site as the development is proposed – which takes traffic onto South Columbia Street. DOT has finished road improvements on South Columbia Street. The improvements include turn lanes, bicycle lanes and pedestrian routes, as well as a median / pedestrian safe haven in the center lane at Purefoy. But there is no traffic light proposed for this intersection and given that it is only 450' from the traffic lights at the by-pass on-ramp it is unlikely that DOT would approve an additional light at Purefoy. The developer has been working with DOT to provide a safe vehicular access design for the site.

Council Member Harrison agreed with Ms. Brewer that the big issue was the safety of people on South Columbia Street. He said that he had asked Mr. Capowski how, as an experienced cyclist, he got onto South Columbia Street from where he lived and was told that he went through the neighborhoods and didn't try getting onto South Columbia Street. He said the last Mobility Report Card received in 2005 showed that South Columbia Street at Purefoy Road had 27,000 trips daily and the UNC traffic report done every two years showed the intersection to be a profound failure. He stated the project was problematic without a traffic signal and that Chapel Hill Transportation Engineering and DOT would probably agree. He said he couldn't figure out the entry lined up with Purefoy Road even after looking at the maps provided. He informed Mr. Szostak that the proper term was stream restoration and it was worthy thing to do but it had a separate permit process from the Town's application process.

Council Member Strom stated that some flexibility in situations that met a public purpose was the prudent way to proceed. He said he didn't hear Mr. Szostak ask to be exempt from the full review process that led to special use permit and that he should be aware of the community's concerns with traffic and how the project would fit into the neighborhood. He said in regards to the trade-off he was asking the Council to make regarding the RCD that he encouraged Mr. Szostak to continue with the project. He added they would have to pay attention to how they wrote the permit to avoid opening any project that didn't meet a similar threshold of his project.

Council Member Kleinschmidt supported the project and thought it was fascinating. He said they needed to make sure the exceptions required a great deal of evidence comparing the present condition and the proposed condition, and ensuring adequate protection for downstream and all the interests they had in protecting Morgan Creek were referenced. He said he was looking forward to seeing how

Mr. Szostak demonstrated how rare the project was and how infrequent it would be that a future developer would ask the same request. He added that he was curious about where it was a perennial stream and that he wanted to reinforce some of the comments from the CDC meeting about the great opportunity he had to make the entrance way of South Columbia Street beautiful.

Mayor Foy agreed and stated they didn't have an interest in hiding the entrance ways. He said that the RCD helped create open space and breathing room in the community and that the proposal respected that aspect of the ordinance. He added that it would be important to Mr. Szostak to continue demonstrating why the proposal was unique and articulating the purposes.

The latest design places all development within the strip between the stream buffer and South Columbia Street. The design has emphasis on a unique building and plaza visible as a gateway when people enter Town from the south. At the same time a large portion of the site is retained in trees and stream.

COUNCIL MEMBER MATT CZAJKOWSKI MOVED, SECONDED BY COUNCIL MEMBER MARK KLEINSCHMIDT, TO ADOPT R-1. THE MOTION WAS ADOPTED UNANIMOUSLY (9-0).

A RESOLUTION TRANSMITTING COUNCIL COMMENTS ON A CONCEPT PLAN FOR THE COLUMBIA STREET ANNEX (2008-02-18/R-1)

Columbia Street Annex
Chapel Hill, NC
Special Use Permit Application, April 2018
Energy Management Plan

Description of how project will be 20% more energy efficient than ASHRAE Standards

The Columbia Street Annex project has been planned to be as near Carbon Zero as possible. The building envelope including its materials and windows will contribute to its efficiency. All windows will use low-E glazing and will possibly be triple glazed in order to provide the best possible energy control, but still allowing operable glazing for fresh air. Insulating units throughout will be designed to exceed ASHRAE standards for heat transmission. Each unit's heating and cooling will be handled by its own high efficiency heat pump unit, exceeding current minimum standards.

Description of how project will ensure indoor air quality, adequate access to natural lighting, and allow for proposed utilization of sustainable energy

The residential levels of the Columbia Street Annex have been designed so that each dwelling unit has access to natural light and ventilation on two opposing ends of each unit. This is a strategy that allows the use of natural cross-ventilation, minimizing the need for air conditioning in warmer months.

This generous access to natural light will also minimize the use of electricity for lighting during daylight hours. Each unit in the building will have its own fresh-air intake louver integral into a high-efficiency heat pump unit, ensuring fresh air exchange in all seasons.

Description of utilization of sustainable forms of energy (Solar, Wind, Hydroelectric, and Biofuelus)

From its inception, Columbia Street annex has been conceived with the implementation of sustainable energy as a core design feature. The roof will allow space for a solar array of approximately 6,000 sf, generating up to 60KW of energy. This amount of power would supply the building's entire power needs during most hours of daylight, and would likely be contributing excess power to the regional grid during peak sun hours when power needs of the building will be low.

Description of how project will maintain commitment to energy efficiency and reduced carbon footprint over time

The building's roof will hopefully house photovoltaic panels. PV panels could supply electric power. Perhaps more importantly, the in-town location of the building and the density of housing provided are the primary means by which the building serves as a model of reduced carbon footprint, allowing many occupants to forego the use of cars as part of the daily commute.

Description of how the project's Transportation Management Plan will support efforts to reduce energy consumption as it affects the community

The Columbia Street Annex site is uniquely situated for its proximity to bus lines serving Chapel Hill and the region. In addition to public transit availability, the site's convenient location relative to UNC Chapel Hill and

the UNC Hospitals is a great opportunity for pedestrian and bicycle commuting, eliminating the need for fossil-fuel based transit for many residents of the complex. The realistic opportunity for walking/biking commuting as an alternative to automobile commuting encourages a decrease in auto use and auto ownership, reducing pollution, energy consumption, and vehicular traffic. Further CSA will provide:

- 2 vehicle charging stations (to service 4 parking spaces)
- Conduit installed during construction to allow for future charging stations to serve at least 20% of parking spaces

COLUMBIA STREET ANNEX – AFFORDABLE HOUSING PROPOSAL

1150 S Columbia Street14 February, 2020

The developer is proposing to construct condominium residential units in a 6-story building. The total number of condominiums will be 52 units. The interior of the building has not been finalized so some of the unit sizes may change, and the total number of units adjusted accordingly. If the maximum number of units were offered, seven affordable units would represent 15% of 45 market rate units.

The developer proposes to provide 8 affordable units, 4 of which would be sold at 80% of AMI, and 4 at 65% AMI. These 8 units represent 18% of the market rate units. If the total unit count decreases, the number of affordable units would also decrease proportionally. If the calculation comes out to a percentage of a unit, the owner would make a payment in lieu for 15% of the partial unit. The payment in lieu funds would allow the Community Home Trust to subsidize affordable units for use by specialized groups such as adults with intellectual disabilities.

It is anticipated that the project will contain studio, 1 bedroom and 2 bedroom options. The 1-bedroom units will be approximately 575 sf. The 2-bedroom units will be approximately 865 sf. The affordable units will all be 1-bedroom size. The majority of the project will offer smaller units and is not targeting families. The developer proposes to make the affordable units available to individuals and couples making less than the median income for the area.

LAND USE PLAN AMENDMENT APPLICATION



TOWN OF CHAPEL HILL Planning Department 405 Martin Luther King Jr. Blvd

405 Martin Luther King Jr. Blvd email planning@townofchapelhill.org www.townofchapelhill.org

Parcel Iden	Parcel Identifier Number(s) (PINs): 9788 20 4501, 9788 20 6500 Date: 17 Oct 2019							
Section A:	Section A: Property Information							
Property A	perty Address(es): 1120/1150 South Columbia S			Street		Zip Code:	27516	
Existing La Category:	nd Use	Low	Residential		Proposed Land L Category:	lse	High Residential	
Associated Atlas Amer	with Zoning ndment?	⊠ Ye	s 🗌 No		Total acres withi boundaries:	n lot	3.2 ac	
Section B:	Applicant,	Owner, a	nd/or Contract	Purchas	er Information		N. A. B.	
Ap	plicant Info	rmation (t	o whom correspo	ondence v	will be mailed)			
Name:	Wendi Rams	den, Coult	er Jewell Thames F	PA	in E _p			
Address:	111 W Main	Street						
City:	Durham		****	State:	NC	Zip	Code: 27701	
Phone:	909-682-036	8		Email:	wramsden@cjtpa.co	m		
inf Signature:	The undersigned applicant hereby certifies that, to the best of their knowledge and belief, all information supplied within this application is true and accurate. Signature: Date: 10-24-2013 Owner/Contract Purchaser Information:							
					C			
\bowtie	Owner				Contract Purchase	i.i		
Name:	C H Hotel Associates Limited Partnership							
Address:	3008 Anders	on Drive, S	uite 120					
City:	Raleigh			State:	NC	Zip	Code: 27609	
Phone:				Email:				
The undersigned applicant hereby certifies that, to the best of their knowledge and belief, all information supplied within this application is true and accurate. Signature: Date:								

Page 1 of 3



LAND USE PLAN AMENDMENT APPLICATION SUBMITTAL REQUIREMENTS

TOWN OF CHAPEL HILL Planning Department

The following items must accompany your application. Failure to do so will result in your application being considered incomplete. For assistance with this application, please contact the Chapel Hill Planning Department (Planning) at planning@townofchapelhill.org.

n/a	Application fee (refer to fee schedule)	Amount Paid \$	0			
n/a	Pre-Application Meeting – with appropriate staff	Meeting Date:				
Χ	Digital Files – provide digital files of all plans and documents					
Χ	Mailing list of owners of property within 1,000 foot perimeter of subject property	erty (see GIS notif	ication tool)			
n/a	Notification Postcard fee for above mailing list (refer to fee schedule)	Amount Paid \$				
Χ	Statement of Justification					
Х	Digital photos of site and surrounding properties					
Χ	Site Map depicting the property to be amended (digital file only, no hard copy required)					
Χ	Area Map (digital file only, no hard copy required)					

Statement of Justification

The Chapel Hill 2020 Land Use Plan is an instrument for implementing the land use goals and objectives of Chapel Hill 2020. The Plan reflects substantial public input and general agreement among various stakeholder groups. It is therefore only intended to be modified under limited circumstances.

The Statement of Justification for the Land Use Plan Amendment should explain how the proposed amendment meets the following criteria:

- a) Consistent with the goals and policies of Chapel Hill 2020, including plans adopted as additions
- Responds to significantly changed conditions since the last time the Land Use Plan was amended.
 Significantly changed conditions may refer to revisions to an adopted policy, unanticipated consequences of an adopted policy, site conditions, and/or development patterns
- c) Enhances the public health, safety, and welfare of the town
- d) Compatible with the land use of adjacent properties and with surrounding development patterns (existing or envisioned by adopted plans)

Include supporting information such as:

- Specifically cited goals of Chapel Hill 2020 and other plans adopted as additions
- Infrastructure projects, transportation projects, and development proposals in the surrounding area
- Surrounding land use patterns
- Physical characteristics of the property

If the applicant is proposing a development project for the property to be amended, please note that specific elements of the proposed development will not be considered as part of the justification. The change of Land Use Category in and of itself should meet the criteria listed above.

Please consistently identify Land Use Categories as stated on the Chapel Hill 2020 Land Use Plan.

Site Map Requirements

Map should be legible and clearly drawn. Map should include the following:

- a) Site name, PIN, and address
- b) Applicant and contact information
- c) Legend, north arrow, and engineering scale denoted graphically and numerically
- d) Boundaries of Land Use Categories applied by the Land Use Plan to subject property and surrounding properties
- e) Significant buildings and landmarks
- f) Existing roads (public & private), rights-of-way, sidewalks, driveways, vehicular parking areas, and street names
- g) Significant land features including vegetation, slopes, streams, water features, drainage features, floodways, and floodplain
- h) Existing easement boundaries for access, water, sewer, electrical, and drainage

Area Map

Map should be legible and clearly drawn. Map should include the following:

- a) Subject property location
- b) Applicant and contact information
- c) Legend, north arrow, and engineering scale denoted graphically and numerically
- d) Significant buildings and landmarks
- e) Existing roads (public & private), rights-of-way, and street names
- f) Planned transportation and infrastructure projects
- g) Protected open space, parks, and greenways (existing or planned)
- h) Corporate limit lines
- i) 1,000 foot notification boundary
- j) For all properties within notification boundary:

Property lines, Land Use Category boundaries, project names (where known), and existing land uses



MAIN OFFICE 111 WEST MAIN STREET DURHAM, NC 27701 p919.682.0368 f919.688.5646

Planning for the Future

COLUMBIA STREET ANNEX

1150 South Columbia Street
LAND USE PLAN AMENDMENT APPLICATION - **STATEMENT OF JUSTIFICATION**24 October 2019

The site is located at the northwest corner of South Columbia Street and the NC 54 westbound entry ramp. The parcel is in a land use designation for Low Density Residential, 1-4 units per acre, and zoned R-2 which permits only single family residential and duplexes, with a maximum FAR of .093. The developer is asking for a rezoning to allowing a mix of uses and more dense development, and therefore is also asking for a Land Use Plan Amendment to allow the rezoning. The parcel is not within any of the 2020 Plan Future Focus Areas, but is along a stretch of road recently improved by NC DOT.

JUSTIFICATION FOR THE LAND USE MAP AMENDMENT

The applicant believes the land use map amendment is justified for two reasons:

- a) Because of changed or changing conditions in a particular area or in the jurisdiction generally, and
- b) To achieve the purposes of the Chapel Hill 2020 Plan.

Changed or Changing Conditions

Since 2000 there has been a large change in the traffic patterns and nearby uses at this location. In 2014/15, DOT made significant improvements to this section of S Columbia Street, widening the street, adding turn lanes, crosswalks, bike lanes, and curb and gutter. There have been no new single family homes built along this stretch of S Columbia Street, and in fact no new development at all in this stretch. Given the configuration of ramps exiting off the by-pass, this corner acts as a gateway into town and currently the corner is either vacant or has fencing/rear yards of homes.

The site is a vacant lot located on a busy street that has recently been improved by NC DOT. Those improvements included bike lanes, sidewalks, safe haven/pedestrian cross walks, and turn lanes. The location of the site is at a gateway into Town and is within walking distance of the UNC campus. The site is currently zoned R-2 but is on a busy section of an arterial road and overlooks the NC 54 bypass. The 2017 NCDOT maps show a traffic count of 14,000 trips daily on this stretch of road. And it is on multiple

Chapel Hill Transit bus routes, 4 of which stop in front of the project site. Because of the proximity of the site to the NC 54 and Fordham Blvd overpass intersection, this site is not conducive to being developed for the single family home use for which it is designated and zoned. The site to the north acts as a gravel parking lot for Merritt's Store across Columbia Street. It is zoned NC. The properties to the west are zoned R-2 and developed as multi-family and single family residential. But these properties are on the west side of the stream and face small residential streets, they do not have direct access onto S Columbia Street or the bypass.

Achieving Goals of the Chapel Hill 2020 Plan

The site is not part of any special protection district or any targeted redevelopment area. The Chapel Hill 2020 Comprehensive Plan was adopted in June 2012 and lists 5 "big ideas" to "serve as beacons to guide the efforts of the Town and the community as Chapel Hill's future is created". The proposed project supports two of these ideas.

The first of these "big ideas" is to implement a bikeable, walkable, green communities plan by 2020. The best use of this site to contribute to that goal would be to have higher density use with tenants and residents more likely to use alternate forms of transportation other than single user car travel. The location of the project along a major transportation corridor with established transit service and an existing bike lane and sidewalk network will encourage the use of alternate transportation methods. An increased density of use on the site would add to the existing mostly single family land use to help provide a mixed-use community.

The fourth big idea pushes for an increase of workforce housing and student housing in the community. Single family housing on a difficult site that would be expensive to access and service would not be geared toward housing prices under the median. A single access serving multiple spaces would help divide the cost of servicing and accessing the parcel among many users, thereby making the built space more affordable for each individual user.

The proposed change in density and use will support many of the 2020 Plan goals which are the same goals as the previous 2000 Comprehensive Plan including:

- Conserving and protecting existing neighborhoods by increasing density on arterial streets which would provide a use buffer between busy streets and quieter residential neighborhoods
- Conserving and protecting natural areas in Chapel Hill by allowing higher density on portions of lots, thereby allowing the remainder of the lot to remain "green"
- Identifying areas of development opportunity
- Encouraging desirable non-residential development working toward a balanced transportation system and complete bicycle/greenway/sidewalk systems, and
- Providing quality facilities and services.

COLUMBIA STREET ANNEX

THE JOHN R. MCADAMS COMPANY, 3-9-98.

TREE SURVEY WAS COMPLETED BY COULTER

JEWELL THAMES PA, 4-26-13.

CONTACT: ROLAND GAMMON

1/C-1 EXISTING CONDITIONS

C H HOTEL ASSOCIATES LIMITED PARTNERSHIP 3008 ANDERSON DRIVE, SUITE 120 RALEIGH, NC 27609 919-821-4665

DAVID ROBERTS PO BOX EST FRANKLIN STREET CHAPEL HILL, NC 27516 919-929-5244

CONSULTANTS:

LANDSCAPE ARCHITECT/CIVIL ENGINEER: COULTER JEWELL THAMES, PA 111 WEST MAIN STREET DURHAM, NC 27701 919-682-0368 CONTACTS: WENDI RAMSDEN, CHUCK HILL

SZOSTAK DESIGN INC 310 ½ WEST FRANKLIN STREET CHAPEL HILL, NC 27516 919-929-5244 CONTACT: PHIL SZOSTAK



SZOSTAK

SZOSTAK DESIGN, INC. Chapel Hill, NC 27516 T 919 929 5244 F 919 960 7967

www.szostakdesign.com



111 West Main Street Durham, N.C. 27701 p 919.682.0368 f 919.688.5646 www.cjtpa.com

COLUMBIA STREET ANNEX

1150 South Columbia Street Chapel Hill, North Carolina

OWNER: CH HOTEL ASSOCIATES LLC. 21 GLENWOOD AVENUE, RALEIGH NC



If this di	rawing is not 24" z	x 36" it is a reduced print - refer to graphic so
	Date 24 Oct 19	For
	14 Feb 20	Town Review Revisions

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IRON PIPE SET

WATER LINE

WATER VALVE

WATER MTR.

EXIST TREE

SLOPES <15%

SLOPES >25%

SLOPES 15-25%

— SANITARY SEWER LINE

STORM DRAINAGE MANHOLE

MULTI FAMILY RESIDENTIAL

NOT FOR CONSTRUCTION DRAWINGS FOR LAND USE PLAN AMENDMENT SUBMITTAL

> SITE MAP **EXISTING CONDITIONS**

> > **C-1**

21 GLENWOOD AVENUE, RALEIGH NC

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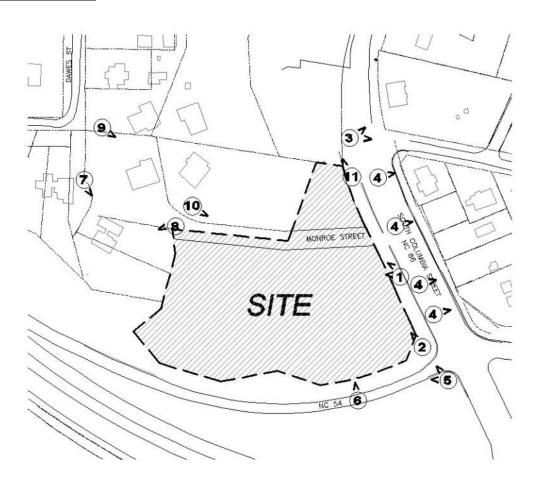
MAIN OFFICE 111 WEST MAIN STREET DURHAM, NC 27701 p919.682.0368 f919.688.5646

Planning for the Future

IMAGES OF SITE AND SURROUNDING AREA COLUMBIA STREET ANNEX – LAND USE PLAN AMENDMENT APPLICATION

1150 South Columbia Street October 24 2019

KEY – PHOTO LOCATIONS



SITE PHOTOS



(1) Project site, view from South Columbia Street



(2) Site as seen from South Columbia / NC 54 overpass.

NORTHEAST



(3) View of Merritt's Store and Purefoy intersection from project site.





(4) Houses across South Columbia Street facing project site. View from project site.

SOUTH





(5) View of NC54 westbound on-ramp from project site.



(6) View of project site from NC54 westbound on-ramp.

WEST





(7) Residential properties west of subject property. (8) Woods between residential property and project

.

NORTHWEST



(8) Rental properties northwest of project site.



(10) View into project site along Monroe right of way, from northwest, and view toward proposed building.

NORTH



(11) Crosswalk and gravel lot north of project site.

COLUMBIA STREET ANNEX

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



Prepared for:

The Town of Chapel Hill Engineering Department

Prepared by:

HNTB North Carolina, PC

343 East Six Forks Road Suite 200 Raleigh, NC 27609

NCBELS License #: C-1554

August 2014



COLUMBIA STREET ANNEX

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



Prepared for:

The Town of Chapel Hill Engineering Department

Prepared by:

HNTB North Carolina, PC

343 East Six Forks Road Suite 200 Raleigh, NC 27609

NCBELS License #: C-1554

August 2014





EXECUTIVE SUMMARY

Project Overview

A commercial/residential mixed-use development, known as the Columbia Street Annex, located along NC Highway 86 (S. Columbia Street) near its intersection with the NC Highway 54 Bypass is being proposed in Chapel Hill. The project proposes to construct 39 residential units and 7,150 square feet of office/retail space on two existing parcels on the west side of S. Columbia Street. **Figure ES-1** shows the general location of the site. The project is anticipated to be fully complete by 2016. This report analyzes the complete build-out scenario for the year 2017 (one year after anticipated completion), the no-build scenario for 2017, as well as 2014 existing year traffic conditions.

The proposed site concept plan shows a single full movement access driveway along S. Columbia Street. No other vehicular access connections are proposed. **Figure ES-2** displays the preliminary concept plan of the Columbia Street Annex and nearby land uses and roadways. The project is expected to provide 68 parking spaces on surface parking lots and an underground garage. This report analyzes and presents the transportation impacts that the Columbia Street Annex will have on the following intersections in the project study area:

- NC 86 (S. Columbia Street) and Purefoy Road/Proposed Site Driveway
- NC 86 (S. Columbia Street) and NC 54/US 15-501 Bypass Westbound Ramps
- US 15-501 and NC 54 Bypass Eastbound Ramps

The impacts of the proposed site at the study area intersections will be evaluated during the AM, noon, and PM peak hours of an average weekday. The following study is based on background traffic for the existing year, 2014, the year following the estimated site build out year of 2016, as well as the estimated site-generated traffic produced by the mixed-use development.

Existing Conditions

Study Area

The site is located in southern Chapel Hill along NC 86 (S. Columbia Street) in the northwest quadrant of its interchange with the NC 54 Bypass. The study area contains two signalized intersections at the interchange ramp terminals. It also includes the existing unsignalized stop-controlled intersection at Purefoy Road adjacent to the site.

NC 86 (S. Columbia Street) is a major arterial facility providing connectivity between downtown Chapel Hill and the UNC Main Campus with the NC 54 Bypass and US 15-501 corridors. Remaining study area network roadways are either interchange access ramps or local neighborhood access streets.

Site Traffic Generation

With the addition of new peak hour trips during the weekday AM, noon, and PM peak hours, there are potential site traffic impacts to the study area intersections. **Table ES-1** shows the site trip generation details, with generation rates and methodologies taken from the *Institute of Transportation Engineers (ITE) Trip Generation Manual, Version 9.*



Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

Table ES-1 Weekday Vehicle Trip Generation Summary Columbia Street Annex

	Daily			AM Peak Hour			Noon Peak Hour			PM Peak Hour		
Trip Generation Statistic	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
Raw Total	333	333	666	9	21	30	23	25	48	38	28	66
Transit/Ped/Bike Reduction	-50	-50	-100	-1	-3	-5	-3	-4	-7	-6	-4	-10
Vehicular Driveway Trips	283	283	566	8	18	25	20	21	41	32	24	56
Pass-By Trips (Retail Only)	-26	-26	-52	-0	-0	-0	-2	-3	-5	-4	-5	-9
New Vehicular Trips Added to Network	257	257	514	8	18	25	18	18	36	28	19	47

^{* -} No ITE Data Available, Assumed Average of AM/PM Peak Hour Data

Background Traffic

Background traffic growth for the 2017 analysis year is expected to come from two sources - ambient regional traffic growth and specific development-related traffic growth. Based on existing information, two Town-approved or recently completed development projects near the project study area are planned to be contribute to background traffic growth by the 2017 analysis year. All remaining estimated traffic growth in the area is assumed to occur due to overall region-wide ambient growth. To account for this, an ambient area-wide traffic growth percentage of 1.2 percent per year was applied to existing traffic volumes based on information from the historic daily traffic growth patterns in the project study (NCDOT and Town of Chapel Hill daily traffic information) and consistent with recent traffic impact studies completed near the project study area.

Impact Analysis

Peak Hour Intersection Level of Service (LOS)

Study results indicate existing traffic operations at all study area intersections are acceptable during the AM and Noon peak hours. In the PM peak hour, the signalized ramp terminal intersections at the NC 54 Bypass operate at a LOS F given current traffic levels and signal timing information. Retiming these signals for anticipated 2017 traffic volumes improves the PM peak deficient operations. Even with the addition of peak hour site-generated trips to the projected 2017 background traffic volumes, no additional study area intersections are expected to experience deficient traffic operations in any peak hour. However, capacity analysis results, if recommended improvements at Purefoy Road and the proposed site driveway are implemented, show a decline in worst-case stop controlled delay and LOS, but this is offset by potential safety improvements in this area.

A summary of the traffic operations for each intersection, related to vehicular delays (intersection average as a whole if signalized, critical movement if stop-controlled) and the corresponding Level-of-Service (LOS) is shown in **Table ES-2** on the following page.

Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

Table ES-2. Columbia Street Annex LOS and Delay (Seconds/Vehicle) Summary

	Peak	2014 Existing		2017 No-Build		2017 Build		2017 Mitigated	
Intersections	Hour	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
NO 00 (0, 0, 1, 1; 0)	AM	D	27.0	D	29.4	D	30.1	E**	36.5**
NC 86 (S. Columbia St.) & Purefoy Road	NOON	С	15.3	С	16.1	С	16.4	C**	20.7**
a r dreiby rtodd	PM	E	36.1	E	42.5	E	44.4	F**	93.1**
NC 86 (S. Columbia St.)	AM	С	27.0	С	22.8	С	22.8	N/A	N/A
& NC 54 Bypass	NOON	С	23.6	С	21.5	С	21.6	N/A	N/A
Westbound Ramps	PM	E	68.9	D	52.7	D	52.8	N/A	N/A
US 15-501 &	AM	С	34.4	С	24.6	С	24.6	N/A	N/A
NC 54 Bypass	NOON	Α	9.9	В	11.6	В	11.6	N/A	N/A
Eastbound Ramps	PM	F	93.0	В	15.2	В	15.2	N/A	N/A
NO 00 (0 0 1 1: 01)	AM	N/A	N/A	N/A	N/A	C*	21.1*	N/A	N/A
NC 86 (S. Columbia St.) & Site Driveway*	NOON	N/A	N/A	N/A	N/A	B*	14.8*	N/A	N/A
a one briveway	PM	N/A	N/A	N/A	N/A	D*	28.2*	N/A	N/A

N/A – Not Applicable or No Improvements Necessary

BOLD/ITALICS - Critical Movement or Overall Intersection Requires Mitigation Per Town TIS Guidelines

Access Analysis

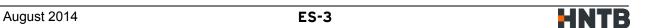
Vehicular site access is to be accommodated by a single full movement access driveway connecting to S. Columbia Street about 425 feet to the north of its signalized intersection with the NC 54 Bypass westbound ramps. Design details related to driveway throat length are shown on the concept plan and assume an approximate 125 foot driveway throat at this driveway. An internal driveway circulation system to all surface and underground parking areas is also shown on the plans.

Driveway distances along S. Columbia Street from the signalized intersections at the NC 54 Bypass Westbound Ramps and Mason Farm Road (to the north of the study area) are acceptable, based on current standards. The proposed spacing between the proposed driveway and adjacent existing driveways (several that are approximately 250-500 feet north of the proposed driveway) is less than the recommended 750 foot spacing along arterial roadways found in Table 4-A-1 in the Town Design Manual.

Access for pedestrians and bicyclists is adequate in the project study area. Sidewalk is present on both sides of S. Columbia Street adjacent to the site and exists along the major street connections along S. Columbia Street. Crosswalk and pedestrian signals exist across the three major signalized study area intersections near the Columbia Street Annex site. Specific bicycle amenities are present along S. Columbia Street, with striped lanes present in both directions.

Crash Analysis

Data from the NCDOT Traffic Safety Unit was provided for recent three and five-year periods for all existing major study area intersections. The primary crash type at all locations was rear-end



^{* -} Worst-Case LOS/Delay for Unsignalized/Stop-Controlled Critical Movement

^{** -} Results for Realigned Site Driveway to Form Fourth Leg of Intersection

Town of Chapel Hill: Traffic Impact Study *Columbia Street Annex* - Proposed Mixed-Use Development

crashes. Overall, the number and severity of crashes are low compared to other locations around Chapel Hill.

Other Transportation-Related Analyses

Other transportation-related analyses relevant to the 2001 Town of Chapel Hill Guidelines for the preparation of Traffic Impact Studies were completed as appropriate. The following topics listed in **Table ES-3** on the following page are germane to the scope of this study.

Table ES-3. Other Transportation-Related Analyses

Analysis	Comment
Long-Range Planning Level Daily Volume- Capacity Analysis	Due to the fact that the proposed site will add less than 600 daily trips to the study area network, no long-range daily v/c analysis was conducted for this study.
Turn Lane Storage Requirements	Storage bay lengths at study area intersections were analyzed using Synchro and HCS 95 th percentile (max) queue length estimates for the 2017 Build Scenario. The S. Columbia Street/NC 54 Bypass westbound ramps intersection westbound approach has existing and projected queues that exceed its current storage bays that may need additional geometric improvement in the future.
	Realignment of the proposed site driveway with Purefoy Road, along with some minor geometric changes to the Purefoy Road approach may allow the creation of a short left-turn storage area (1-2 vehicles) that may help operations at this intersection. No other recommendations for improvements to storage bays are expected, based on the analysis results.
Appropriateness of Acceleration/ Deceleration Lanes	The site concept plan shows no specifics related to acceleration/deceleration lanes, other than the creation of an exclusive northbound left-turn lane on S. Columbia Street. Based on existing cross-section details with the U-0624 project, corridor speeds on S. Columbia Street and capacity analysis results, no specific acceleration or deceleration lanes are recommended at proposed site access point. No other specific acceleration/deceleration lane issues were analyzed in the project study area.
Pedestrian and Bicycle Analysis	Existing pedestrian and bicycle access and connectivity is adequate along the S. Columbia Street corridor adjacent to the site. Sidewalk exists along the NC 86 and US 15-501 corridors and pedestrian crossings and signals are present on at least one quadrant of signalized intersections. An additional mid-block crosswalk exists 250 feet to the north of the proposed site along S. Columbia Street. Delineated bicycle lanes and wide outside lanes are present in the project study area. The Morgan Creek Trail Greenway has access in the area just to the south of the NC 54 Bypass, providing additional bicycle and pedestrian connectivity to the area.
Public Transportation Analysis	Public transportation service to the study area, and to the proposed site, is excellent with multiple bus stops and multiple local and regional bus routes on S. Columbia Street proximate to the site.





Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

<u>Mitigation Measures/Recommendations</u>

Planned Improvements

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2014-2017.

Background Committed Improvements

There are no specific geometric or operational improvements to study area roadway intersections or facilities related to background private development projects that are expected to be completed between 2014 and 2017. To make an comparative assessment of potential improvements to signal timing (by optimizing signal cycle lengths, splits and offsets for projected 2017 traffic volumes) it was assumed that signal timing reoptimization would occur for the S. Columbia Street corridor by the year 2017 with or without the development of the Columbia Street Annex project.

Applicant Committed Improvements

Based on the preliminary site concept plans and supporting development information provided, there are the following specific external transportation-related improvements proposed adjacent to the Columbia Street Annex:

- Provision of a northbound left-turn lane and pavement marking modifications to the existing NC 86 (S. Columbia Street) southbound additional inside through travel lane that develops south of Purefoy Road.
- Provision of a crosswalk in this vicinity.
- Provision of continuous sidewalk along site frontage.

Necessary Improvements

Based on traffic capacity analyses for the 2017 design year, and analyses of existing study area turning bay storage lengths and site access, the following improvements are recommended as being necessary for adequate transportation network operations (see **Figures ES-3 and 4**).

1) Retime the NC 54 Bypass interchange ramp terminal intersections to optimize overall capacity given the existing intersection geometrics and progression along NC 86 (S. Columbia Street) and US 15-501. Reoptimizing these two intersections with the rest of the US 15-501 corridor south of the interchange may improve operations at the signalized ramp terminals for 2017 build-out+1 year conditions, particularly in the PM peak hour.

The proposed signal timing improvements are recommended in order to mitigate existing system operational deficiencies and are recommended whether or not the Columbia Annex project is constructed.

2) To provide additional storage capacity for the NC 54 Bypass westbound off-ramp for 2017 build-out+1 year anticipated conditions, the existing inside left-turn lane storage bay at this approach to NC 86 (S. Columbia Street) should be lengthened from 275 feet to 650 feet. This will allow improved traffic flow during this signal phase and will improve the overall safety and efficiency of traffic flow at this intersection.





Town of Chapel Hill: Traffic Impact Study *Columbia Street Annex* - Proposed Mixed-Use Development

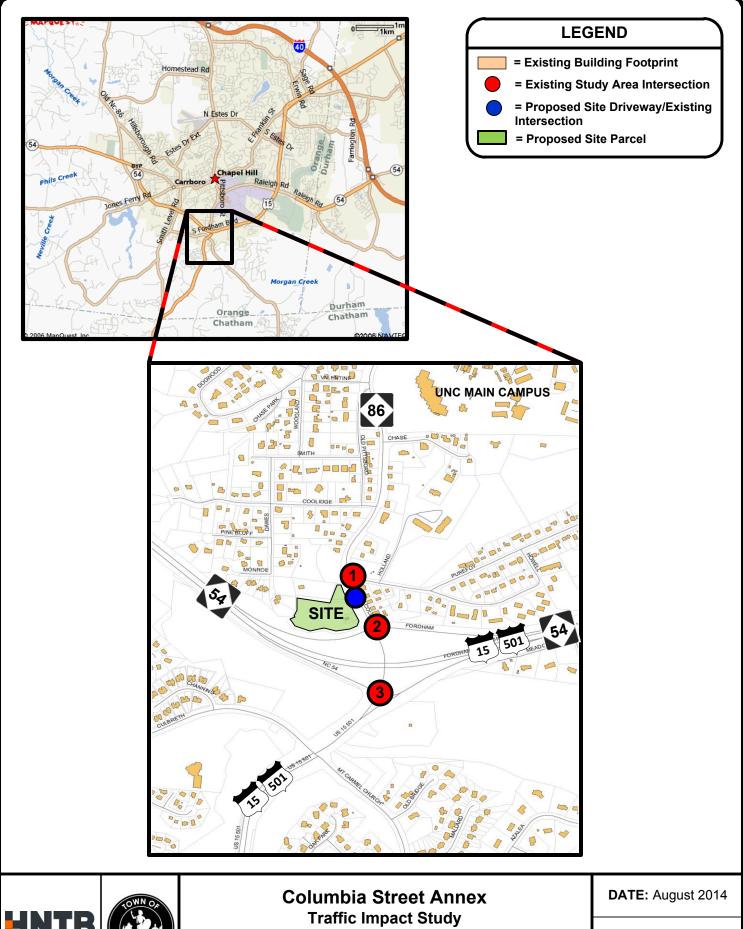
This improvement is recommended based on anticipated queue storage needs in 2017 with or without Columbia Street Annex site traffic impacts.

3) Due to the fact that the proposed site access driveway does not align with the existing Purefoy Road on the opposite side of NC 86 (S. Columbia Street), it is recommended that the Applicant consider investigating the feasibility for realigning the proposed site access to immediately opposite Purefoy Road. This improvement would not necessarily reduce the number of vehicular conflict points in this vicinity, but it would prevent left-turning movements from conflicting with each other from the driveway and Purefoy Road, and it would improve sight lines from the minor street approaches and provide a more orderly means of judging and accepting the limited available gaps in the high volume S. Columbia Street traffic pattern. In addition to this improvement, additional minor geometric modifications to this newly realigned intersection (see **Figure 14**) may enhance traffic operations and safety, as well as provide for the maximum amount of southbound through traffic storage at the adjacent NC 54 Bypass westbound off-ramp intersection, and provide for a short westbound left-turn pocket on Purefoy Road.

Capacity analysis results indicate that, though aligning the site driveway and Purefoy Road produces longer projected vehicular delays than if the driveway and Purefoy Road were operating as separate three-legged intersections with NC 86 (S. Columbia Street), in reality, there would be little safe opportunity for left-turning traffic from the site driveway and Purefoy Road to use the two-way left-turn lane refuge area (that is in actuality becomes a southbound through lane for NC 86). Aligning the driveway and Purefoy Road provides better clarity for delineation of acceptable gaps and turning movements. Maximum queue length results for the most critical approach (westbound left-turns) indicate there would be little difference (3-4 vehicles maximum queue) between the existing proposed offset configuration and the realigned configuration.

This improvement is recommended specifically for the Columbia Street Annex project.



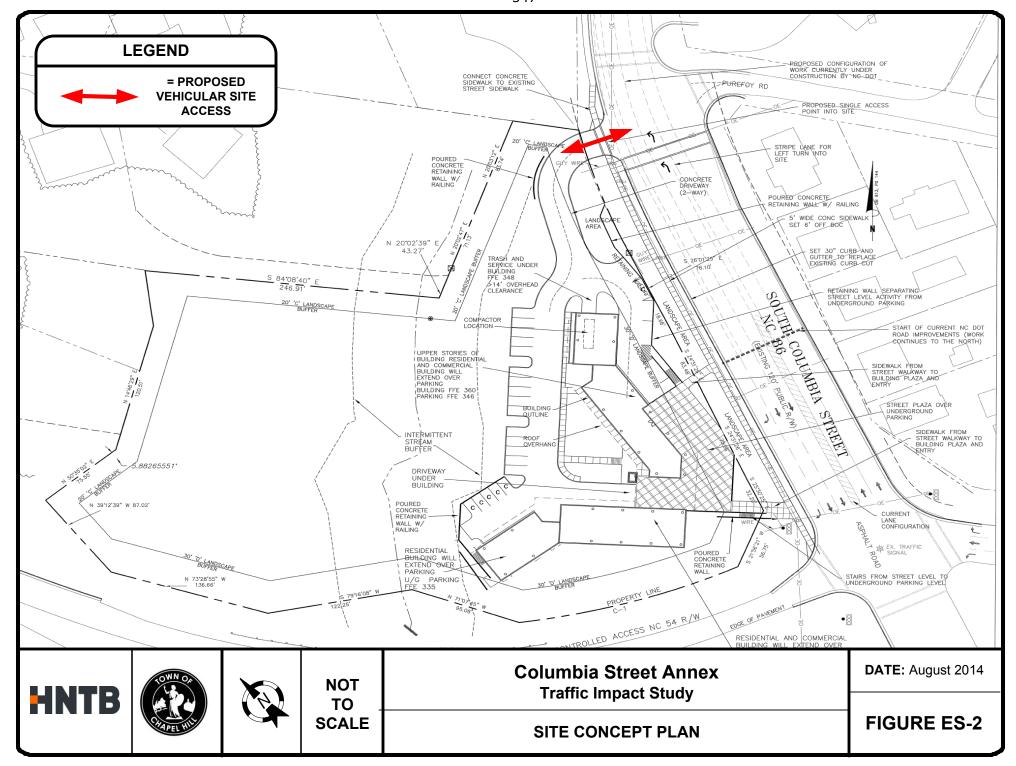


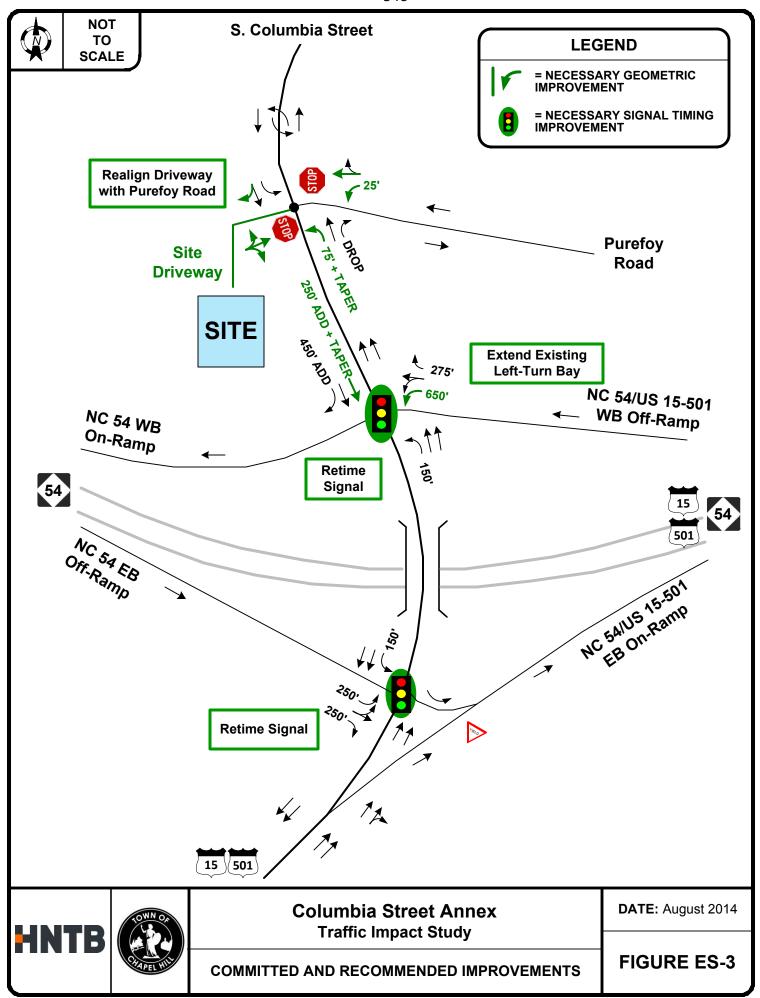
PROJECT STUDY AREA MAP

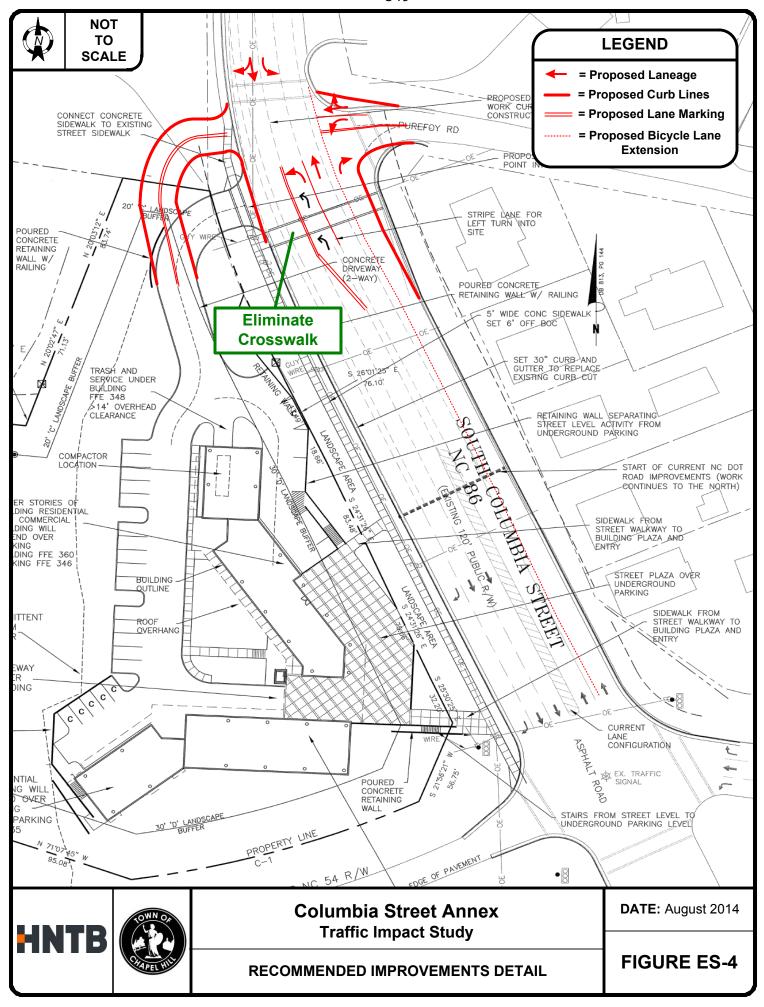
FIGURE ES-1











COLUMBIA STREET ANNEX

1150 SOUTH COLUMBIA STREET, CHAPEL HILL, NC 27514

SPECIAL USE PERMIT APPLICATION - 2018/20

PROJECT DATA

EXISTING USE VACANT RESIDENTIAL

MIXED USE - RESIDENTIAL, COMMERCIAL, RETAIL PROPOSED USE USE GROUP

9788204502 PARCEL 2 9788205716 PARCEL 3 9788206500

131,340.04 SF - PARCEL EXISTING LAND AREA

> <u> 6,967.75 SF - MONROE ST R/W</u> 160,990.47 SF TOTAL NET LAND AREA

51,769 SF net

POST RECOMBINATION AREAS: LAND AREA IN RCD managed use zone LAND AREA IN RCD upland zone

177,090 SF / 4.065 ACRES

1.64 ACRES NET / 1.804 AC GROSS PROJECT AREA

DISTURBED AREA 1.64 ACRES

EXISTING ZONING PROPOSED ZONING

1.2 x 86,437 = 103,724 sf MU-V ARTERIAL $RCD - streamside 0.01 \times 35,935 = 359 sf$

 $RCD - managed use 0.019 \times 38,618 = 734$

104,817 SF MAXIMUM BUILDING SIZE

BASED ON GROSS LAND AREA ~~~~~

EXISTING BUILDING SIZE 57.000 SF residential PROPOSED BUILDING SIZE 4,000 SF general business

maximum 61,000 SF total (incl mechanical, excl underground parking

 $\sim\sim\sim\sim$

1 SPACE/300 SF TO 1 SPACE /200 SF

MIX OF STUDIO / 1 BDRM /2 BDRM

NON-RESIDENTIAL SPACE IS INTENDED

FOR USE BY RESIDENTS FOR WORK/LIVE

4,000 SF GENERAL BUSINESS

= 13 MIN 20 MAX

1.0 TO 1.75 PER UNIT

USE. THERE IS NO PARKING

NON-RESIDENTIAL SPACE.

SPECIFICALLY RESERVED FOR THE

= 52 MIN 91 MAX

52 RESIDENTIAL UNITS

RESIDENTIAL: maximum 52 UNITS TOTAL (15% WILL BE DESIGNATED AFFORDABLE)

PROPOSED FAR

REQUIRED PARKING

BASED ON MAXIMUM BLDG SIZE MIN 65 SPACES, MAX 111 SPACES PROPOSED PARKING 69 SPACES INCLUDING:

3 VAN ACCESSIBLE ADA (all in underground parking) 21 STANDARD SURFACE SPACES

17 UNDERGROUND SPACES INCLUDING APPROX. 4 COMPACT 28 UNDERGROUND GARAGE (DOUBLE STACKED)

PROPOSED CAR CHARGING STATIONS 1 LOCATED IN UNDERGROUND GARAGE SERVING 2 SPACES (3% OF PARKING)

EXISTING IMPERVIOUS 48,950 SF PROPOSED IMPERVIOUS

REQUIRED BICYCLE PARKING GENERAL BUSINESS: 4 + 2 per 2500 sf = 6 SPACES

> (20% CLASS | AND 80% CLASS ||) RESIDENTIAL: 1 per 4 units = 13 SPACES

(90% CLASS I AND 10% CLASS II)

TOTAL BIKE PARKING REQUIRED WILL MEET LUMO REQUIREMENT

LOCATIONS: UNDERGROUND PARKING, SURFACE PLAZA, IN RESIDENTIAL UNITS PROPOSED BICYCLE PARKING

EXISTING TREE CANOPY 134,000 SF = 80.8% OF SITE

REQUIRED TREE CANOPY 40% = 58,724 SF CALCULATION ON SHEET SUP-7.0 PROPOSED TREE CANOPY 66,331 SF (EXISTING RETAINED COVERAGE)

REQUIRED RECREATION SPACE

RECREATION SPACE PROVIDED

0.046 RSR

GROSS LOT AREA IS 177.090 SF

PROJECT AREA IS 71,450 SF

RESIDENTIAL PERCENTAGE OF PROJECT IS 93.4% PROJECT AREA x RECREATION MULTIPLIER x PERCENTAGE RESIDENTIAL

= 71,450 x .046 x .934 = 3,070 SF PAYMENT IN LIEU IS \$12/SF

3,070 SF ACTIVE RECREATION SPACE PROVIDED AS FITNESS CENTER IN THE BUILDING FOR

SHORTFALL IN FINAL INTERIOR FITNESS SPACE SQUARE FOOTAGE WILL BE MADE UP AS

PAYMENT IN LIEU AT \$12/SF

SILE CULBRETH RD TO SOUTHERN VILLAGE VICINITY MAP NTS

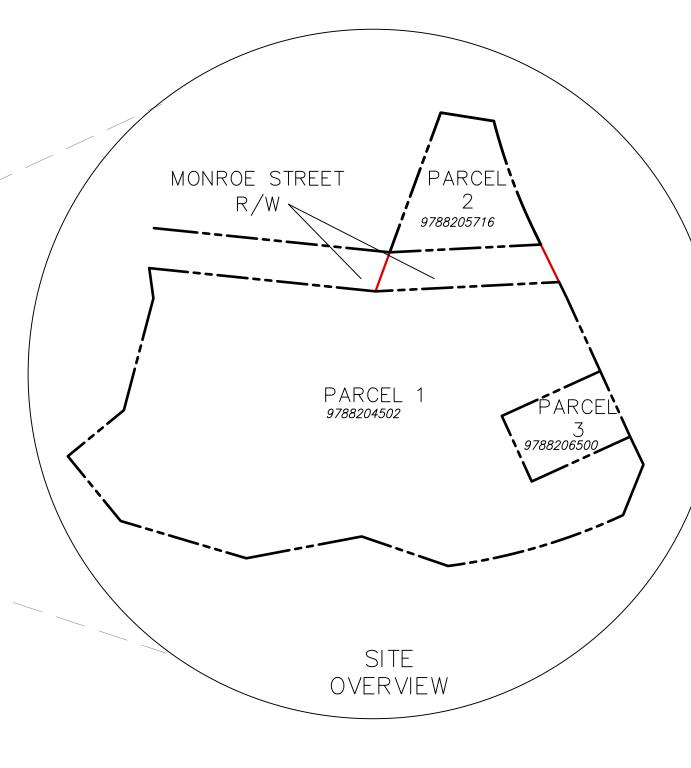
TO UNC AND

DOWNTOWN



- PARCELS MUST BE RECOMBINED PRIOR TO ZCP APPROVAL.
- THE BUILDINGS ARE REQUIRED TO BE SPRINKLED.
- 3. LAYOUT IS BASED ON A RIGHT-IN / RIGHT-OUT DRIVEWAY CONFIGURATION.
- THE APPLICANT MUST SUBMIT A CONSTRUCTION MANAGEMENT PLAN FOR APPROVAL BY THE TOWN PRIOR TO ISSUANCE OF A ZONING COMPLIANCE PERMIT.
- THE PROJECT WILL MEET THE TOWN'S 15% AFFORDABLE HOUSING REQUIREMENT,
- WITH THE PROVISION OF 7 AFFORDABLE UNITS PROVIDED IN THE BUILDING. CONTRACTORS MUST COMPLETE THE TOWN'S TREE PROTECTION SEMINAR PRIOR
- TO TREE PROTECTION FENCE INSTALLATION. CONTACT ADAM NICHOLSON, URBAN FORESTRY, 919-969-5006.
- DEVELOPER WILL BE REQUIRED TO INSTALL ADA RAMPS AND PEDESTRIAN ACTIVATED LED SIGNAL AT THE EVENTUAL SOUTH COLUMBIA STREET CROSSWALK LOCATION IN FRONT OF OR JUST NORTH OF THE PROJECT SITE.

REVISED S COLUMBIA STREET TRAFFIC COUNTS WILL BE REQUIRED AT ZCP SUBMITTAL.

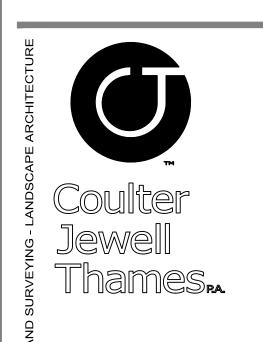


LIST OF SHEETS

	COVER SHEET
SUP-0.1	AREA MAP
SUP-1.0	EXISTING CONDITIONS, TREE SURVEY,
	DEMOLITION, AND TREE PROTECTION PLAN
SUP-1.1	SITE ANALYSIS
SUP-1.2	TREESAVE PLAN
SUP-2.0	SITE PLAN - OVERALL
SUP-2.1	SITE PLAN - UNDERGROUND PARKING
SUP-3.0	GRADING PLAN
SUP-3.1	SITE / RCD SECTIONS
SUP-3.2	DRIVEWAY/PARKING SECTIONS
SUP-3.3	STORMWATER MAINTENANCE PLAN
SUP-4.0	EROSION CONTROL PLAN
SUP-5.0	UTILITY PLAN
SUP-6.0	NOT USED
SUP-7.0	LANDSCAPE AND TREESAVE PLAN
SUP-8.0	SITE DETAILS
SUP-8.1	STORMWATER DETAILS
SUP-8.2	STREAM RESTORATION DETAILS
SUP-8.3	SITE DETAILS
A-5.1	ELEVATIONS - EAST, WEST
A-5.2	ELEVATIONS - NORTH, SOUTH
A-5.3	ZONING ENVELOPE DIAGRAM

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COLUMBIA STREET ANNEX

1150 South Columbia Street Chapel Hill, North Carolina

CH HOTEL ASSOCIATES LLC. 3008 ANDERSON DRIVE, SUITE 120 RALEIGH NC 27609



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Date	For						
<u>24 May 18</u>	Addition of RCD data						
20 May 19	Review Comment Revisions						
24 Oct 19	Review Comment Revisions						
14 Feb 20	Review Comment Revisions						
11 May 20	Review Comment Revisions						
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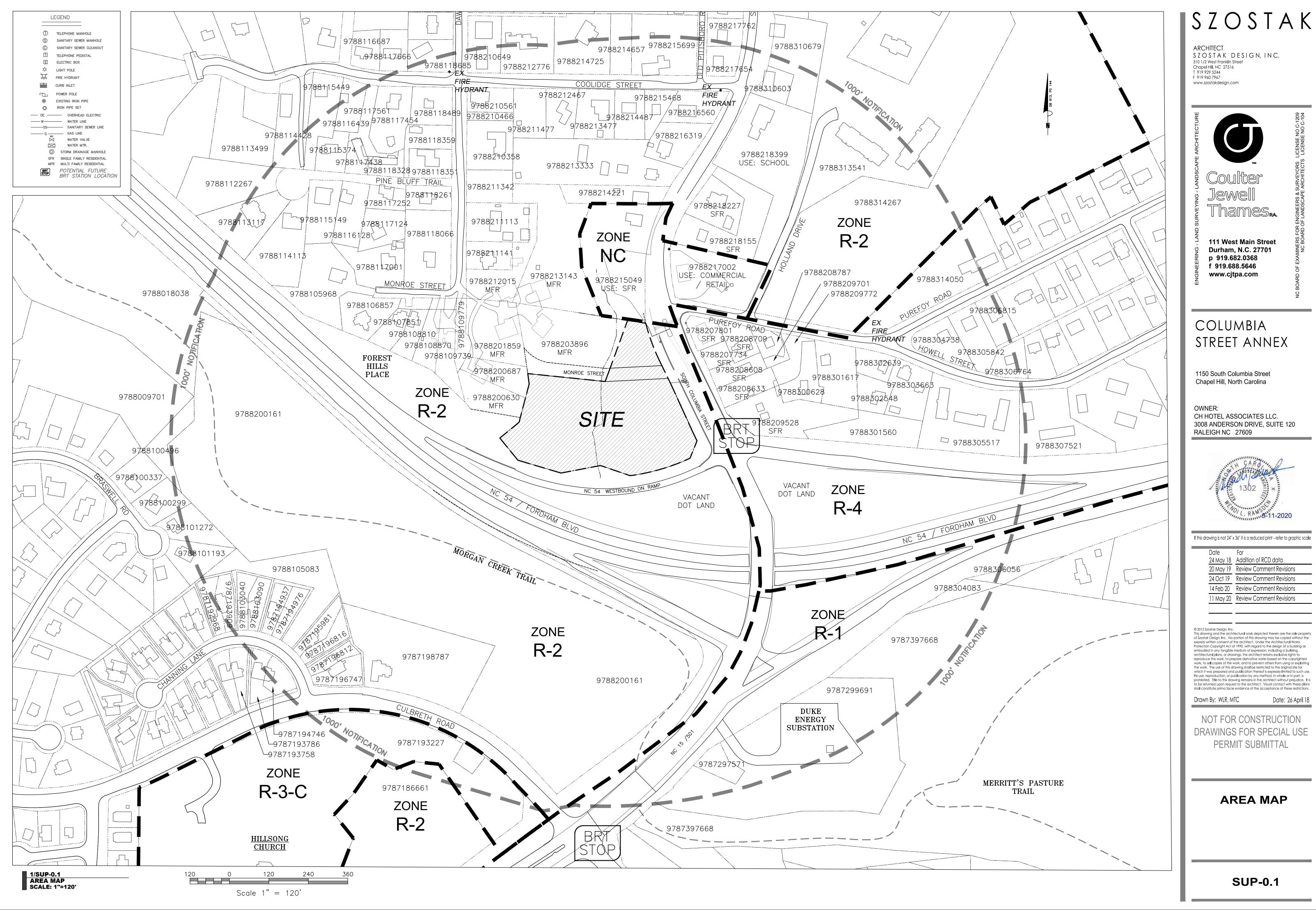
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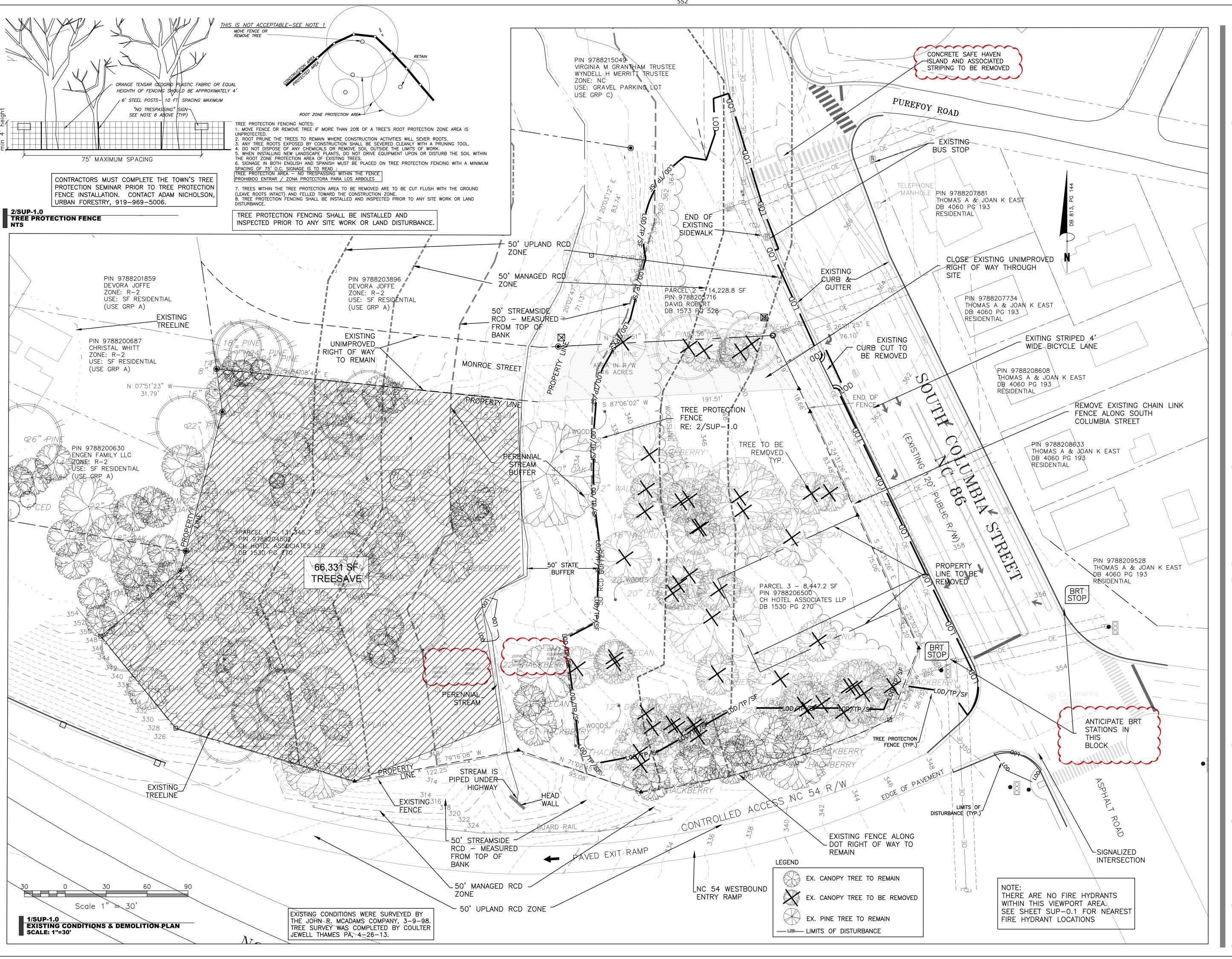
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COVER

SUP-0.0



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EXISTING
CONDITIONS, TREE
SURVEY, AND
DEMOLITION PLAN

SUP-1.0

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> SITE **ANALYSIS**

> > **SUP-1.1**

D STORM DRAINAGE MANHOLE SFR SINGLE FAMILY RESIDENTIAL

MFR MULTI FAMILY RESIDENTIAL

SLOPES >25%



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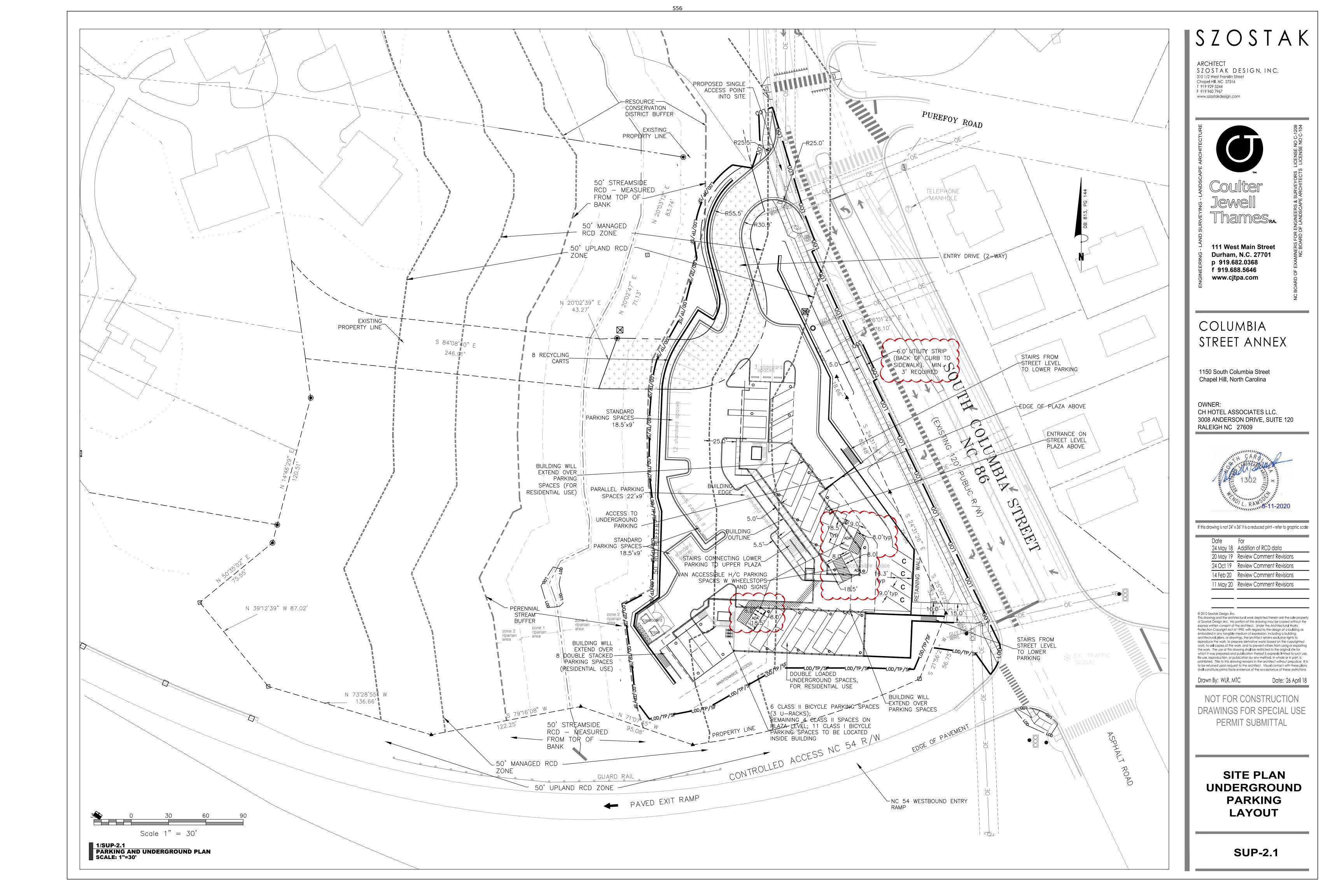
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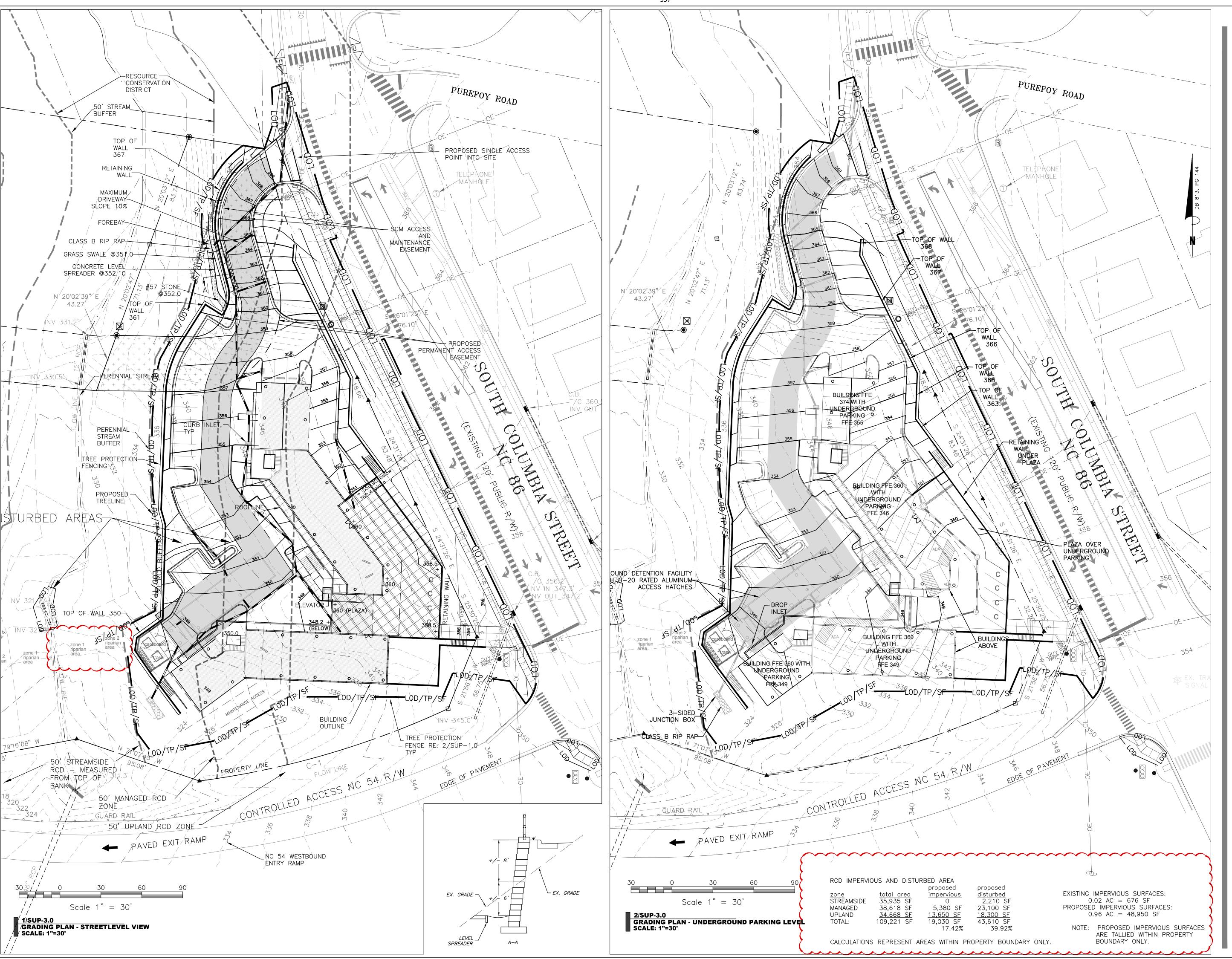
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OVERALL
SITE PLAN
STREET LEVEL
BUILDING PLAN

SUP-2.0





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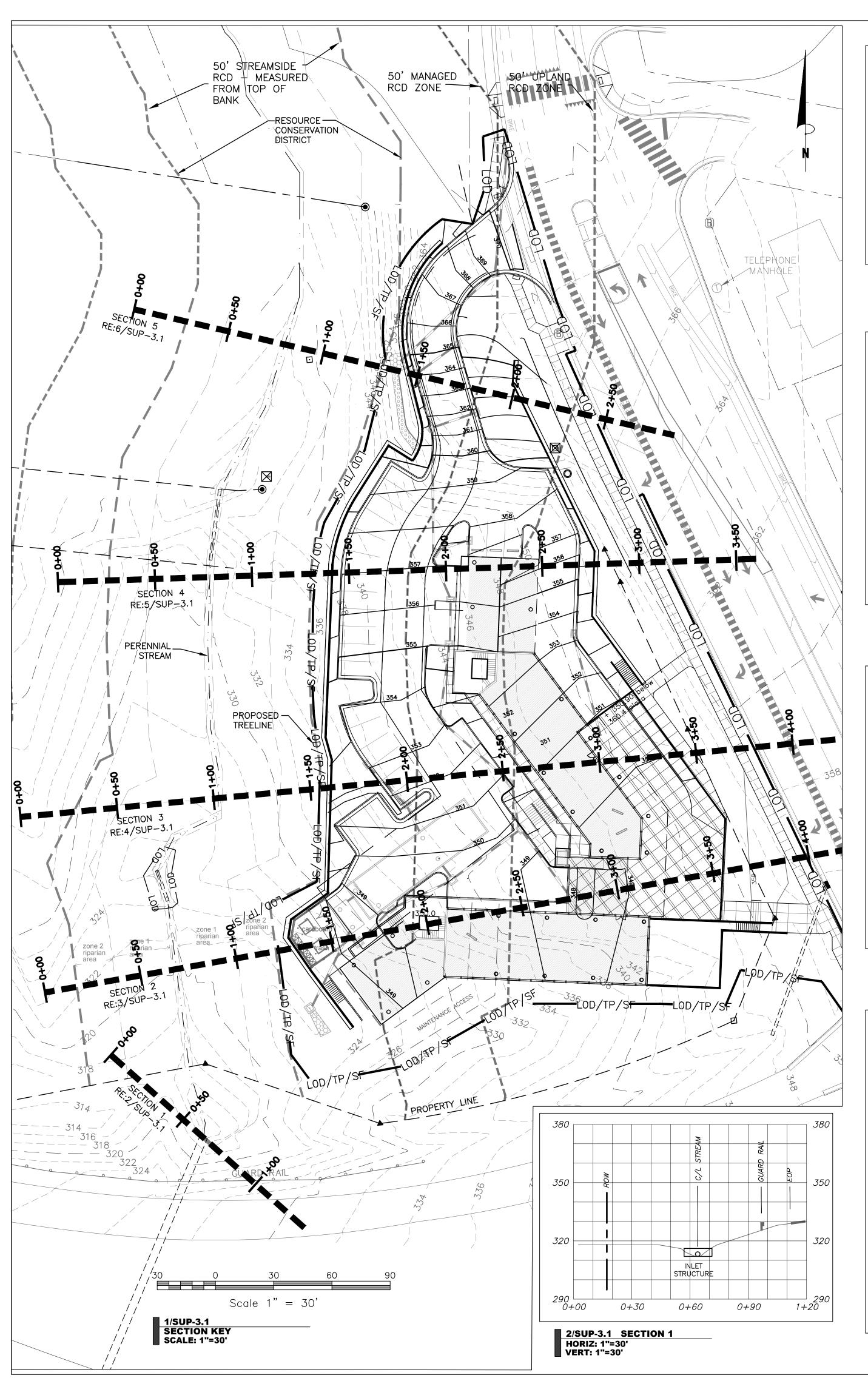
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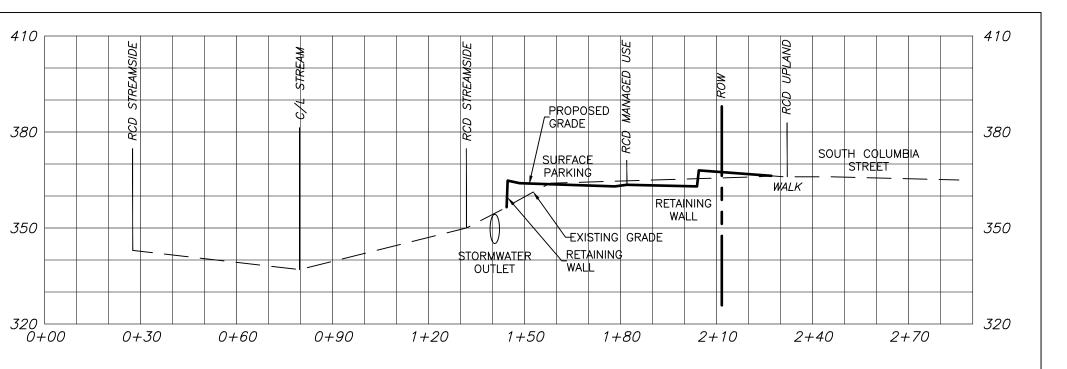
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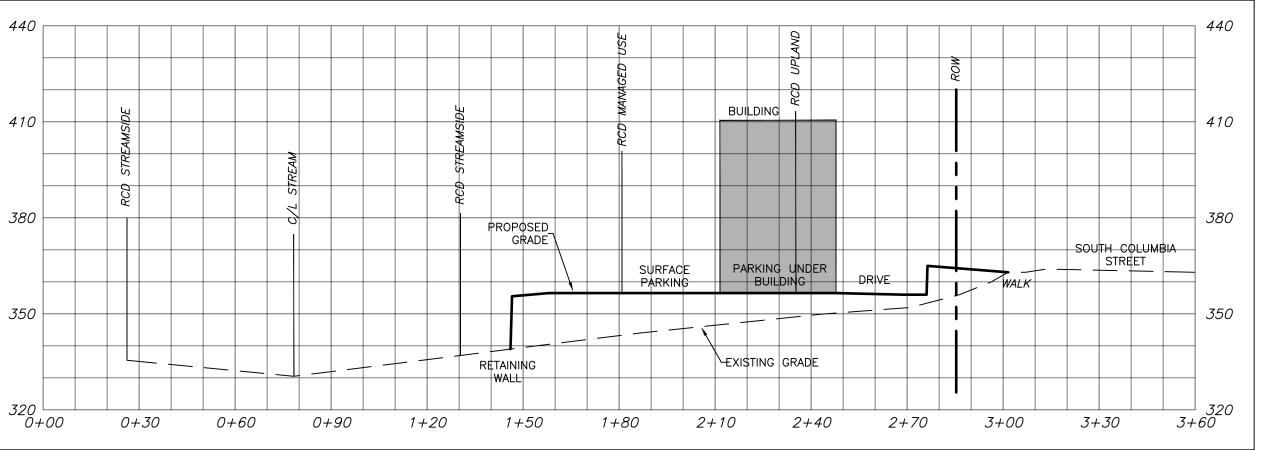
GRADING, TREE PROTECTION AND STORMWATER PLAN

SUP-3.0

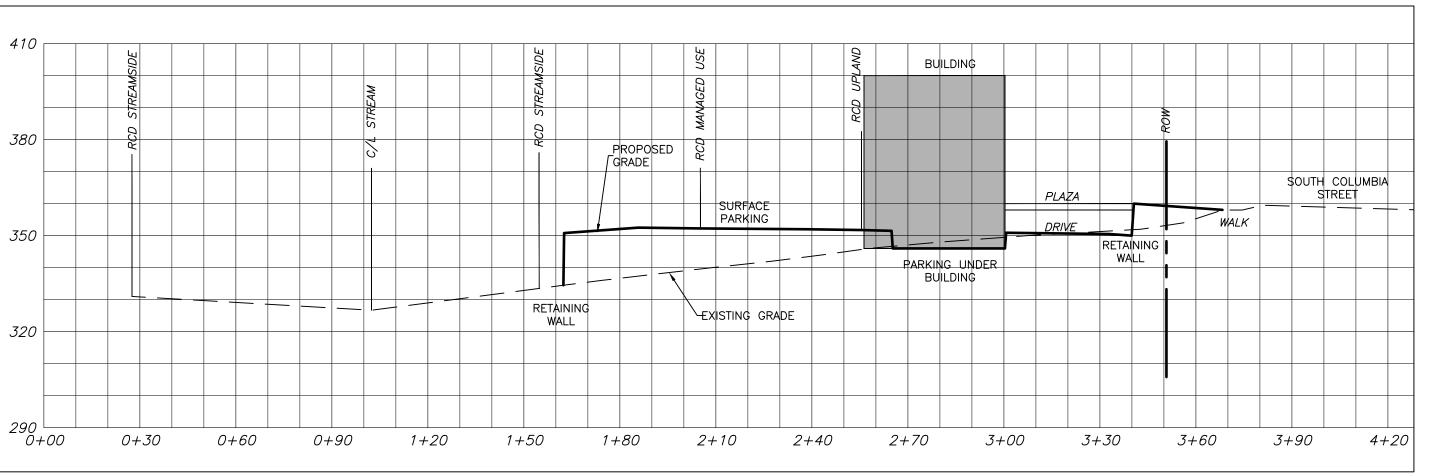




6/SUP-3.1 SECTION 5 HORIZ: 1"=30' VERT: 1"=30'

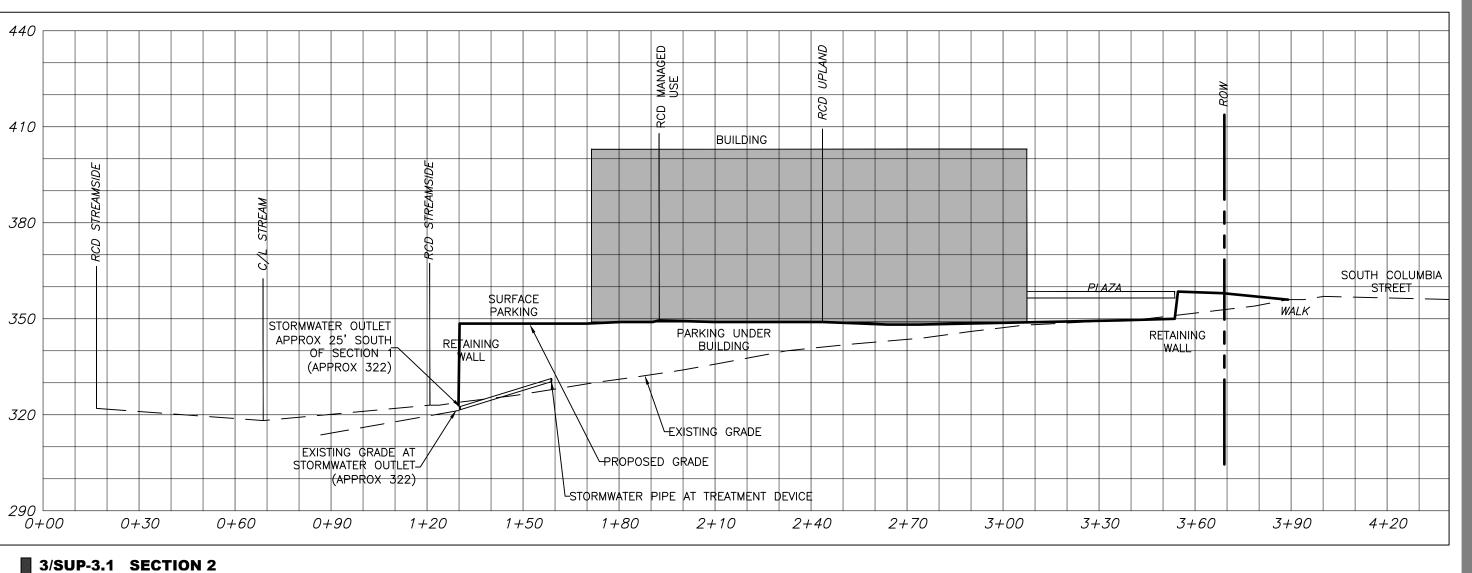


5/SUP-3.1 SECTION 4
HORIZ: 1"=30'
VERT: 1"=30'



4/SUP-3.1 SECTION 3 HORIZ: 1"=30' VERT: 1"=30'

HORIZ: 1"=30' VERT: 1"=30'



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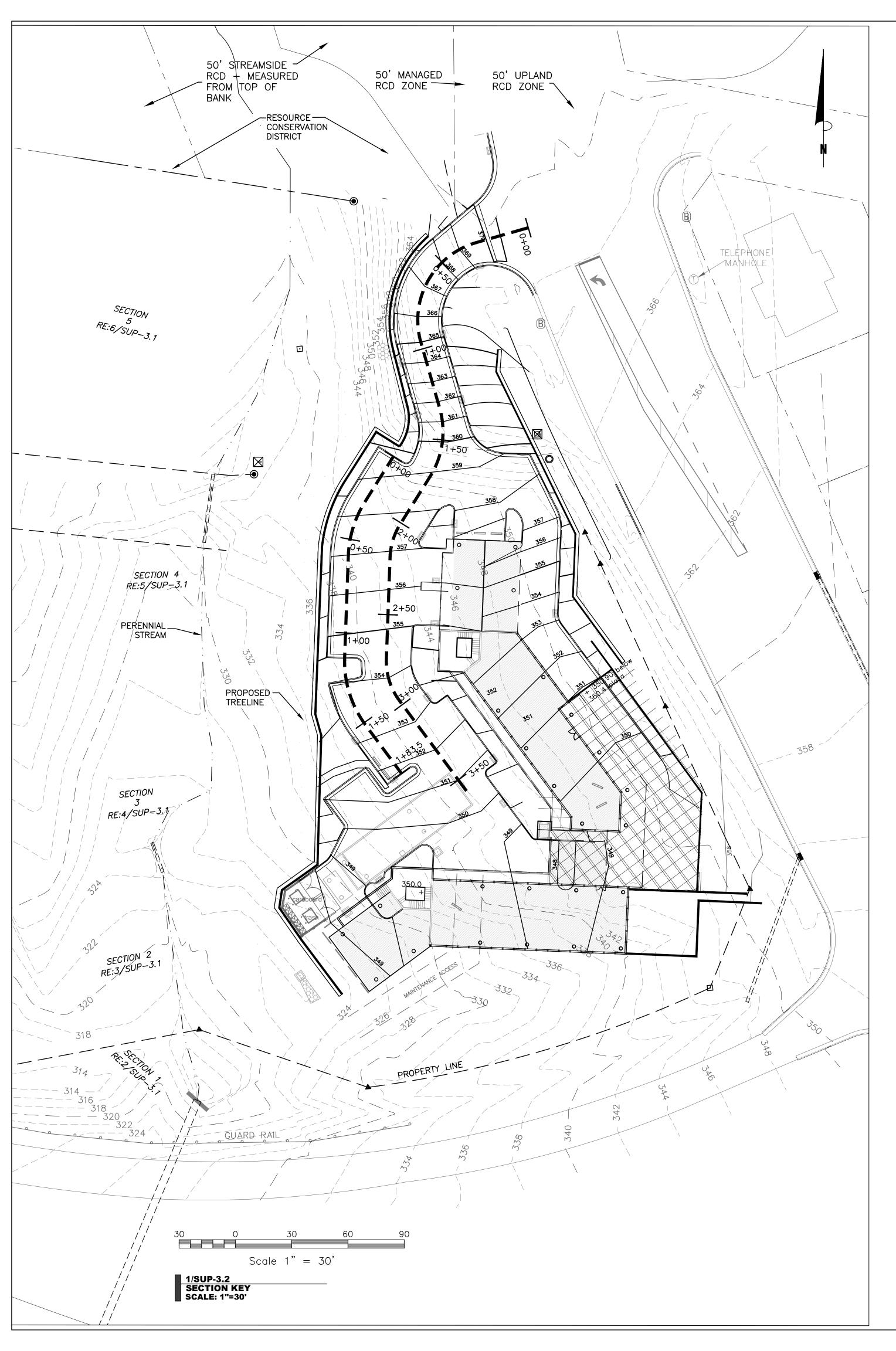
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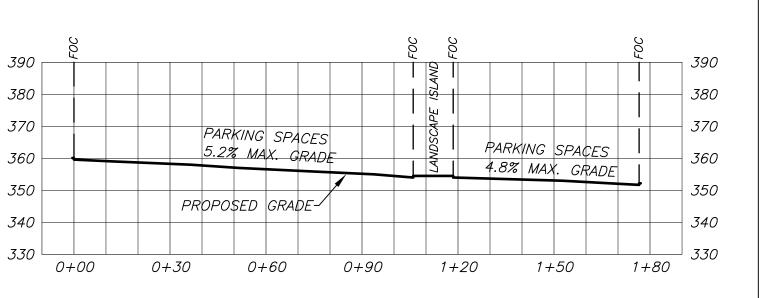
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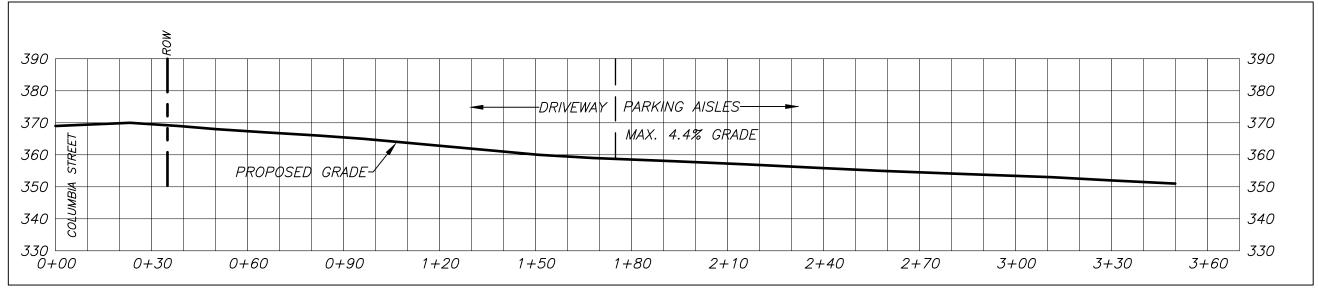
SITE / RCD SECTIONS

SUP-3.1





2/SUP-3.2 SECTION 1 HORIZ: 1"=30' VERT: 1"=30'



3/SUP-3.2 SECTION 2
HORIZ: 1"=30'
VERT: 1"=30'

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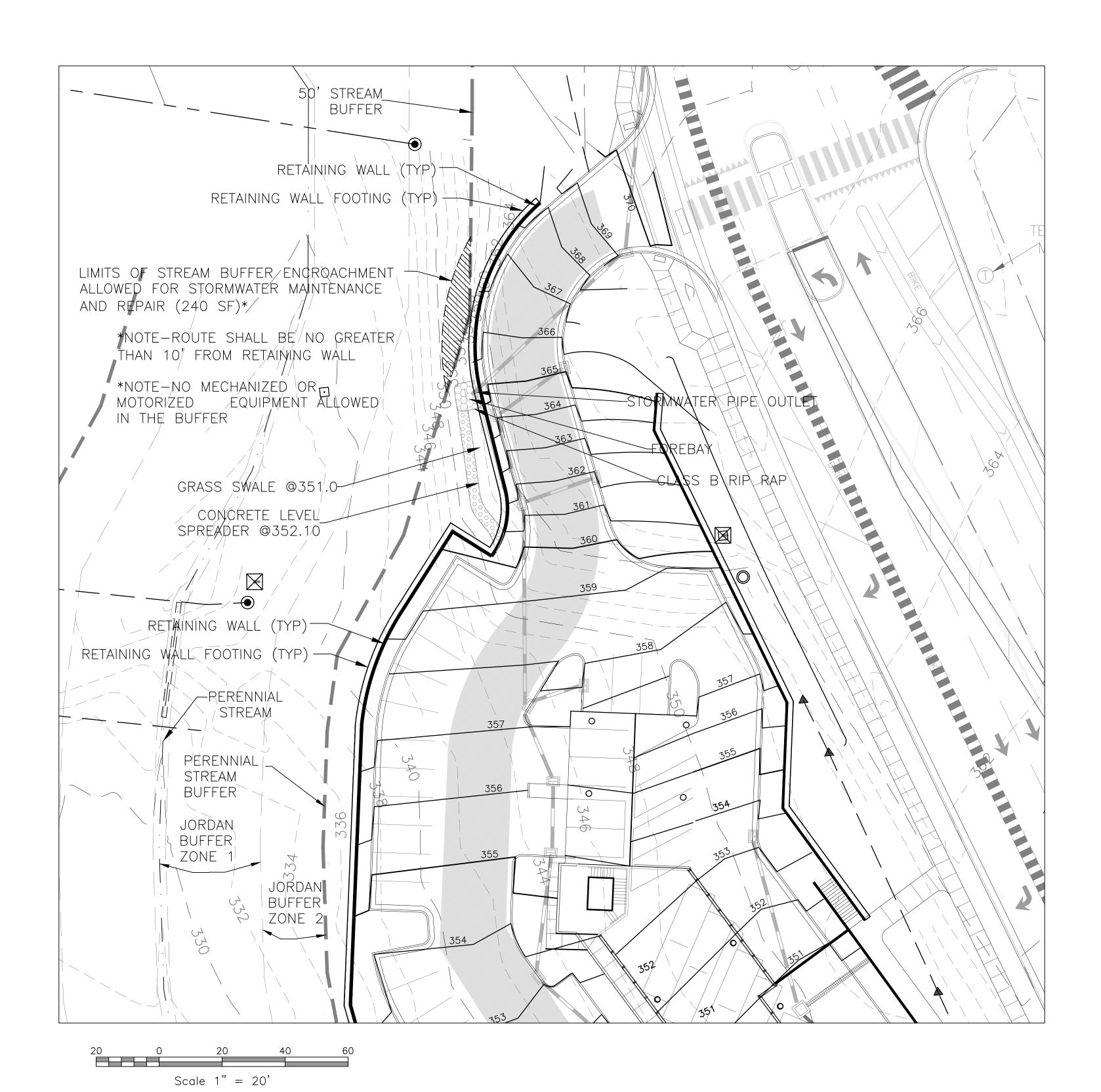
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DRIVE / PARKING SECTIONS

SUP-3.2



1/SUP-3.3
STORMWATER MAINTENANCE PLAN
SCALE: 1"=20'

NOTES:

- 1. THE STORMWATER PIPING OUTLET AND LEVEL SPREADER BEHIND THE RETAINING WALL NEAR THE PROJECT ENTRANCE HAS LIMITED ACCESS BECAUSE OF THE PROTECTIVE STREAM BUFFER IN THIS LOCATION. AS SHOWN ON THE DRAWINGS, THE PIPING AND LEVEL SPREADER ARE NOT LOCATED WITHIN THIS BUFFER.
- 2. ACCESS TO THE PIPING OUTLET AND LEVEL SPREADER FOR MAINTENANCE AND/OR REPAIR IS ALLOWABLE, WITH STRICT LIMITATIONS AS FOLLOWS:
- 3. PERSONNEL WITH HAND OPERATED TOOLS OR EQUIPMENT MAY ACCESS THE STORMWATER PIPING OUTLET AND LEVEL SPREADER ON FOOT ALONG A ROUTE BEHIND THE RETAINING WALL. THE ROUTE SHALL BE LIMITED TO WITHIN 10' OF THE WALL TO AVOID DAMAGE TO THE BUFFER AREA AS MUCH AS POSSIBLE. NO TREES MAY BE REMOVED ALONG THIS ROUTE FOR MAINTENANCE PURPOSES. ANY DISTURBANCE OF THE GROUND BY THIS FOOT TRAFFIC SHALL BE REPAIRED IMMEDIATELY AFTER COMPLETION OF THE STORMWATER MAINTENANCE.
- 4. NO MECHANICAL OR MOTORIZED EQUIPMENT IS ALLOWED TO TRAVERSE THIS ROUTE THROUGH THE BUFFER TO ACCESS THE PIPING OUTLET OR LEVEL SPREADER.
- 5. IF MECHANICAL OR MOTORIZED EQUIPMENT IS REQUIRED FOR MAINTENANCE OR REPAIR ACTIVITIES BEHIND THE RETAINING WALL, THE EQUIPMENT SHALL BE LOWERED OVER THE WALL SUCH THAT THE EQUIPMENT DOES NOT ENCROACH INTO THE STREAM BUFFER.

1/SUP-3.3
STORMWATER MAINTENANCE PLAN NOTES

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COLUMBIA STREET ANNEX

1150 South Columbia Street Chapel Hill, North Carolina

OWNER: CH HOTEL ASSOCIATES LLC. 3008 ANDERSON DRIVE, SUITE 120 RALEIGH NC 27609



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Date
24 May 18
Addition of RCD data
20 May 19
Review Comment Revisions
24 Oct 19
Review Comment Revisions
Review Comment Revisions

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11 May 20 Review Comment Revisions

Drawn By: WLR, MTC Date: 26 April 18

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STORMWATER MAINTENANCE PLAN

SUP-3.3

50 UPLAND RCD ZONE-50' MANAGED-RCD ZONE 50' STREAMSIDE— RCD - MEASURED PUREFOY ROAD FROM TOP OF BANK 50' STREAM BUFFER TELEPHONE MANHOLE GRAVEL. CONSTRUCTION-ENTRANCE N 20°02'39" 43.27 PERENNIAL STRÈAM BUFFÆR TREE PROTECTION-· FENCING 5 A LIMITS OF-SD/STURBANGE SHAT FENCE-SILT FENCE OUTLET (TYP. DAM - TEMPORARY SEDIMENT BASIN TREE PROTECTION FENCE RE: 2/SUP-1.0 50' STREAMSIDE _RCD/ + MEASURED FROM TOP OF _BANK\ 50' MANAGED RCD ZONE_ GUARD RATL 50 UPLAND RCD ZONÉ PAVED EXIT RAMP NC 54 WESTBOUND ENTRY RAMP Scale 1" = 30'1/SUP-4.0 EROSION CONTROL PLAN SCALE: 1"=30'

 INSTALL SILT FENCE PRIOR TO WORK IN ANY AREA TO PREVENT SILT FROM LEAVING SITE.

2. ALL STORMWATER PIPING SHALL BE PROTECTED FROM FLOW UNTIL STABILIZATION IS ACHIEVED. PROTECTION SHALL BE INSTALLED AT THE END OF CONSTRUCTION EACH WORK DAY AND AT ANY TIME RAIN EVENTS CAUSE FLOW INTO OPEN TRENCH, PER DETAILS.

3. ALL STORMWATER STRUCTURES SHALL BE PROTECTED FROM INFLOW UNTIL STABILIZATION IS ACHIEVED USING EITHER STAKED HARDWARE CLOTH OR SILT SACKS, PER DETAILS.

4. CONSTRUCTION ENTRANCES SHOWN ARE IN SUGGESTED LOCATIONS. CONSTRUCTION ENTRANCES SHALL BE INSTALLED AND MAINTAINED SUCH THAT NO CONSTRUCTION TRAFFIC MAY LEAVE THE SITE WITHOUT PASSING OVER THE ENTRANCE DEVICE.

2/SUP-4.0 EROSION CONTROL NOTES

- 1. INSTALL ALL TREE PROTECTION FENCING REQUIRED.
- 2. OBTAIN ALL APPROVALS AND PERMITS NECESSARY TO BEGIN AMD COMPLETE THE PROJECT. THE APPROVALS AND PERMITS MUST BE OBTAINED BEFORE THE DISTURBANCE BEGINS SO THAT WORK WILL NOT BE INTERRUPTED OR DELAYED.
- 3. SCHEDULE AND HOLD A PRE—CONSTRUCTION CONFERENCE TO DISCUSS EROSION CONTROL PLAN AND SCHEDULE WITH THE OWNER OR OWNER'S REPRESENTATIVE, GRADING CONTRACTOR, AND EROSION CONTROL PERSONNEL BEFORE STARTING ANY DEMOLITION, TIMBERING, GRUBBING, CLEARING, OR GRADING ACTIVITIES.
- 4. CLEAR ONLY THE AREA NECESSARY TO GAIN ACCESS TO AND INSTALL THE INITIAL EROSION CONTROL DEVICES: THE CONSTRUCTION ENTRANCE, SILT FENCE AND OUTLETS, DIVERSION DITCHES, ARCH FILTERS, AND CHECK DAMS. ALL DITCHES ARE TO BE SEEDED AND MULCHED IMMEDIATELY AFTER THEY ARE INSTALLED.
- 5. CLEAR AND GRADE THE REMAINDER OF THE SITE. INSTALL STORM DRAINS AND OTHER UTILITIES.
- 6. INSTALL TEMPORARY INLET PROTECTION AND RIP RAP APRONS AS SOON AS POSSIBLE AFTER PIPE HAS BEEN INSTALLED AND DRAINAGE STRUCTURES HAVE BEEN INSTALLED AND BACKFILLED. ENSURE THAT UPSTREAM EROSION CONTROL DEVICES ARE IN PROPER WORKING ORDER TO PREVENT RIP RAP APRONS FROM BECOMING CLOGGED WITH SILT.
- 7. INSTALL CURB AND GUTTER, PAVEMENT BASE, AND PAVEMENT. REMOVE TEMPORARY INLET PROTECTION AND INSTALL SILT SACKS IN INLETS.
- 8. INSPECT AND MAINTAIN THE SEDIMENT CONTROL DEVICES SO THEY CONTINUE TO FUNCTION PROPERLY. THE PERSON RESPONSIBLE FOR EROSION CONTROL WILL OVERSEE THE INSPECTING AND MAINTENANCE OF THESE DEVICES.

 MEASURES SHOULD BE INSPECTED WEEKLY AND AFTER A RAINFALL OF MORE THAN 0.5 INCHES. SEDIMENT TO BE REMOVED FROM TRAPS WHEN SEDIMENT DEPTH IS ONE HALF OF THE TOTAL DEPTH OF TRAP. EROSION CONTROL DEVICES SHALL BE REPLACED WHEN DAMAGED OR DO NOT APPEAR TO BE WORKING PROPERLY.
- 9. IF IT IS DETERMINED DURING THE COURSE OF CONSTRUCTION THAT SIGNIFICANT SEDIMENT IS LEAVING THE PROJECT SITE DESPITE PROPER IMPLEMENTATION AND MAINTENANCE OF THE EROSION CONTROL PLAN, THE PERSON RESPONSIBLE FOR THE LAND DISTURBING ACTIVITY IS OBLIGATED TO TAKE ADDITIONAL PROTECTIVE ACTION.
- 10. KEEP MUD, DUST, AND DEBRIS OFF THE PUBLIC STREET AT ALL TIMES. IF MUD OR DEBRIS IS TRACKED FROM THE SITE, USE A SHOVEL AND BROOM TO REMOVE IT IMMEDIATELY. IF MUD, DUST, AND DEBRIS ARE NOT KEPT OFF THE STREET, ENFORCEMENT ACTION (REVOKING THE GRADING PERMIT AND/OR A STOP WORK ORDER) MAY BE TAKEN.
- 11. SILT FENCE AND DIVERSION DITCHES ARE TO BE RELOCATED AS NECESSARY AS GRADING PROGRESSES TO ENSURE THAT NO SEDIMENT LEAVES THE SITE AND RUNOFF ENTERS THE SEDIMENT BASIN.
- 12. SEEDING AND RESTABILIZATION OF DISTURBED AREAS MUST BE ACCOMPLISHED PER THE PLANS AND SPECIFICATIONS WITHIN THE TIMEFRAMES GIVEN IN THE STABILIZATION TIME TABLE PROVIDED IN THE SEEDING SCHEDULE.

 CONTRACTOR IS RESPONSIBLE FOR TEMPORARY SEEDING THAT IS REQUIRED DURING THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.
- 13. WHEN CONSTRUCTION IS COMPLETED, ALL TEMPORARY EROSION CONTROL DEVICES MUST BE REMOVED AFTER THE DRAINAGE AREA HAS BEEN SUFFICIENTLY STABILIZED TO RESTRAIN EROSION. REMOVE AND PROPERLY DISPOSE OF ACCUMULATED SEDIMENT AND DEBRIS FROM THE DEVICES AND STABILIZE THE LOCATIONS.
- 14. ARRANGE A FINAL INSPECTION WITH THE EROSION CONTROL INSPECTOR TO CONFIRM THAT ALL THE REQUIREMENTS OF THE APPROVED EROSION CONTROL PLAN HAVE BEEN COMPLETE
- 15. SANDFILTERS ARE NOT TO RECEIVE RUNOFF FROM THE SITE UNTIL ALL EROSION AND SEDIMENT PHASES ARE COMPLETED. ALL INLETS TO SANDFILTERS SHALL REMAIN SEALED UNTIL SITE IS COMPLETELY STABILIZED.

3/SUP-4.0 CONSTRUCTION SEQUENCE

Ground Stabilization* Stabilization Stabilization Time Time Frame Frame Exceptions Description Perimeter dikes, 7 days swales, ditches, and slopes **High Quality** Water (HQW) Slopes steeper 7 days than 3:1 Slopes 3:1 or 7 days All other areas 7 days with slopes flatter than 4:1

* "Extensions of time may be approved by the permitting authority based on weather or other site-specific conditions that make compliance impracticable" (Section 11.B(2)(b))

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Drawn By: WLR, MTC

7-DAY STABILIZATION AREAS INCLUDE:

ALL AREAS BETWEEN LIMITS OF

ALL AREAS BETWEEN LIMITS OF

DISTURBANCE AND STORM LINE FROM

DISTURBANCE AND WALL 3

SWALES 1 - 6

MH1 TO MH3

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EROSION CONTROL PLAN

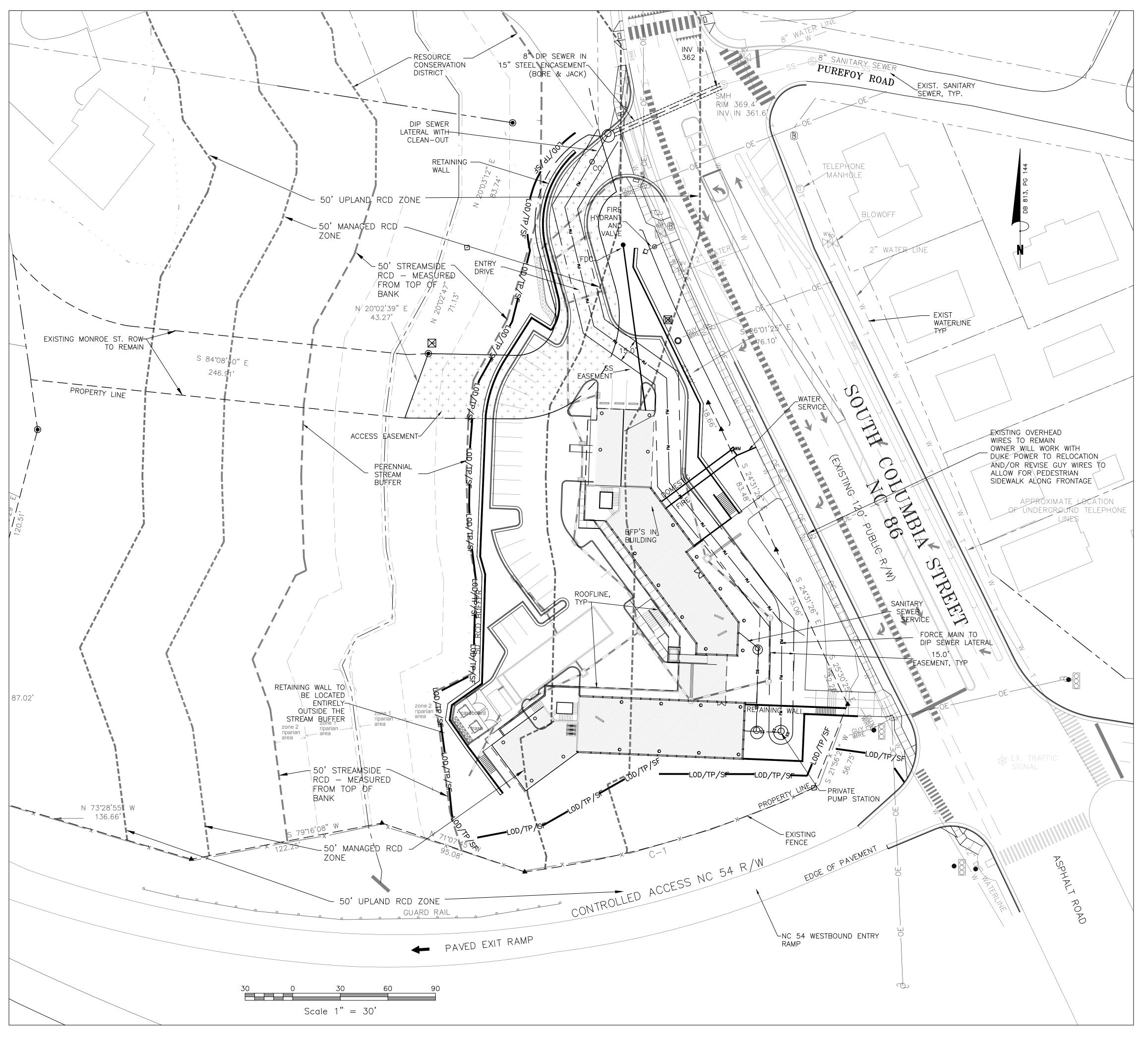
SUP-4.0

NOTE:
EROSION CONTROL MEASURES
SHOWN ARE THE MINIMUM
REQUIRED. CONTRACTOR
SHALL INSTALL ADDITIONAL
MEASURES AS MAY BE
NECESSARY TO PREVENT
SEDIMENT FROM LEAVING THE
SITE OR ENTERING THE
EXISTING AND PROPOSED
STORMWATER BMP'S. ANY
REQUIRED SEDIMENT
REMOVAL SHALL BE THE
RESPONSIBILITY OF THE

CONTRACTOR.

FIRE PROTECTION NOTES:

- 1. All construction an demolition conducted shall be in compliance of the current edition of Chapel 14 of the NC
- 2. When the building being constructed requires standpipes, a temporary standpipe connection will be constructed with ready Fire Department Access when the building is not more than 40' in height. Such standpipes shall provide usable connections adjacent to the stairs and shall continue with building progression always being not more than one floor below the highest floor of the building. NC FPC 2012 Section 1413.



1/SUP-5.0 UTILITY PLAN SCALE: 1"=30'

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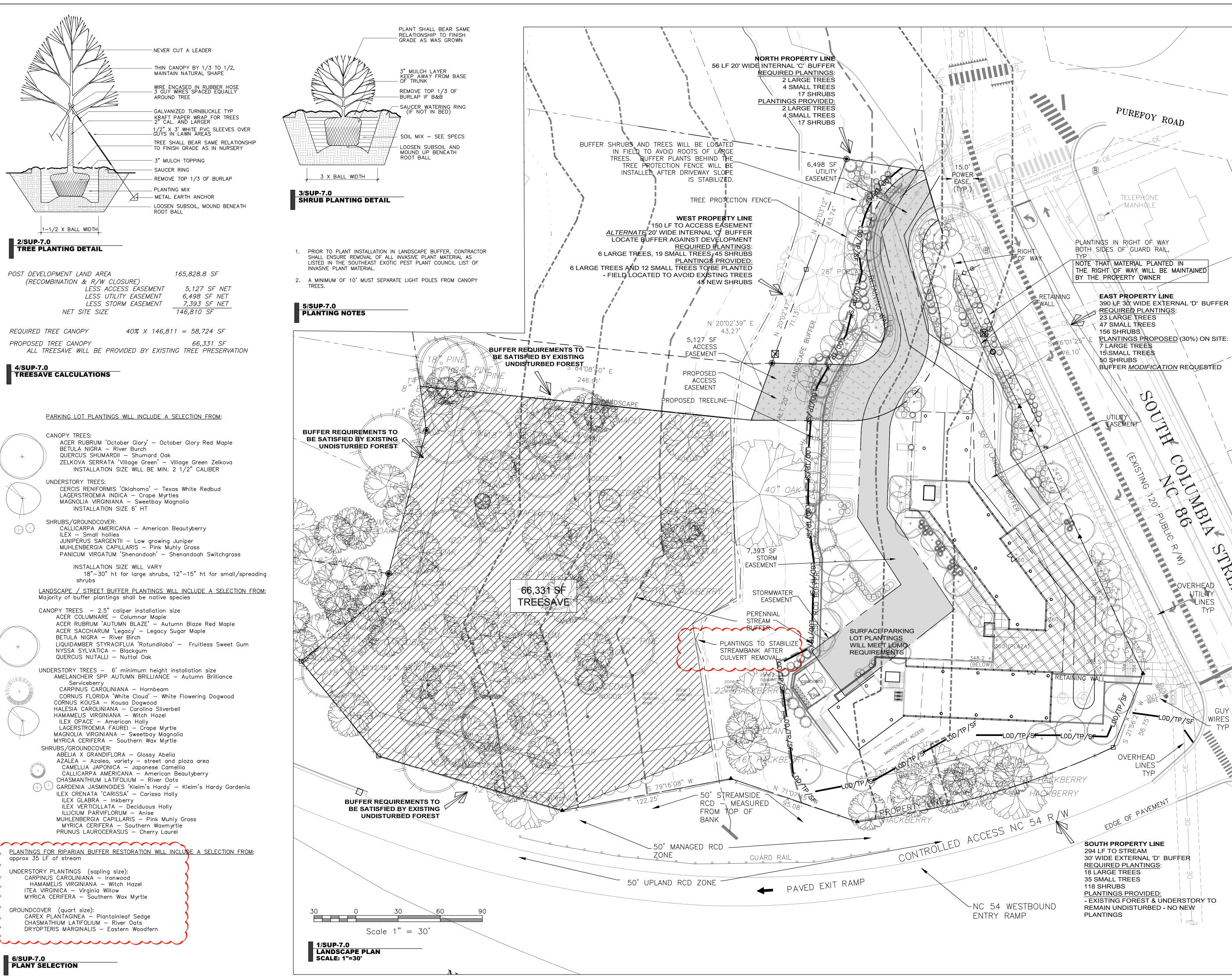
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Drawn By: WLR, MTC

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UTILITY PLAN

SUP-5.0



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LANDSCAPE AND TREESAVE

SUP-7.0

PLAN

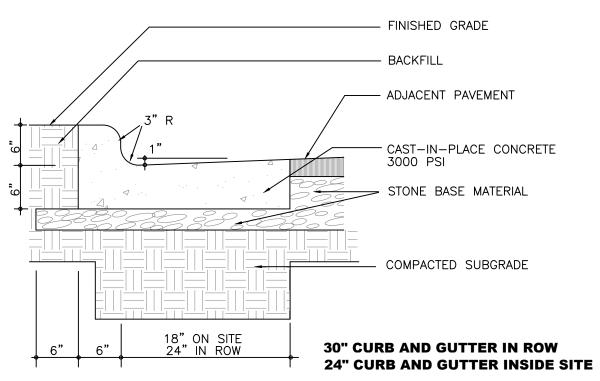
-CONTRACTION JOINTS SHALL BE SPACED AT 10 FOOT INTERVALS. EXCEPT THAT A 15 FOOT SPACING MAY BE USED WHEN A MACHINE IS USED OR WHEN SATISFACTORY SUPPORT FOR THE FACE FORM CAN BE OBTAINED WITHOUT THE USE OF TEMPLATES AT 10 FOOT INTERVALS. JOINT SPACING MAY BE ALTERED BY THE ENGINEER TO PREVENT UNCONTROLLED CRACKING.

-CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS. WHERE SUCH JOINTS ARE NOT FORMED BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2" SHALL BE MAINTAINED.

-ALL CONTRACTION JOINTS SHALL BE FILLED WITH JOINT FILLER.

-JOINTS SHALL MATCH LOCATIONS WITH JOINT IN ABUTTING SIDEWALK.

-JOINTS SHALL MATCH LOCATIONS WITH JOINT IN ABUTTING SIDEWALK.
-EXPANSION JOINTS SHALL BE SPACED AT 90 FOOT INTERVALS AND ADJACENT TO ALL RIGID OBJECTS.



STANDARD PARKING AREAS: FINAL PAVEMENT SECTION WILL BE PROVIDED BY GEO—TECHNICAL ENGINEER BASED SITE SPECIFIC

STANDARD ASPHALT PAVING - PARKING BAYS

GEO-TECHNICAL REPORT

3"- TYPE S-9.5B PAVEMENT

- 8" AGGREGATE BASE

COURSE COMPACTED

- COMPACTED SUBGRADE

1/SUP-8.0 24" CONCRETE CURB & GUTTER NTS

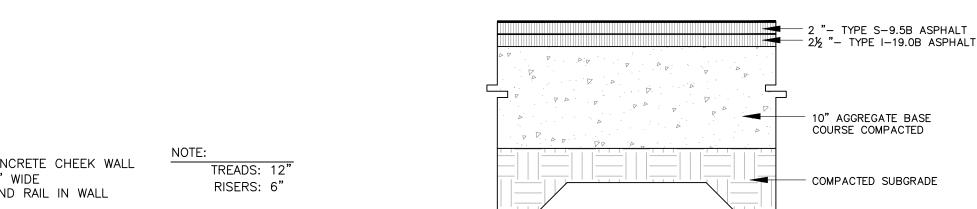
WHERE RAILING ENDS AT TOP OF STAIRS, EXTEND 1' PAST THE TOP

MASONRY SCREEN

ROLL-OUT

STEEL PIPE BOLLARD

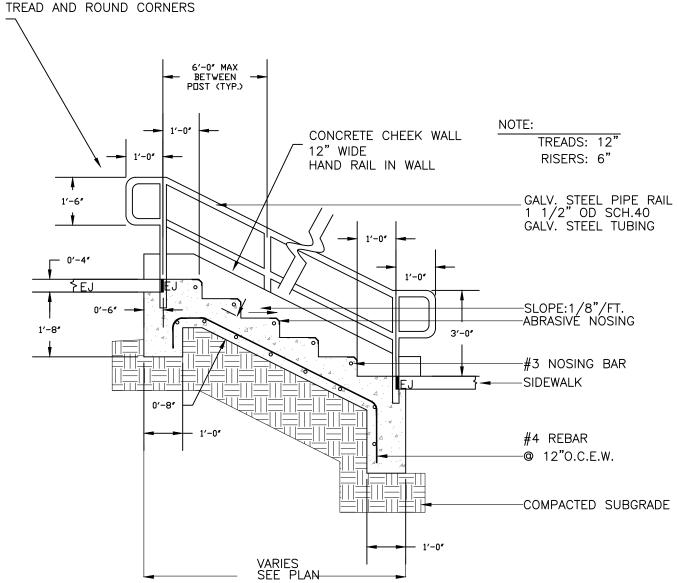
RECYCLE CARTS



2/SUP-8.0

3/SUP-8.0

HEAVY DUTY ASPHALT PAVING - DRIVE AISLES



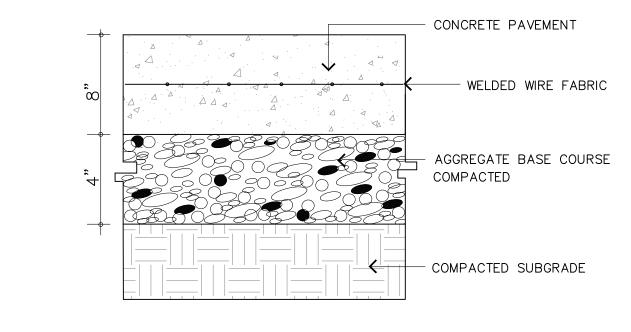
5/SUP-8.0

TYPICAL EXTERIOR STAIR AND RAILING

CARDBOARD DUMPSTER

26'

_SOLID_WASTE DUMPSTER

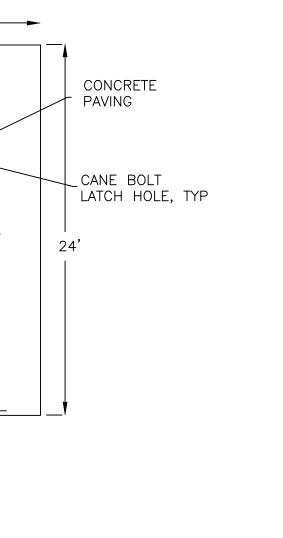


NOTES:

1. ALL CONCRETE TO 4000 PSI, PROPERLY AIR ENTRAINED FOR EXTERIOR GRADE CONCRETE MIXES.

2. CABC BASE AND SOIL SUBGRADE TO BE COMPACTED TO 98% OF THE ASTM D-698 STD. PROCTOR MDD AND PROOFROLLED AND APPROVED BY GEOTECHNICAL PERSONNEL.

6/SUP-8.0 DUMPSTER PAD CONCRETE PAVING

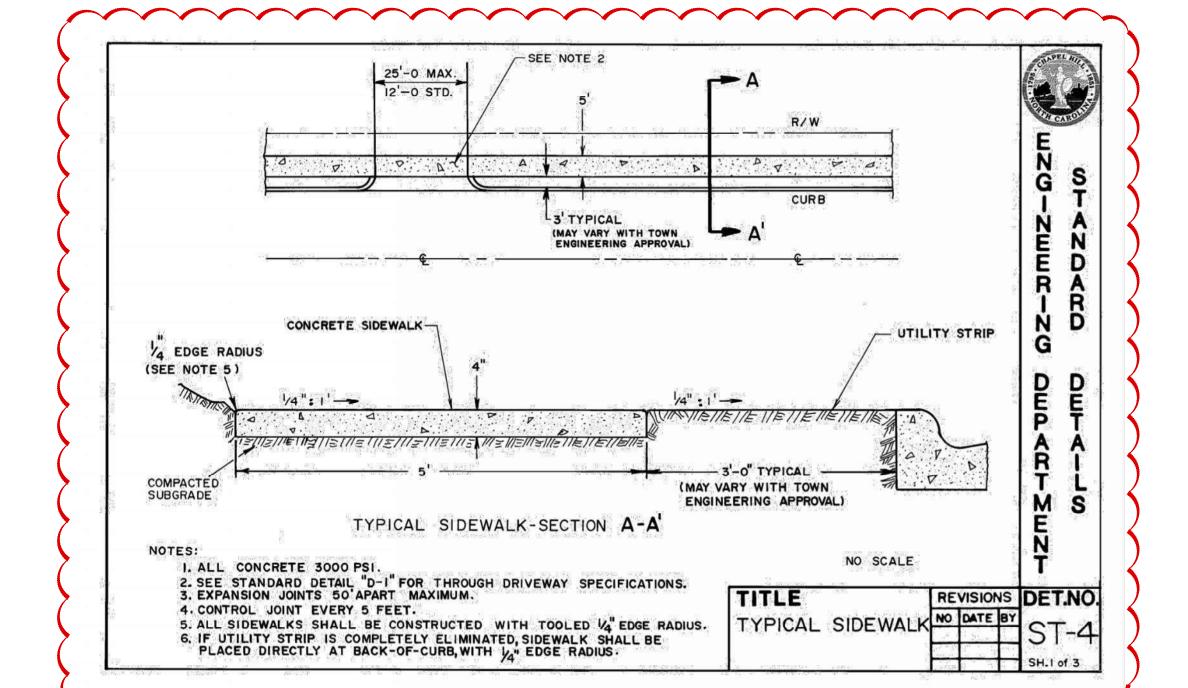


NOTES: MARKINGS TO BE MADE FROM THRMOPLASTIC AND HIGH VISIBILITY GREEN IN COLOR.

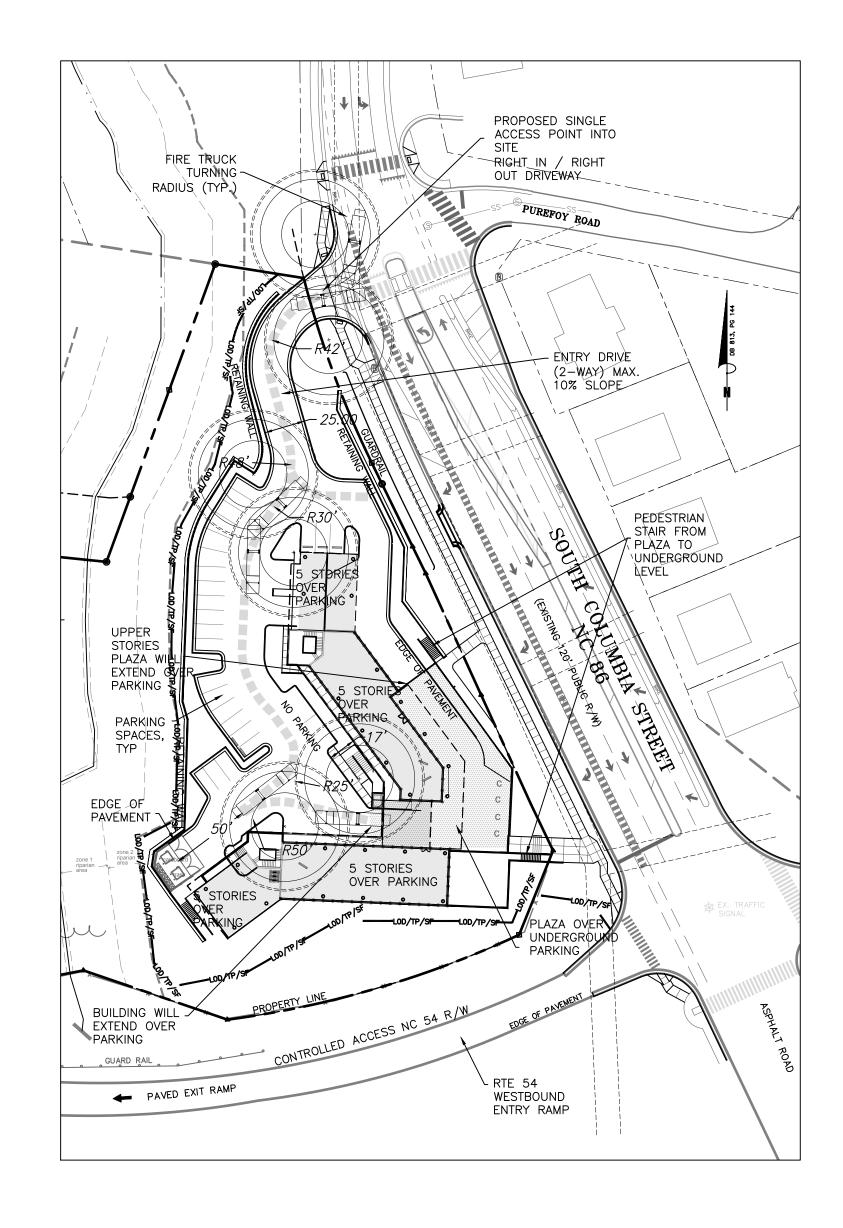
7/SUP-8.0

DUMPSTER ENCLOSURE





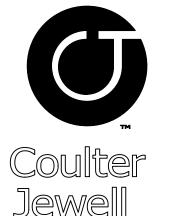
4/SUP-8.0
CONCRETE SIDEWALK



10/SUP-8.0
FIRE LANE ACCESS / SOLID WASTE PICKUP ACCESS
SCALE: 1"=60"

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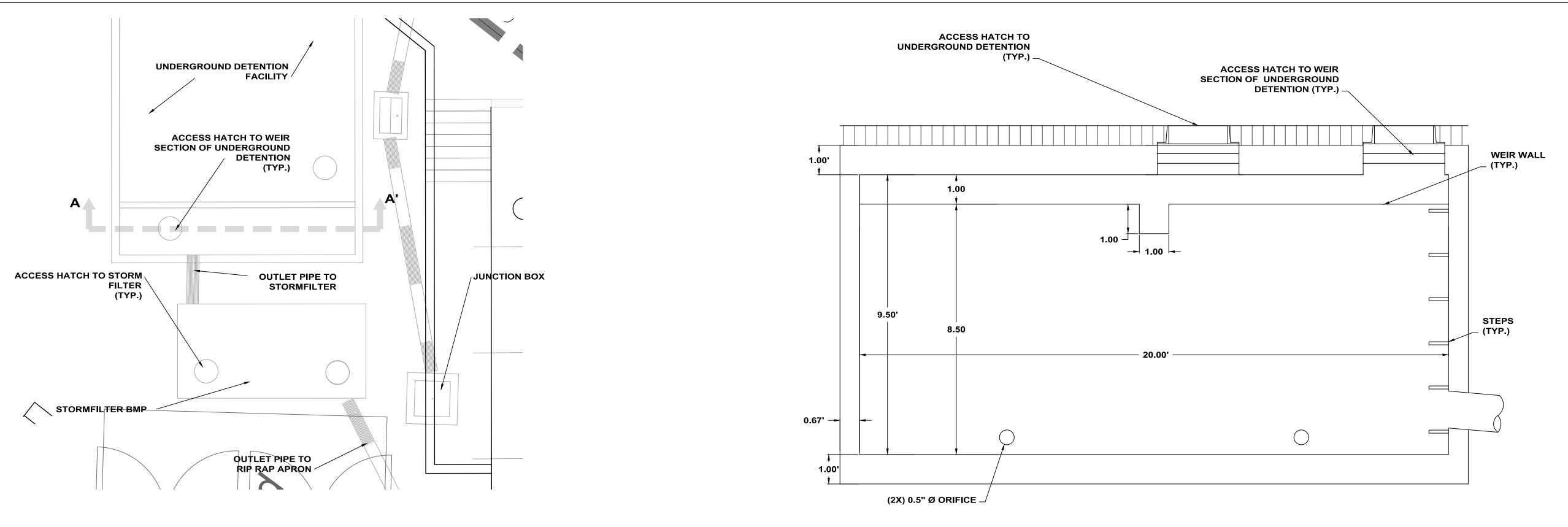
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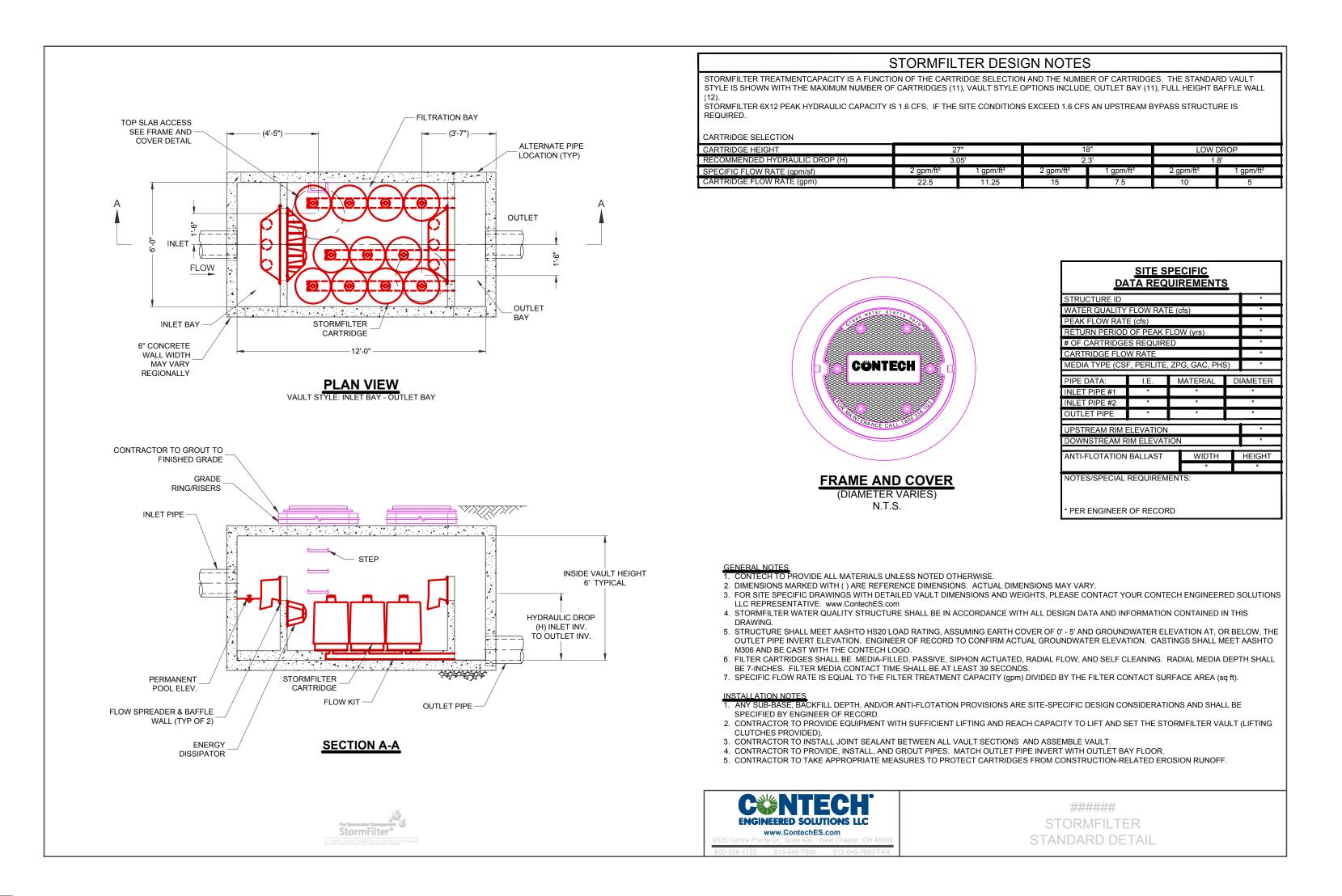
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SITE DETAILS

SECTION A-A' VIEW



1/SUP-8.1 UNDERGROUND DETENTION FACILITY

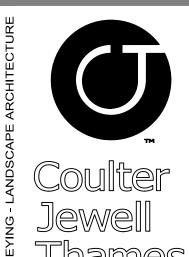


2/SUP-8.1 STORMFILTER BMP

PLAN VIEW

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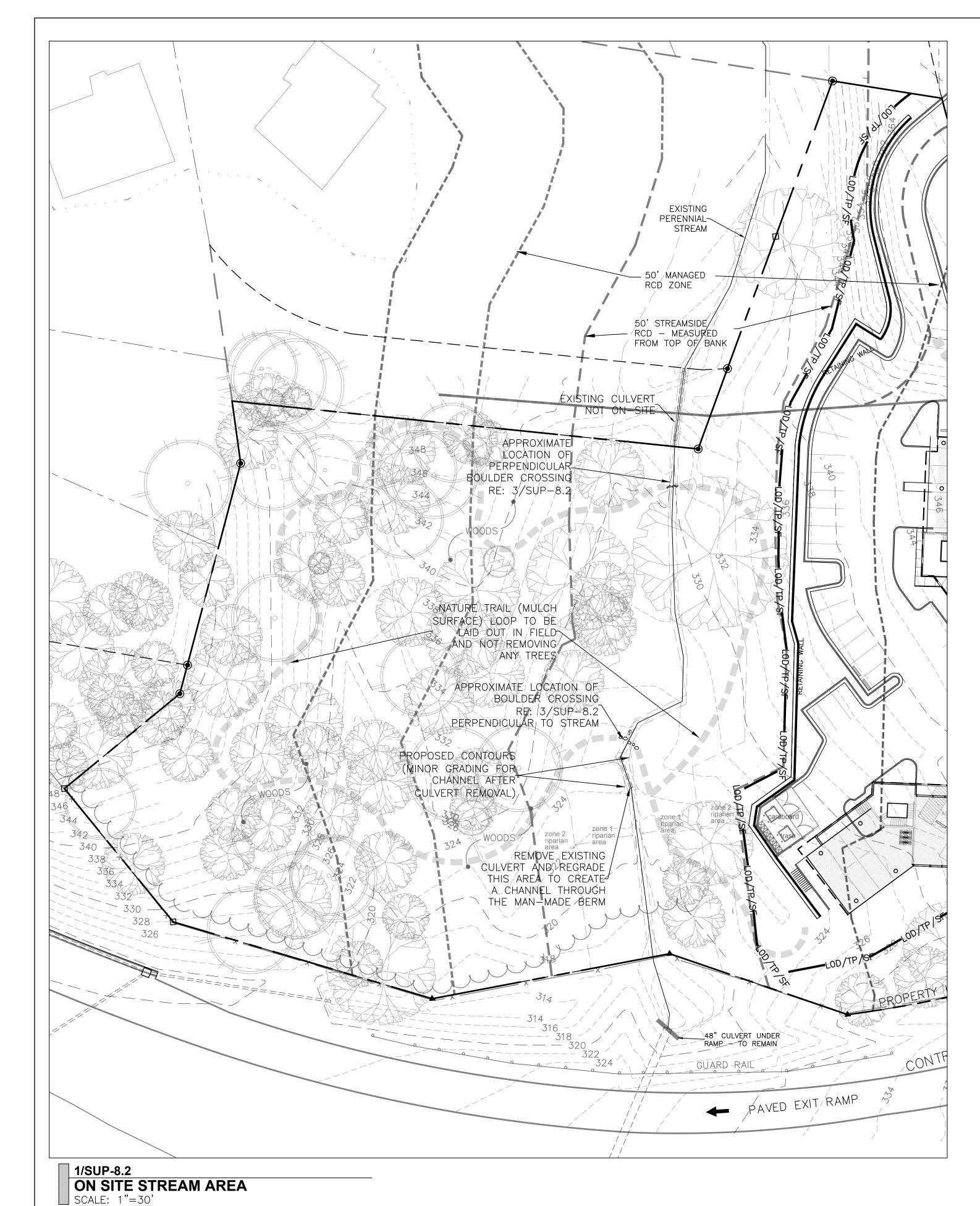
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STORMWATER DETAILS



STREAM AREA PROPOSED WORK:

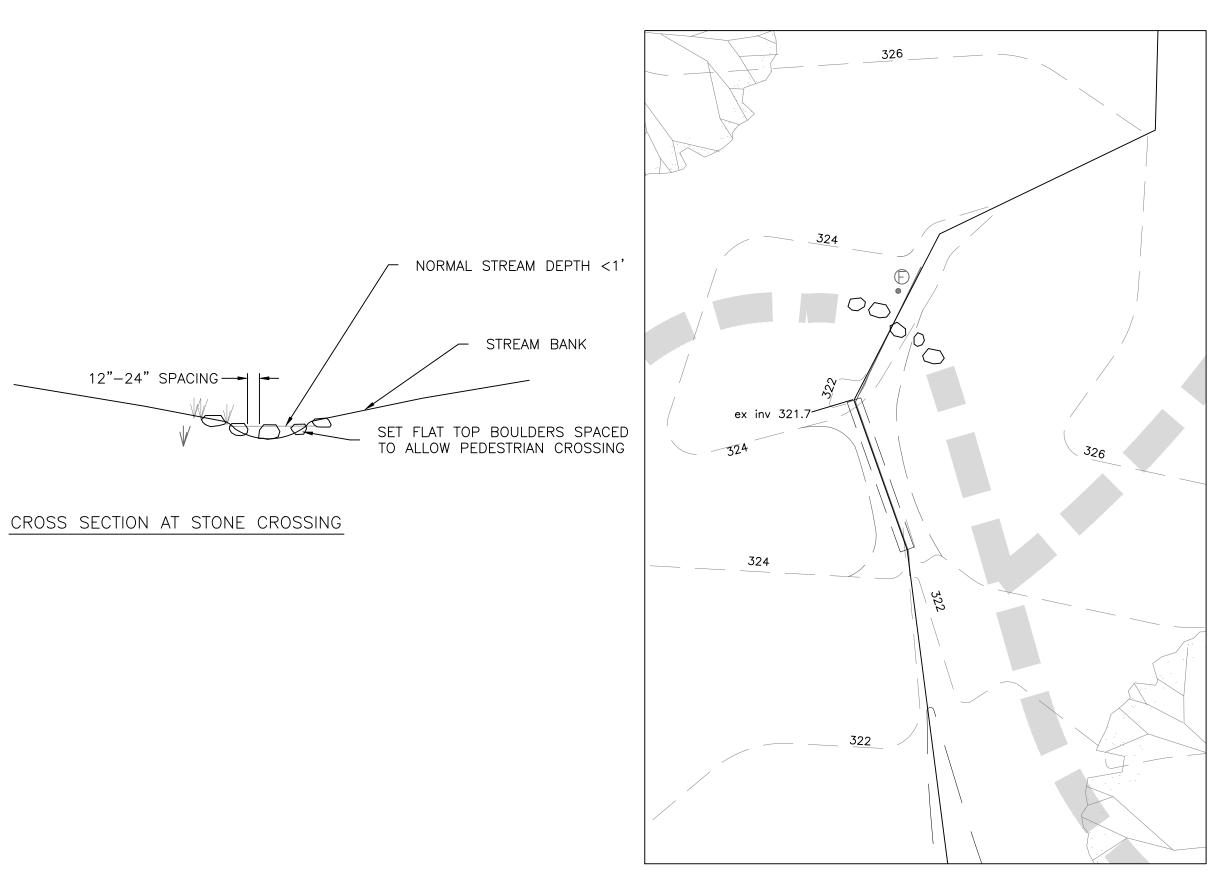
- 1. REMOVE TRASH FROM THE STREAM AND
- STREAMSIDE RCD AREA

 2. REMOVE INVASIVE PLANT MATERIAL FROM THE
- STREAMSIDE RCD AREA

 3. REMOVE CULVERT AND REGRADE TO ALLOW STREAM FLOW THROUGH EXISTING BERM
- 4. PLANT SEDGE PLUGS INTO THE DISTURBED AREA NEAR THE CULVERT REMOVAL TO STABILIZE THE
- DISTURBED AREA.

 5. CREATE A MULCH SURFACE NATURE TRAIL INCLUDING TWO STREAM CROSSINGS OF STRATEGICALLY PLACED BOULDERS

2/SUP-8.2 SUMMARY OF WORK TO BE DONE IN STREAM AREA



PLAN VIEW

3/SUP-8.2
ON SITE STREAM CROSSING
SCALE: 1"=10"

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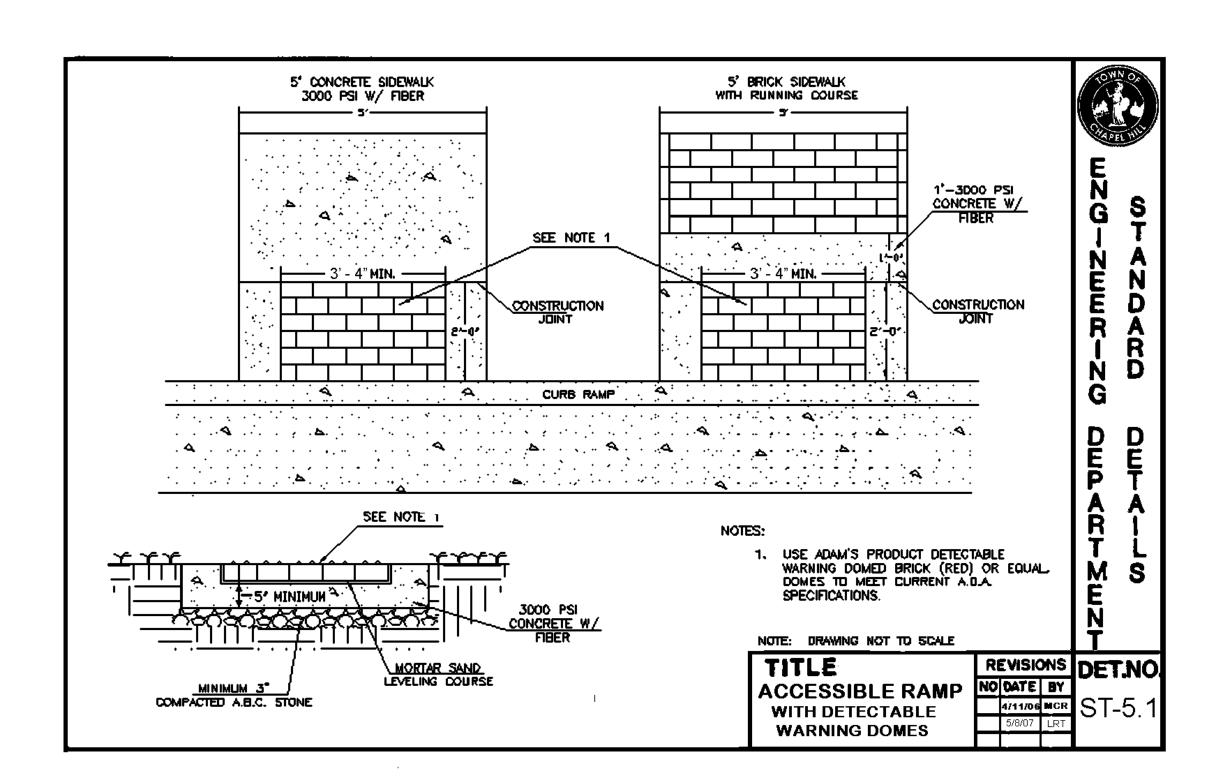
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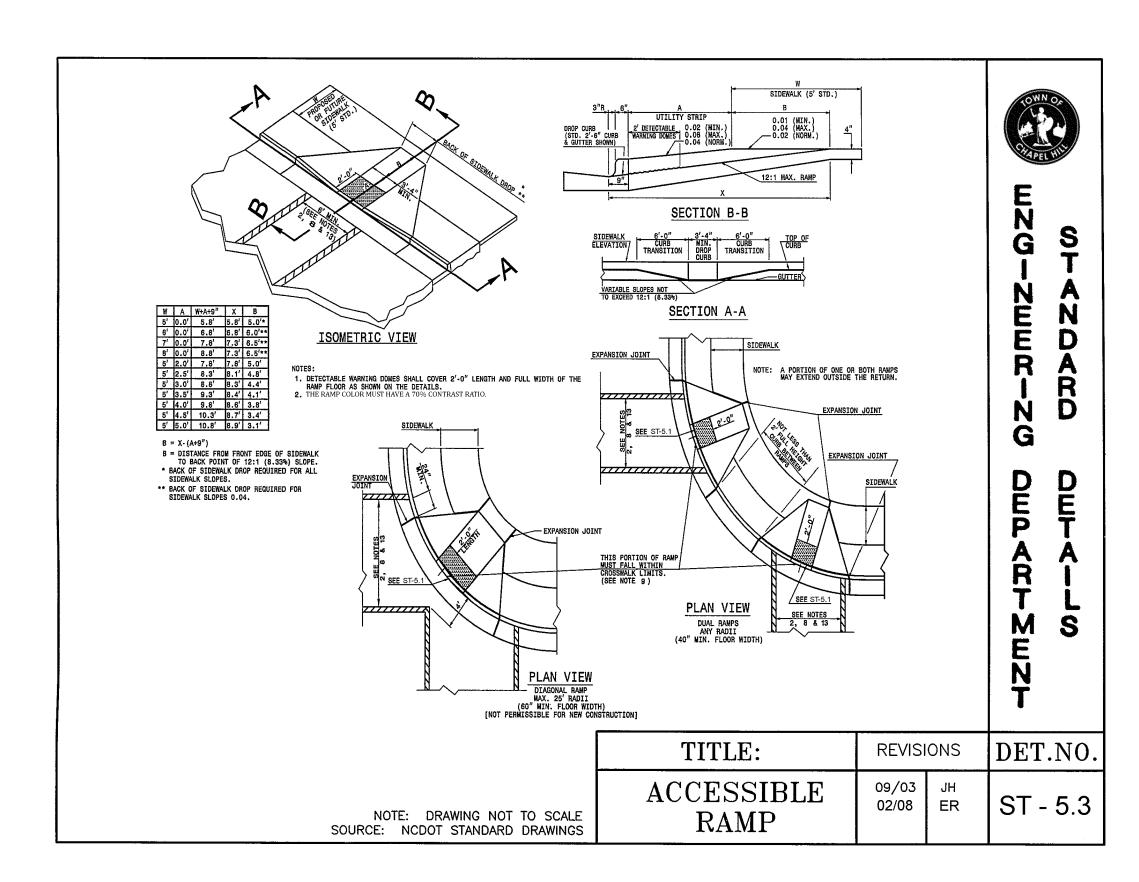
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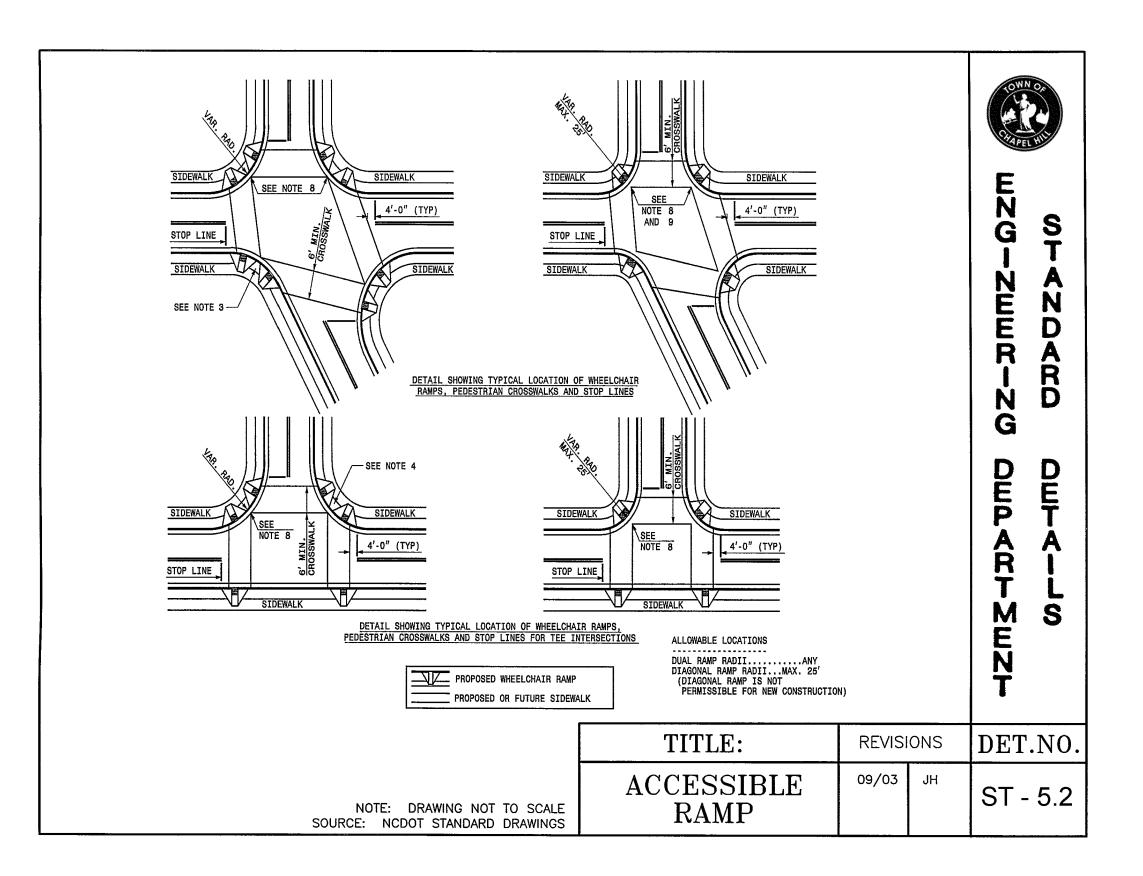
STREAM RESTORATION DETAILS



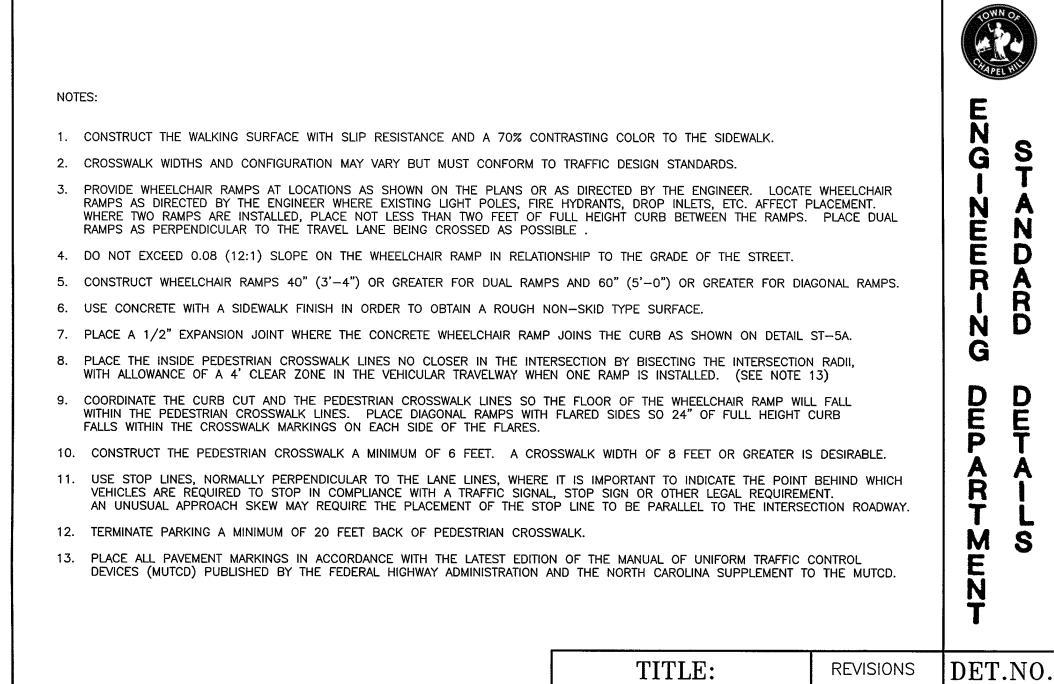
1/SUP-8.3
ACCESSIBLE RAMP
NTS



3/SUP-8.3
ACCESSIBLE RAMP



2/SUP-8.3
ACCESSIBLE RAMP
NTS



SOURCE: NCDOT STANDARD DRAWINGS

ACCESSIBLE

09/03 JH

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COLUMBIA STREET ANNEX

1150 South Columbia Street Chapel Hill, North Carolina

OWNER: CH HOTEL ASSOCIATES LLC. 3008 ANDERSON DRIVE, SUITE 120 RALEIGH NC 27609



If this drawing is not 24" x 36" it is a reduced print - refer to graphic scale

Date 24 May 18	For Addition of RCD data
20 May 19	Review Comment Revision
24 Oct 19	Review Comment Revision
14 Fab 20	Paview Comment Pavision

14 Feb 20 Review Comment Revisions

11 May 20 Review Comment Revisions

Review Comment Revisions

Review Comment Revisions

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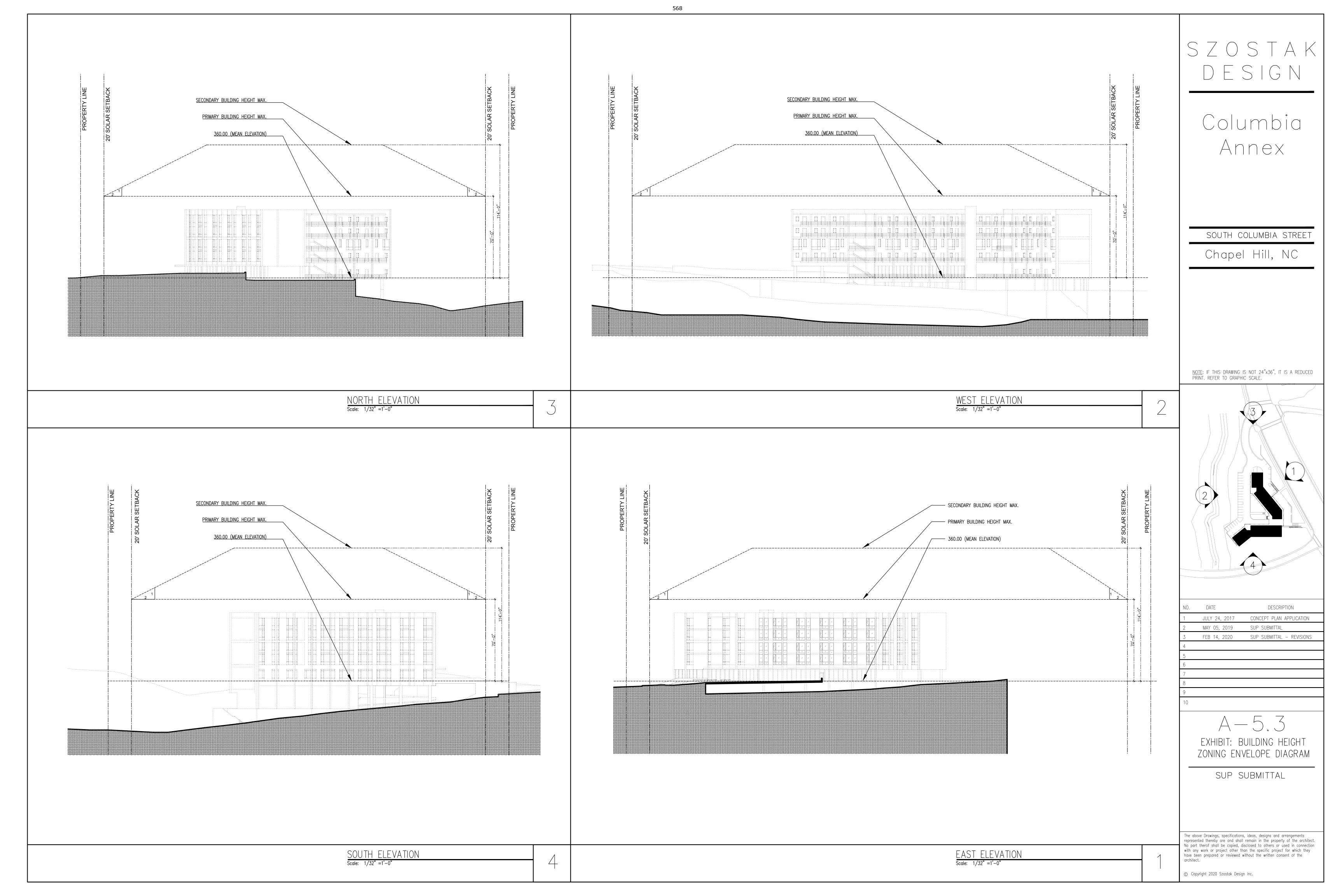
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SITE DETAILS



COLUMBIA STREET ANNEX

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



Prepared for:

The Town of Chapel Hill Engineering Department

Prepared by:

HNTB North Carolina, PC

343 East Six Forks Road Suite 200 Raleigh, NC 27609

NCBELS License #: C-1554

August 2014



COLUMBIA STREET ANNEX

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



Prepared for:

The Town of Chapel Hill Engineering Department

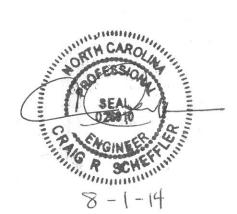
Prepared by:

HNTB North Carolina, PC

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NCBELS License #: C-1554

August 2014





EXECUTIVE SUMMARY

Project Overview

A commercial/residential mixed-use development, known as the Columbia Street Annex, located along NC Highway 86 (S. Columbia Street) near its intersection with the NC Highway 54 Bypass is being proposed in Chapel Hill. The project proposes to construct 39 residential units and 7,150 square feet of office/retail space on two existing parcels on the west side of S. Columbia Street. **Figure ES-1** shows the general location of the site. The project is anticipated to be fully complete by 2016. This report analyzes the complete build-out scenario for the year 2017 (one year after anticipated completion), the no-build scenario for 2017, as well as 2014 existing year traffic conditions.

The proposed site concept plan shows a single full movement access driveway along S. Columbia Street. No other vehicular access connections are proposed. **Figure ES-2** displays the preliminary concept plan of the Columbia Street Annex and nearby land uses and roadways. The project is expected to provide 68 parking spaces on surface parking lots and an underground garage. This report analyzes and presents the transportation impacts that the Columbia Street Annex will have on the following intersections in the project study area:

- NC 86 (S. Columbia Street) and Purefoy Road/Proposed Site Driveway
- NC 86 (S. Columbia Street) and NC 54/US 15-501 Bypass Westbound Ramps
- US 15-501 and NC 54 Bypass Eastbound Ramps

The impacts of the proposed site at the study area intersections will be evaluated during the AM, noon, and PM peak hours of an average weekday. The following study is based on background traffic for the existing year, 2014, the year following the estimated site build out year of 2016, as well as the estimated site-generated traffic produced by the mixed-use development.

Existing Conditions

Study Area

The site is located in southern Chapel Hill along NC 86 (S. Columbia Street) in the northwest quadrant of its interchange with the NC 54 Bypass. The study area contains two signalized intersections at the interchange ramp terminals. It also includes the existing unsignalized stop-controlled intersection at Purefoy Road adjacent to the site.

NC 86 (S. Columbia Street) is a major arterial facility providing connectivity between downtown Chapel Hill and the UNC Main Campus with the NC 54 Bypass and US 15-501 corridors. Remaining study area network roadways are either interchange access ramps or local neighborhood access streets.

Site Traffic Generation

With the addition of new peak hour trips during the weekday AM, noon, and PM peak hours, there are potential site traffic impacts to the study area intersections. **Table ES-1** shows the site trip generation details, with generation rates and methodologies taken from the *Institute of Transportation Engineers (ITE) Trip Generation Manual, Version 9.*

Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

Table ES-1 Weekday Vehicle Trip Generation Summary Columbia Street Annex

		Daily		AM	Peak H	lour	Noor	Peak	Hour	PM Peak Hour			
Trip Generation Statistic	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	
Raw Total	333	333	666	9	21	30	23	25	48	38	28	66	
Transit/Ped/Bike Reduction	-50	-50	-100	-1	-3	-5	-3	-4	-7	-6	-4	-10	
Vehicular Driveway Trips	283	283	566	8	18	25	20	21	41	32	24	56	
Pass-By Trips (Retail Only)	-26	-26	-52	-0	-0	-0	-2	-3	-5	-4	-5	-9	
New Vehicular Trips Added to Network	257	257	514	8	18	25	18	18	36	28	19	47	

^{* -} No ITE Data Available, Assumed Average of AM/PM Peak Hour Data

Background Traffic

Background traffic growth for the 2017 analysis year is expected to come from two sources - ambient regional traffic growth and specific development-related traffic growth. Based on existing information, two Town-approved or recently completed development projects near the project study area are planned to be contribute to background traffic growth by the 2017 analysis year. All remaining estimated traffic growth in the area is assumed to occur due to overall region-wide ambient growth. To account for this, an ambient area-wide traffic growth percentage of 1.2 percent per year was applied to existing traffic volumes based on information from the historic daily traffic growth patterns in the project study (NCDOT and Town of Chapel Hill daily traffic information) and consistent with recent traffic impact studies completed near the project study area.

Impact Analysis

Peak Hour Intersection Level of Service (LOS)

Study results indicate existing traffic operations at all study area intersections are acceptable during the AM and Noon peak hours. In the PM peak hour, the signalized ramp terminal intersections at the NC 54 Bypass operate at a LOS F given current traffic levels and signal timing information. Retiming these signals for anticipated 2017 traffic volumes improves the PM peak deficient operations. Even with the addition of peak hour site-generated trips to the projected 2017 background traffic volumes, no additional study area intersections are expected to experience deficient traffic operations in any peak hour. However, capacity analysis results, if recommended improvements at Purefoy Road and the proposed site driveway are implemented, show a decline in worst-case stop controlled delay and LOS, but this is offset by potential safety improvements in this area.

A summary of the traffic operations for each intersection, related to vehicular delays (intersection average as a whole if signalized, critical movement if stop-controlled) and the corresponding Level-of-Service (LOS) is shown in **Table ES-2** on the following page.



Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

Table ES-2. Columbia Street Annex LOS and Delay (Seconds/Vehicle) Summary

	Peak	2014 Existing		2017 N	lo-Build	2017	Build	2017 Mitigated		
Intersections	Hour	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	
NO 00 (0, 0, 1, 1; 0)	AM	D	27.0	D	29.4	D	30.1	E**	36.5**	
NC 86 (S. Columbia St.) & Purefoy Road	NOON	С	15.3	С	16.1	С	16.4	C**	20.7**	
a r dicioy rtoad	PM	Е	36.1	E	42.5	Е	44.4	F**	93.1**	
NC 86 (S. Columbia St.)	AM	С	27.0	С	22.8	С	22.8	N/A	N/A	
& NC 54 Bypass	NOON	С	23.6	С	21.5	С	21.6	N/A	N/A	
Westbound Ramps	PM	Ε	68.9	D	52.7	D	52.8	N/A	N/A	
US 15-501 &	AM	С	34.4	С	24.6	С	24.6	N/A	N/A	
NC 54 Bypass	NOON	Α	9.9	В	11.6	В	11.6	N/A	N/A	
Eastbound Ramps	PM	F	93.0	В	15.2	В	15.2	N/A	N/A	
NO 00 (0, 0, 1, 1, 0)	AM	N/A	N/A	N/A	N/A	C*	21.1*	N/A	N/A	
NC 86 (S. Columbia St.) & Site Driveway*	NOON	N/A	N/A	N/A	N/A	B*	14.8*	N/A	N/A	
a one briveway	PM	N/A	N/A	N/A	N/A	D*	28.2*	N/A	N/A	

N/A – Not Applicable or No Improvements Necessary

BOLD/ITALICS - Critical Movement or Overall Intersection Requires Mitigation Per Town TIS Guidelines

Access Analysis

Vehicular site access is to be accommodated by a single full movement access driveway connecting to S. Columbia Street about 425 feet to the north of its signalized intersection with the NC 54 Bypass westbound ramps. Design details related to driveway throat length are shown on the concept plan and assume an approximate 125 foot driveway throat at this driveway. An internal driveway circulation system to all surface and underground parking areas is also shown on the plans.

Driveway distances along S. Columbia Street from the signalized intersections at the NC 54 Bypass Westbound Ramps and Mason Farm Road (to the north of the study area) are acceptable, based on current standards. The proposed spacing between the proposed driveway and adjacent existing driveways (several that are approximately 250-500 feet north of the proposed driveway) is less than the recommended 750 foot spacing along arterial roadways found in Table 4-A-1 in the Town Design Manual.

Access for pedestrians and bicyclists is adequate in the project study area. Sidewalk is present on both sides of S. Columbia Street adjacent to the site and exists along the major street connections along S. Columbia Street. Crosswalk and pedestrian signals exist across the three major signalized study area intersections near the Columbia Street Annex site. Specific bicycle amenities are present along S. Columbia Street, with striped lanes present in both directions.

Crash Analysis

August 2014

Data from the NCDOT Traffic Safety Unit was provided for recent three and five-year periods for all existing major study area intersections. The primary crash type at all locations was rear-end



^{* -} Worst-Case LOS/Delay for Unsignalized/Stop-Controlled Critical Movement

^{** -} Results for Realigned Site Driveway to Form Fourth Leg of Intersection

Town of Chapel Hill: Traffic Impact Study *Columbia Street Annex* - Proposed Mixed-Use Development

crashes. Overall, the number and severity of crashes are low compared to other locations around Chapel Hill.

Other Transportation-Related Analyses

Other transportation-related analyses relevant to the 2001 Town of Chapel Hill Guidelines for the preparation of Traffic Impact Studies were completed as appropriate. The following topics listed in **Table ES-3** on the following page are germane to the scope of this study.

Table ES-3. Other Transportation-Related Analyses

Analysis	Comment
Long-Range Planning Level Daily Volume- Capacity Analysis	Due to the fact that the proposed site will add less than 600 daily trips to the study area network, no long-range daily v/c analysis was conducted for this study.
Turn Lane Storage Requirements	Storage bay lengths at study area intersections were analyzed using Synchro and HCS 95 th percentile (max) queue length estimates for the 2017 Build Scenario. The S. Columbia Street/NC 54 Bypass westbound ramps intersection westbound approach has existing and projected queues that exceed its current storage bays that may need additional geometric improvement in the future.
	Realignment of the proposed site driveway with Purefoy Road, along with some minor geometric changes to the Purefoy Road approach may allow the creation of a short left-turn storage area (1-2 vehicles) that may help operations at this intersection. No other recommendations for improvements to storage bays are expected, based on the analysis results.
Appropriateness of Acceleration/ Deceleration Lanes	The site concept plan shows no specifics related to acceleration/deceleration lanes, other than the creation of an exclusive northbound left-turn lane on S. Columbia Street. Based on existing cross-section details with the U-0624 project, corridor speeds on S. Columbia Street and capacity analysis results, no specific acceleration or deceleration lanes are recommended at proposed site access point. No other specific acceleration/deceleration lane issues were analyzed in the project study area.
Pedestrian and Bicycle Analysis	Existing pedestrian and bicycle access and connectivity is adequate along the S. Columbia Street corridor adjacent to the site. Sidewalk exists along the NC 86 and US 15-501 corridors and pedestrian crossings and signals are present on at least one quadrant of signalized intersections. An additional mid-block crosswalk exists 250 feet to the north of the proposed site along S. Columbia Street. Delineated bicycle lanes and wide outside lanes are present in the project study area. The Morgan Creek Trail Greenway has access in the area just to the south of the NC 54 Bypass, providing additional bicycle and pedestrian connectivity to the area.
Public Transportation Analysis	Public transportation service to the study area, and to the proposed site, is excellent with multiple bus stops and multiple local and regional bus routes on S. Columbia Street proximate to the site.





Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

Mitigation Measures/Recommendations

Planned Improvements

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2014-2017.

Background Committed Improvements

There are no specific geometric or operational improvements to study area roadway intersections or facilities related to background private development projects that are expected to be completed between 2014 and 2017. To make an comparative assessment of potential improvements to signal timing (by optimizing signal cycle lengths, splits and offsets for projected 2017 traffic volumes) it was assumed that signal timing reoptimization would occur for the S. Columbia Street corridor by the year 2017 with or without the development of the Columbia Street Annex project.

Applicant Committed Improvements

Based on the preliminary site concept plans and supporting development information provided, there are the following specific external transportation-related improvements proposed adjacent to the Columbia Street Annex:

- Provision of a northbound left-turn lane and pavement marking modifications to the existing NC 86 (S. Columbia Street) southbound additional inside through travel lane that develops south of Purefoy Road.
- Provision of a crosswalk in this vicinity.
- Provision of continuous sidewalk along site frontage.

Necessary Improvements

Based on traffic capacity analyses for the 2017 design year, and analyses of existing study area turning bay storage lengths and site access, the following improvements are recommended as being necessary for adequate transportation network operations (see **Figures ES-3 and 4**).

1) Retime the NC 54 Bypass interchange ramp terminal intersections to optimize overall capacity given the existing intersection geometrics and progression along NC 86 (S. Columbia Street) and US 15-501. Reoptimizing these two intersections with the rest of the US 15-501 corridor south of the interchange may improve operations at the signalized ramp terminals for 2017 build-out+1 year conditions, particularly in the PM peak hour.

The proposed signal timing improvements are recommended in order to mitigate existing system operational deficiencies and are recommended whether or not the Columbia Annex project is constructed.

2) To provide additional storage capacity for the NC 54 Bypass westbound off-ramp for 2017 build-out+1 year anticipated conditions, the existing inside left-turn lane storage bay at this approach to NC 86 (S. Columbia Street) should be lengthened from 275 feet to 650 feet. This will allow improved traffic flow during this signal phase and will improve the overall safety and efficiency of traffic flow at this intersection.





Town of Chapel Hill: Traffic Impact Study

Columbia Street Annex - Proposed Mixed-Use Development

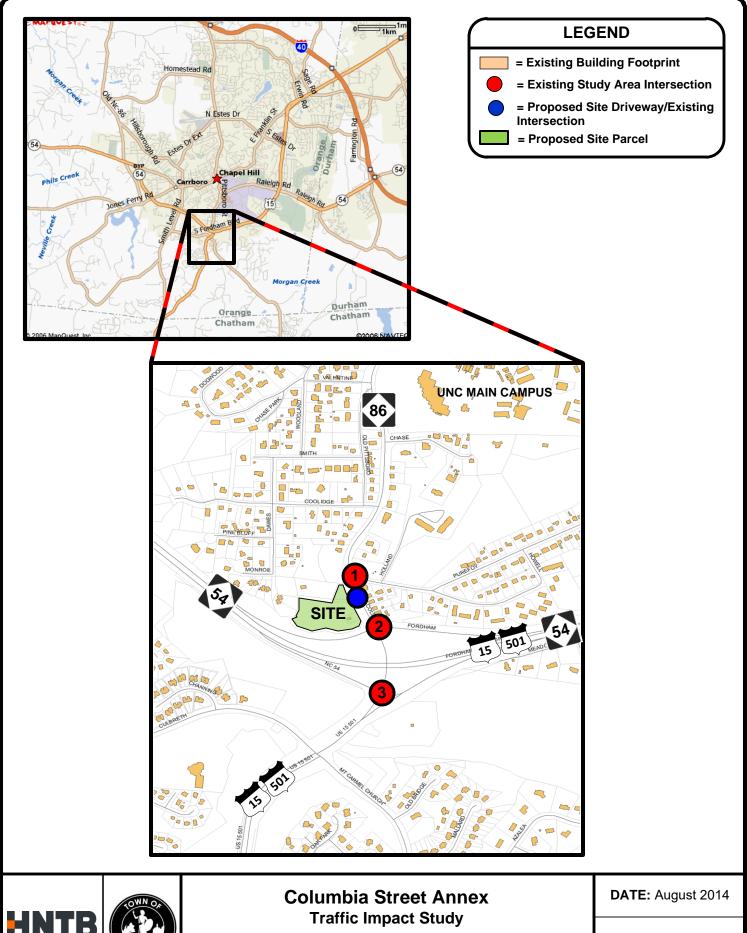
This improvement is recommended based on anticipated queue storage needs in 2017 with or without Columbia Street Annex site traffic impacts.

3) Due to the fact that the proposed site access driveway does not align with the existing Purefoy Road on the opposite side of NC 86 (S. Columbia Street), it is recommended that the Applicant consider investigating the feasibility for realigning the proposed site access to immediately opposite Purefoy Road. This improvement would not necessarily reduce the number of vehicular conflict points in this vicinity, but it would prevent left-turning movements from conflicting with each other from the driveway and Purefoy Road, and it would improve sight lines from the minor street approaches and provide a more orderly means of judging and accepting the limited available gaps in the high volume S. Columbia Street traffic pattern. In addition to this improvement, additional minor geometric modifications to this newly realigned intersection (see **Figure 14**) may enhance traffic operations and safety, as well as provide for the maximum amount of southbound through traffic storage at the adjacent NC 54 Bypass westbound off-ramp intersection, and provide for a short westbound left-turn pocket on Purefoy Road.

Capacity analysis results indicate that, though aligning the site driveway and Purefoy Road produces longer projected vehicular delays than if the driveway and Purefoy Road were operating as separate three-legged intersections with NC 86 (S. Columbia Street), in reality, there would be little safe opportunity for left-turning traffic from the site driveway and Purefoy Road to use the two-way left-turn lane refuge area (that is in actuality becomes a southbound through lane for NC 86). Aligning the driveway and Purefoy Road provides better clarity for delineation of acceptable gaps and turning movements. Maximum queue length results for the most critical approach (westbound left-turns) indicate there would be little difference (3-4 vehicles maximum queue) between the existing proposed offset configuration and the realigned configuration.

This improvement is recommended specifically for the Columbia Street Annex project.



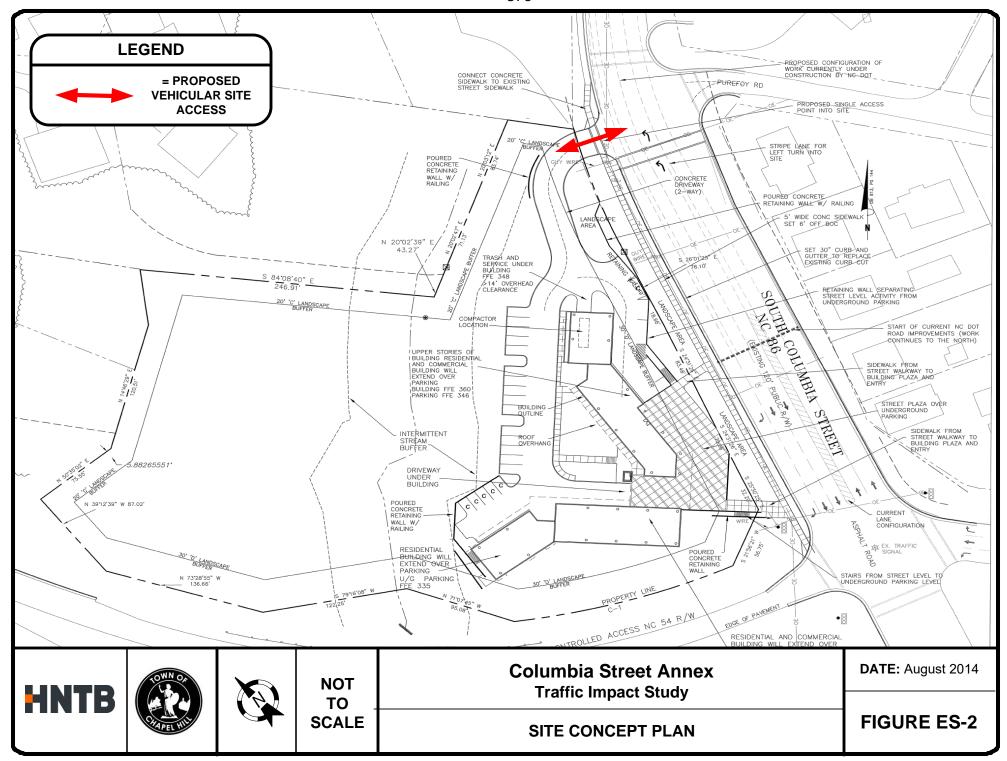


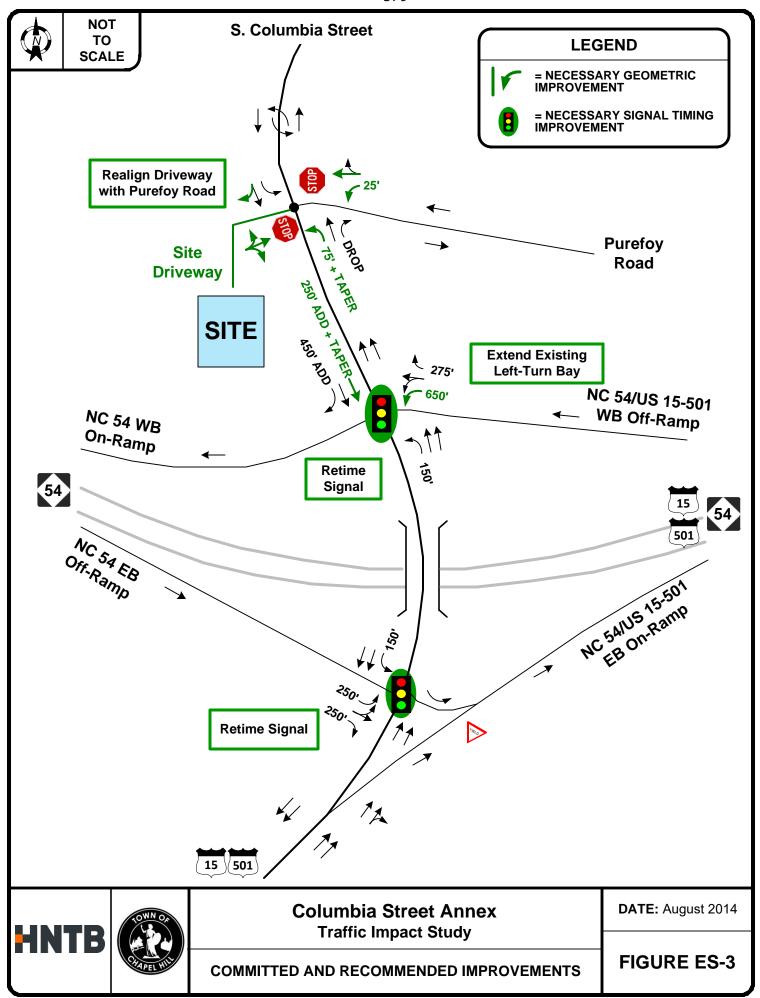
PROJECT STUDY AREA MAP

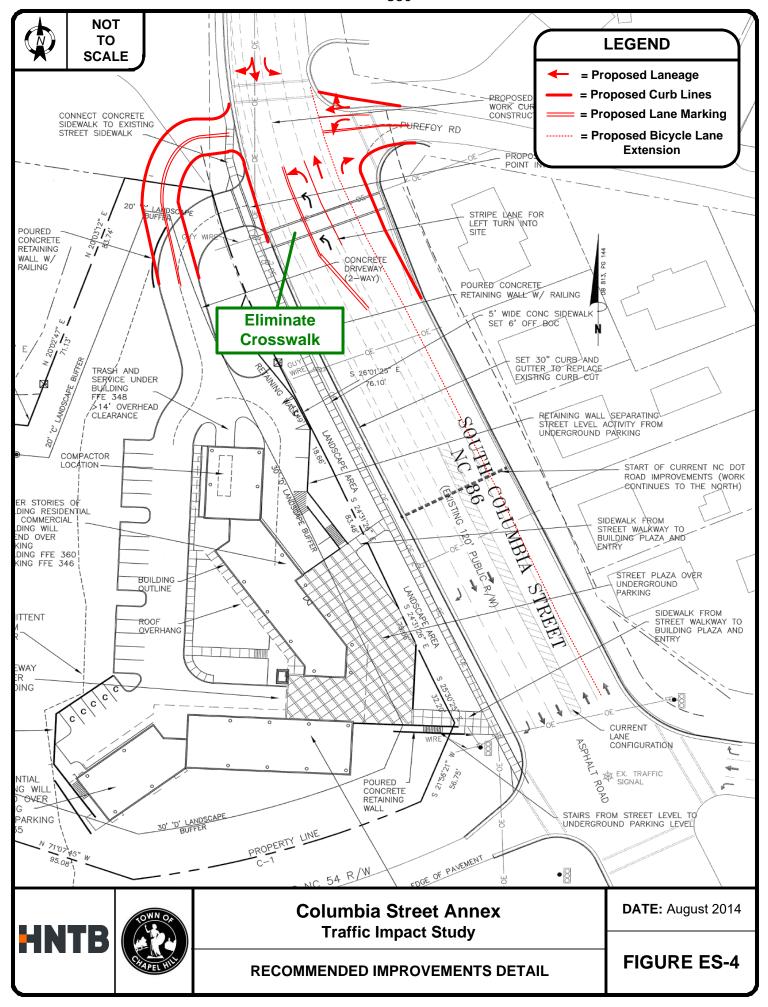
FIGURE ES-1











Prepared by Craig Scheffler, HNTB

December 3, 2020

15%

34%

Original 2014 TIS

Older ITE Trip Generation Data

				Daily			AM Peak Hour			Noon Peak Hour			PM Peak Hour		
Land Use	LUC Code	Density	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	
Apartment	220	39 Units	180	180	360	5	18	23	15	16	31	25	14	39	
Retail	820	7150 sq ft	153	153	306	4	3	7	8	9	17	13	14	27	
		Raw Total	333	333	666	9	21	30	23	25	48	38	28	66	
	Transit/Ped/Bike	e Reduction	50	50	100	1	3	5	3	4	7	6	4	10	
Vehicular Driveway Trips			283	283	566	8	18	25	20	21	41	32	24	56	
	Pass-By Trips (Retail Only)	26	26	52	0	0	0	2	3	5	4	5	9	
New Vehicular Trips Added to Network		257	257	514	8	18	25	18	18	36	28	19	47		

2020 Update

Updated ITE Trip Generation Data

			Daily			AM Peak Hour			Noon Peak Hour			PM Peak Hour		
Land Use	LUC Code	Density	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
Apartment (Mid-Rise)	221	52 units	141	141	282	5	13	18	10	12	22	14	10	24
		Raw Total	141	141	282	5	13	18	10	12	22	14	10	24
Т	ransit/Ped/Bik	e Reduction	21	21	42	1	2	3	2	2	3	2	2	4
New Vehicular	Trips Added	to Network	120	120	240	4	11	15	8	10	19	12	8	20
Apartment (original ITE														
Code)	220	52 units	176	176	352	6	20	26	14	16	30	21	12	33



Vegetated Riparian Buffers

Riparian Buffers

Vegetated riparian buffers are the areas adjacent to streams which are instrumental in reducing impacts to streams and the greater watershed from adjacent land use activities. Riparian buffers protect water quality by:

- filtering out pollutants from adjacent land uses;
- slowing and infiltrating runoff, which reduces peak flows and downstream flooding;
- stabilizing stream banks and stream channels, which helps prevent erosion;
- providing shade, which cools water temperatures, helps maintain dissolved oxygen levels, and reduce algae levels;
- facilitating the exchange of groundwater and surface water; and
- providing organic nutrients to support aquatic food webs.

Riparian buffers also improve air quality, provide important habitat for fish and wildlife, provide recreational opportunities, and have aesthetic value. These functions are well documented across numerous studies. In addition, protecting riparian areas allows ecological systems to be more resilient to natural and human-induced changes, including climate change.

There is an economic as well as environmental value in protecting riparian buffers, not just for property owners and the community, but also for the greater watershed. Jordan Lake supplies drinking water to nearly 700,000 Triangle residents, and offers important regional and statewide recreational opportunities, with a million or more visitors a year. Jordan Lake is also a future drinking water source for OWASA. According to NCDEQ (2016), riparian buffers provide many financial benefits to both the property owner and the community, including: decreasing the need for public investment in stormwater management, flood control and pollution removal; increased property values; and reduced land maintenance costs (compared to formal lawns and other landscaped areas). Protection of water quality by the protection of riparian buffers also lowers costs associated with expensive drinking water treatment plants or upgrades needed to purify water in degraded watersheds—saving money for municipalities and utilities.

In determining an appropriate riparian buffer width, the following four criteria are recommended considerations: the desired functions of the buffer, value of the resource protected, physical characteristics of the riparian area, and intensity of adjacent land use. The following figures provides buffer width ranges associated with the most accepted riparian buffer functions according to USDA (1998).



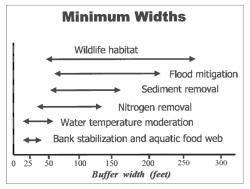


Figure 6 - 3. Range of minimum widths for meeting specific buffer objectives.

In 2002, the NC Wildlife Resources Commission published guidance for local governments regarding generalized mitigation measures for all watersheds within North Carolina to address indirect and cumulative impacts of projects to wildlife and water quality. This guidance recommends 100-foot forested buffers on perennial streams, 50-foot forested buffer on intermittent streams, and encourages the implementation of buffers on ephemeral streams. The guidance also recommends limiting impervious surfaces to less than 10% of the watershed.

Headwater Streams

Headwater streams include ephemeral, intermittent, and perennial streams that are the very beginnings of larger or streams and rivers. In the United States, headwater stream systems represent 79% of the overall river network and drain 70% of the land area. Small but often overlooked, headwater streams are of great importance to the ecosystem and are critical in terms of influencing water quality and quantity in streams and watersheds. Headwater streams, which are the smallest tributaries of streams and rivers in a watershed, serve as an essential hydrological and ecological linkage to downstream waters. Many small headwater streams in the Town of Chapel Hill form the headwaters of Bolin Creek, Booker Creek, Little Creek, and Morgan Creek, all of which drain to Jordan Lake.

Headwater streams are vulnerable to land development since they are relatively easy to fill in, channelize, or relocate due to their smaller flows. Loss of native vegetation, creation of compacted soils, addition of impervious area and associated storm drainage infrastructure from development eliminates the water quality improvement that would occur in a naturally flowing system and results in an increase in both the rate and volume of stormwater flow.

The North Carolina Department of Environment and Natural Resources (now NCDEQ) conducted a multiyear research study to examine the water quality and aquatic life values of headwater streams and wetlands across the state. Researchers determined that headwater streams provide significant reductions in nutrients and sediment across the state and provide a significant source of organic matter for downstream ecosystems. The research findings show that proper protection and management of headwater streams and wetlands is critical to protect downstream water quality and aquatic life in the state. (NCDWQ 2006).

Resource Conservation District (RCD)

Riparian buffer protection in Chapel Hill dates to 1984 when the initial Resource Conservation District (RCD) was approved and implemented along watercourses within the Town's planning jurisdiction. The RCD is intended to be applied to the areas within and along watercourses in order to:

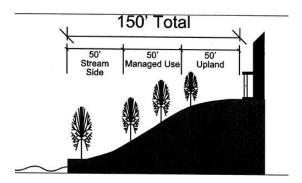
preserve the water quality of the Town's actual or potential water supply sources



- minimize danger to lives and properties from flooding in and near the watercourses
- preserve the water-carrying capacity of the watercourses and protect them from erosion and sedimentation
- retain open spaces and greenways and to protect their environmentally sensitive character
- preserve urban wildlife and plant life habitats from the intrusions of urbanization
- provide air and noise buffers to ameliorate the effects of development, and
- preserve and maintain the aesthetic qualities and appearance of the town. [LUMO Section 3.6.3].

The RCD ordinance establishes allowable uses and activities within the RCD buffer, and dimensional regulations for disturbed area and impervious area allowed on a property.

The resource conservation district riparian buffer for a perennial stream is 150 feet consisting of a 50 foot stream side area, 50 foot managed use area, and 50 foot upland area. The riparian width for an intermittent stream is 50 feet.

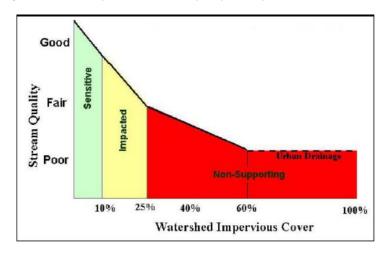


Stream and Watershed Impacts from Impervious Surfaces

According to research from the Center for Watershed Protection and many other sources, streams become impaired when impervious surfaces cover just 10% of a watershed. Streams in watersheds where impervious surfaces cover 25% of the watershed area typically cannot support aquatic life or attain water quality standards and are severely degraded from a physical and biological standpoint (CWP 2003).



Figure 1. Relationship between stream quality and imperviousness (CWP 2003).



Stormwater Control Measures (SCM)

The Town's stormwater ordinances require development projects to meet certain standards regarding water quality treatment, peak flow rates, and volume detention. These ordinances are typically addressed by the construction of a designed stormwater control measure (SCM) such as a wet pond, bioretention, permeable pavement. A meta-analysis of 52 modelling studies globally to relate stormwater control measures and hydrologic changes indicates that impervious surface mitigated by stormwater control measures does indicate reduction in runoff and peak flows (Bell 2000). Even with the runoff reduction, studies by Bell 2000 and Hopkins 2019 note that streamflow magnitude and timing were altered by urbanization even with SCMs treating 100% of the impervious area (Hopkins 2019). The prominent driver for streamflow response in these studies was the impervious coverage not the number or density of stormwater control measures.

Stream Restoration

Stream restoration or enhancement activities can achieve many objectives:

- Control severe stream bank erosion and instability
- Repair damage from floods
- Improve aquatic habitat or riparian forest conditions
- Compensate for impacts that result from highway projects or other site development

Stream restoration re-creates meanders, stabilizes soil, and installs gently sloping stream banks. Stream restoration is not always possible due to constraints such as utility crossings, structures, and roadways. A modest approach to restoration is to stabilize eroding stream banks and add native vegetation but does not address the natural flow of the stream. A basic restoration project consists of replanting stream banks and riparian areas. Restoration is a difficult and expensive project which takes time with respect to construction and maturation of the trees. Due to past degradation, stream restoration is an important aspect of watershed management in concert with preserving riparian buffers.

Citations

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North Carolina Wildlife Resources Commission (NCWRC). *Guidance Memorandum to Address Secondary and Cumulative Impacts to Aquatic and Terrestrial Wildlife Resources and Water Quality* (2002) https://www.ncwildlife.org/portals/0/Conserving/documents/2002 GuidanceMemorandumforSecondaryandCum ulativeImpacts.pdf

North Carolina Environmental Management Commission, Department of Environmental Quality. *Study of the State's Riparian Buffer Protection Program Pursuant to SL 2015-246* (2016) http://media2.newsobserver.com/content/media/2016/4/12/BufferReport-March2016.pdf

Palone, Roxane S. and Albert H. Todd. *Chesapeake Bay Riparian Handbook: A Guide for Establishing and Maintaining Riparian Forest Buffers* (1998) Report prepared for United State Department of Agriculture. https://www.fs.usda.gov/naspf/sites/default/files/publications/chesapeake_bay_riparian_handbook.pdf



Stream classification for the Columbia Street Annex Property

The Town of Chapel Hill uses the North Carolina Division of Water Resources (NCDWR) methodology to determine stream classifications. The three categories of classification are perennial, intermittent, and ephemeral as defined below according to NCDWG methodology. Citation of the appropriate section of the North Carolina Administrative Code (NCAC) is shown in the parentheses following each definition.

Perennial stream means a well-defined channel that contains water year-round during a year of normal rainfall with the aquatic bed located below the water table for most of the year. Groundwater is the primary source of water for a perennial stream, but it also carries stormwater runoff. A perennial stream exhibits the typical biological, hydrological, and physical characteristics commonly associated with the continuous conveyance of water. [15A NCAC 02B .0233(2)(i)]

Intermittent stream means a well-defined channel that contains water for only part of the year, typically during winter and spring when the aquatic bed is below the water table. The flow may be heavily supplemented by stormwater runoff. An intermittent stream often lacks the biological and hydrological characteristics commonly associated with the conveyance of water. [15A NCAC 02B .0233(2)(g)]

Ephemeral stream means a feature that carries only stormwater in direct response to precipitation with water flowing only during and shortly after large precipitation events. An ephemeral stream may or may not have a well-defined channel, the aquatic bed is always above the water table, and stormwater runoff is the primary source of water. An ephemeral stream typically lacks the biological, hydrological, and physical characteristics commonly associated with the continuous or intermittent conveyance of water. [15A NCAC 02B .0233(2)(d)]

Due to the broad variability in the natural characteristics of small streams and the impacts of urbanization that may result, some stream segments will not precisely fit the stream definitions and verification criteria. Such channels and/or channel segments shall be classified based on all available evidence including field investigation and map resources upstream of, within, and downstream of the segment in question.

Columbia Street Annex – Proposed Modifications to RCD Regulations

The proposed project entitled Columbia St Annex is requesting modifications to the RCD regulations which include

- Land disturbance beyond regulatory allowance in the managed use zone and upland use zones
- Imperviousness area beyond regulatory allowance in the upland zone
- Proposed parking and building are not allowable uses in the managed or upland zones
- Disturbing all steep slopes exceeding 25% on site which exceeds the allowable limit



Table Summarizing RCD Zone Disturbed and Impervious Areas

RCD Zone	Total Area	Proposed Disturbance	Maximum Disturbance by Code (LUMO)	Proposed Impervious	Maximum Impervious by Code (LUMO)
Streamside	35,935 sf	2,210 sf 6.15%	7,187 sf 20%	0	3,594 sf 10%
Managed	38,618 sf	23,100 sf 59.8%	15,447 sf 40%	5,380 sf 13.9%	7,724 sf 20%
Upland	34,668 sf	18,300 sf 52.8%	13,867 sf 40%	13,650 sf 39.4%	6,934 sf 20%

Over various stream determinations, the stream has consistently been considered "variant" for most of its length across the property meaning that natural stream environmental has been disturbed and is characterized by sediment deposition and diffuse flow. Excessive sedimentation from past land uses have buried natural stream features (and in some areas, the whole channel), and hydrology (baseflow) has been altered in these "variant" stream reaches as a result.

A stream classification is valid for 5 years, which was formalized by a council resolution on October 22, 2003. Over time, there can be changes in hydrology or watershed characteristics that impact a stream and thus its classification. Since 2011, hydrology has changed at this site and other sites in Chapel Hill. According to USGS stream gage data, streamflow (based on monthly mean flow) in the Chapel Hill area was below-average in 2011. The stream classification from the June 7, 2004 determination was perennial and was reclassified as intermittent on May 25, 2011. During the site visit on October 5, 2017, for the current stream determination, strong baseflow as observed for the upstream feature after several weeks of no rainfall and several aquatic organisms (larval salamander, damselfly nymph, and phantom cranefly larvae) were documented. These aquatic organisms are strong biological indicators that the stream reach is perennial (based on NCDWR research and guidance). In accordance with Town stream classification procedures, "variant" stream segments are classified the same as the stream segment immediately upstream for other than headwater segments. Since the upstream segment of this stream was determined to be perennial, the downstream "variant" segment located on this property is also classified as perennial.

The applicant appealed the Town's stream determination to the NC Division of Water Resources on March 28, 2018. The state conducted an on-site determination to review the feature on June 6, 2018. In an excerpt from their report shared below, Site C refers to a location north of the property and Site B is on the property.

"The DWR has determined that the stream at the locations labeled Site A and Site B on the attached map are "perennial" and subject to the Jordan Lake Buffer Rule.

The portion of the stream including Site C, though impacted, has been determined to carry the perennial" designation and is subject to the Jordan Lake Buffer Rule.

The feature, between Site C and Site B (labeled "Impacted Variant Section" on the attached map), has been heavily impacted by offsite sedimentation and buried under fill and debris from historic development activities. For regulatory purposes, this portion of the feature is



designated as " not subject" to the Jordan Lake Buffer Rule."

The state confirmed that the stream classification is perennial. This area is impacted by years of sedimentation and burial by fill and debris from historic development activities and invasive vegetation has colonized near Site C. A copy of NC DWR stream classification has been attached for reference.



ROY COOPER Governor MICHAEL S. REGAN Secretary LINDA CULPEPPER Interim Director

June 19, 2018

DWQ Project # 2018-0516 Orange County

Philip Szostak 310 ½ W Franklin St Chapel Hill, NC 27516

Subject Property: Columbia Street Annex, 1150 South Columbia Street

Unnamed Tributary to Morgan Creek

On-Site Determination for Applicability to the Jordan Lake Watershed Riparian Area Protection Rules (15A NCAC 02B .0267)

Dear Mr. Szostak:

On March 29, 2018, the Division of Water Resources (DWR) received your request to appeal an on-site determination made by the Town of Chapel Hill as provided in 15A NCAC 02B .0267 (4)(d). On June 6, 2018, Niki Maher and Shelton Sullivan of DWR conducted an on-site determination to review the feature located on the subject properties for applicability to the Jordan Buffer Rules (15A NCAC 2B .0267).

The feature was evaluated at two locations (labeled as "Site A" and "Site B" on the attached map initialed by Niki Maher on Jun 19, 2018) using the DWR Stream Classification Form. At Site A, the stream was evaluated to have a score of 32.5 on the DWR form. At Site B, the stream was evaluated to have a score of 33.5 on the DWR form. The form states that the "stream is at least intermittent if \geq 19 or perennial if \geq 30." The forms are attached to this letter.

An additional site was located below site A, labeled on the attached map as "Site C". Though the channel is still evident and biology found in wetted portions of the stream in this location support the stream continuing to carry the "perennial" designation, this portion of the stream around and below site C is heavily impacted by sedimentation. Approximately 18 inches of loosely associated sediment was measured in the channel, with the parent streambed substrate and water found beneath the sediment. Large scale debris and fill (from the parking lot upslope to the east) is still evident in the streamside areas east of the stream, but doesn't appear to encroach on the active channel as much at this location.

Further downstream of Site C, the area is characterized by a change in topography resulting in a broader, flatter valley. This area is impacted by years of sedimentation and burial by fill and debris from historic development activities, though rather than sediment confined to a discrete channel within a narrow valley as above (near Site C), the larger scale load is spread across the breadth of the valley floor and has been compacted over time, making it impossible to auger through to reach parent material. Further confounding determination efforts, invasive vegetation (privet, especially) has colonized

throughout, and the channel that is more obvious upstream is difficult to locate in this section. Though this area may have historically been a wetland area and/or included a stream channel within it, it appears to have normalized to its current impacted state. Over time, stormwater events may downgrade through sediment and through or around debris to the original channel, but the difficulty in making a stream determination at this time precludes DWR from regulating it as a perennial stream.

The DWR has determined that the stream at the locations labeled Site A and Site B on the attached map are "perennial" and subject to the Jordan Lake Buffer Rule.

The portion of the stream including Site C, though impacted, has been determined to carry the "perennial" designation and is subject to the Jordan Lake Buffer Rule.

The feature, between Site C and Site B (labeled "Impacted Variant Section" on the attached map), has been heavily impacted by offsite sedimentation and buried under fill and debris from historic development activities. For regulatory purposes, this portion of the feature is designated as "not subject" to the Jordan Lake Buffer Rule.

This determination shall replace the determination originally performed by the Town of Chapel Hill and shall expire five years from the date of this letter.

This determination can be contested as provided in General Statute 150B by filing a written petition for an administrative hearing to the Office of Administrative Hearings (hereby known as OAH) within sixty (60) calendar days.

A petition form may be obtained from the OAH at http://www.ncoah.com/ or by calling the OAH Clerk's Office at (919) 431-3000 for information. A petition is considered filed when the original and one (1) copy along with any applicable OAH filing fee is received in the OAH during normal office hours (Monday through Friday between 8:00am and 5:00pm, excluding official state holidays).

The petition may be faxed to the OAH at (919) 431-3100, provided the original and one copy of the petition along with any applicable OAH filing fee is received by the OAH within five (5) business days following the faxed transmission.

Mailing address for the OAH:

If sending via U.S. Postal Service:
Office of Administrative Hearings
6714 Mail Service Center
Raleigh, NC 27699-6714

If sending via delivery service (UPS, FedEx, etc.)
Office of Administrative Hearings
1711 New Hope Church Road
Raleigh, NC 27609-6285

One (1) copy of the petition must also be served to DENR:

William F. Lane, General Counsel Department of Environmental Quality 1601 Mail Service Center Raleigh, NC 27699-1601 This letter only addresses the applicability to the buffer rules and does not approve any activity within buffers or within waters of the state. If you have any additional questions or require additional information, please call Niki Maher at (919) 807-6367.

Sincerely,

Karen Higgins, Supervisor 401 & Buffer Permitting Unit

KAH/NM

Enclosures: USGS Topo, Soil Survey, Site Map, Stream ID Forms

cc: Allison Weakley, Town of Chapel Hill- via email: aweakley@townofchapelhill.org

Danny Smith, DWR RRO

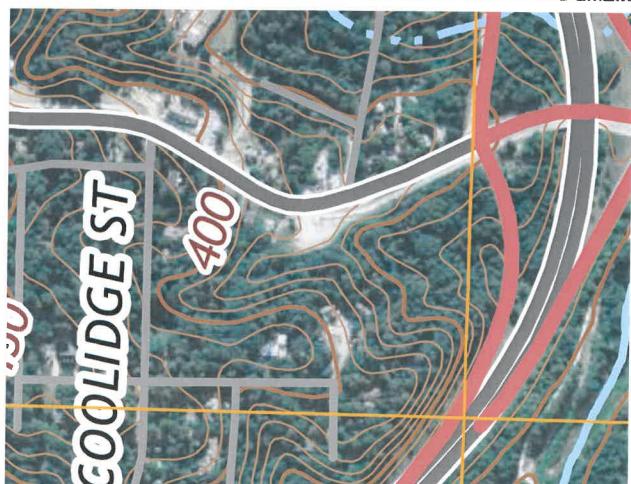
401 & Buffer Permitting Branch files

Filename: 18-0516SColumbiaSt(Orange)_bufferappeal

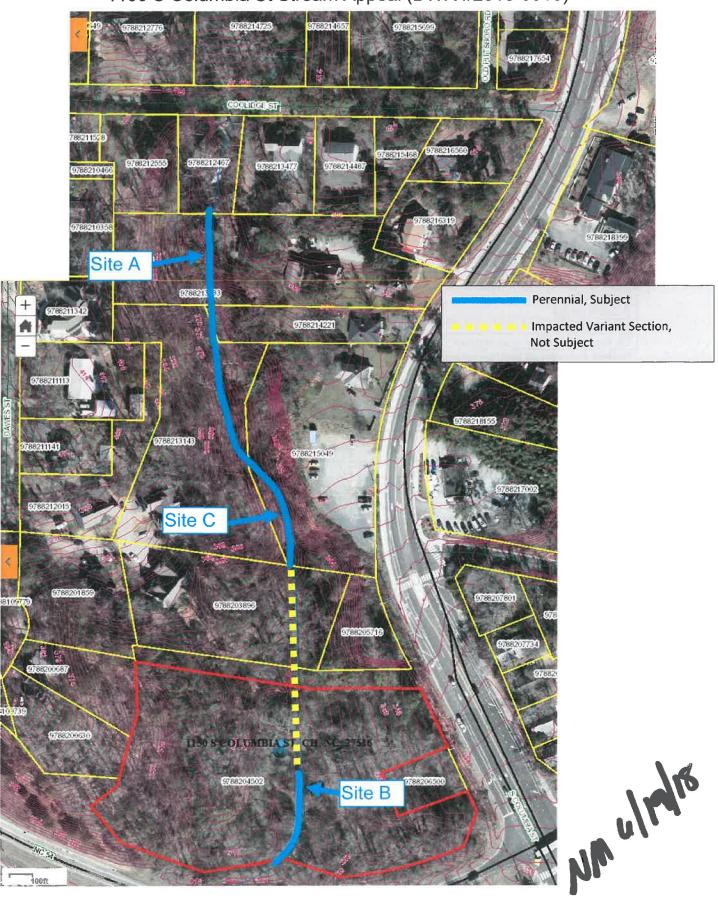
2016 USGS Topo Map, Chapel Hill Quad

1977 NRCS Soil Survey, Orange County





1150 S Columbia St Stream Appeal (DWR #2018-0516)



Site A

Date: 6/6/18	Project/Site:	Project/Site: 1505 (always 5 1975) County:		
Evaluator: NA Sos	A			
Total Points: Stream is at least intermittent if ≥ 19 or perennial if ≥ 30° 32.5	Stream Determ Ephemeral Int	Other e.g. Quad Name:		
. Geomorphology (Subtotal = 13.5)	Absent	Weak	Moderate	Strong
Continuity of channel bed and bank	0	1	(2)	3
Sinuosity of channel along thalweg	0 -	0	2	3
In-channel structure; ex. riffle-pool, step-pool, ripple-pool sequence	0	1	2	(3)
Particle size of stream substrate	0	1	2	(3)
Active/relict floodplain	(0)	1	2	3
Depositional bars or benches	(0)	1	2	3
Recent alluvial deposits	0	1	0	3
Headcuts	0	(1)	2	3
Grade control	0	(0.5)	1	1,5
). Natural valley	0	0.5	0	1.5
. Second or greater order channel	No	Yes =		
artificial ditches are not rated; see discussions in manual				
. Hydrology (Subtotal = \$)				
Presence of Baseflow	0	1	②	3
3. Iron oxidizing bacteria	00		2	3
Leaf litter	(13)		0.5	0
. Sediment on plants or debris	0	0.5	0.3	1,5
. Organic debris lines or piles	0	0.5		1.5
. Soil-based evidence of high water table?	No		Ye&=	
Biology (Subtotal = 1)	1.4	-	100	
B. Fibrous roots in streambed	(2)	2	1	0
Rooted upland plants in streambed	(3) (3)	2	1	0
Macrobenthos (note diversity and abundance)	0	1	(2)	3
. Aquatic Molfusks	0	0	2	3
. Fish	0	0.5	1	1.5
. Crayfish	0	63	1	1.5
. Amphibiens	0	0.5	(1)	1.5
. Algae	Ó	6 5	1	1.5
. Wetland plants in streambed	0	FACW = 0.75; OBL =		1.0
perennial streams may also be identified using other method	ds Seen 35 of manual	THE PROPERTY OF THE PROPERTY O	THE OFFICE A	
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Site B

NC DWQ Stream Identification For	m Version 4.11		_			
Date: 6/6/18	m 1 1001	Six B (MCH Six 5)	Latitude:			
Evaluator: NM / SOS	County: Ora	nge	Longitude:	Longitude:		
Total Points: Stream is at least intermittent if ≥ 19 or perennial if ≥ 30*		ermittent (Perennia)	Other e.g. Quad Nam	ther g. Quad Name:		
A. Geomorphology (Subtotal = 14.5)	Absent	Weak	Moderate	Strong		
13- Continuity of channel bed and bank	0	1	2	3		
2. Sinuosity of channel along thalweg	0	1	2	3		
In-channel structure: ex. riffle-pool, step-pool, ripple-pool sequence	D	A Section of the second section of the second section of the section of the second section of the section of th	2	3		
4. Particle size of stream substrate	0	1 -	2	3		
5. Active/relict floodplain	0 -	. O	2	3		
6. Depositional bars or benches	0	0	2	3		
7. Recent alluvial deposits	0	1	②	3		
8. Headcuts	0	0	2	3		
9. Grade control	0	0.5	a	1.5		
IO. Natural valley	0	(0.9)	1	1.5		
11. Second or greater order channel	Né	= ġ	Yes	2 3		
artificial ditches are not rated; see discussions in manual						
B. Hydrology (Subtotal = 10)						
2. Presence of Baseflow	0	1	2	3		
3. Iron oxidizing bacteria	0	1	(2)	3		
14. Leaf litter	₹.5	1	0.5	C		
5. Sediment on plants or debris	Q	0.5	Ø	1.5		
6. Organic debris lines or piles	0 (0.5)		1	1.5		
7. Soil-based evidence of high water table?	No	= 0	Yes =	3)		
C. Biology (Subtotat = 의)						
8. Fibrous roots in streambed	(3)	2	1	0		
Rooted upland plants in streambed		2	1	Q		
O, Macrobenthos (note diversity and abundance)	0	(Ď	2	3		
Aquatic Moliusks	0	1	(3)	3		
2. Fish	(0)	0.5	1	1.5		
3. Crayfish	(D)	0.5	1	1.5		
4. Amphibiens	(0)	0.5	1	1.5		
5. Algae	(0)	0.5	1	1.5		
6. Wetland plants in streambed		FACW = 0.75; OBL	= 1.5 Other ≠ 0			
*perennial streams may also be identified using other metho	ids, See p. 35 of manual.					
lotes:						
sketch: impas in mag in a company of a compa	iron quitacking	Starting to 115		ow calvers of		
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Physical 18

Site C

NC DWQ Stream Identification Form Version 4.11

Date: 6/7/18	Project/Site: (1505 Columbia 5	Latitude:
Evaluator: NM /SIS	County: Orange	Longitude:
Total Points: Stream is at least intermittent if ≥ 19 or perennial if ≥ 30°	Stream Determination (circle one) Ephemeral Intermittent Perennial	Other s.g. Guad Name:

A. Geomorphology (Subtotal =)	Absent	Weak	Moderate	Strong	
Continuity of channel bed and bank	0	1	2	3	
2. Sinvosity of channel along thalweg	0	1	2	3	
In-channel structure: ex. riffle-pool, step-pool, ripple-pool equence	0	1	2	3	
4. Particle size of stream substrate	0	1	2	3	
5. Active/relict floodplain	0	1	2	3	
6. Depositional bars or benishes	0	1	1/2	3	
7. Recent alluvial deposits	0	1	2	3	
8. Headcuts	Ö	1 ,	2	3	
9. Grade control	0	0.5	1	1.5	
10. Natural valley	0	0.5	1	1.5	
11. Second or greater order channel	No	# ()	Yes :	= 3	
artificial ditches are not rated; see discussions in manual	/				
B. Hydrology (Subtotal =)	\ /				
12. Presence of Baseflow		1	2	3	
13. Iron oxidizing bacteria	0	1	2	3	
14. Leaf litter	1.5	1	0.5	0	
15. Sediment on plants or debris	0	0.5	1	1.5	
16. Organic debris lines or piles	0	0.5	1	1.5	
7. Soil-based evidence of high water table?	No × O			3	
C. Biology (Subtotal =)					
8. Fibrous roots in streambed	3	2	1	0	
9. Rooted upland plants in streambed	3	2		0	
20. Macrobenthos (note diversity and abundance)	0	18	2	3	
₹1. Aquatio Mollusts	0	1	2	3	
22. Fish	Ö	0.5	1	1.5	
23. Craylish	0	0.5	1	1.5	
4. Amphibians	0	0.5	1	1.5	
5. Algae	0	0.5	1	1.5	
26. Wetland plants in streambed		FACW = 0.75; C)BL = 1.5 Other = 0		
perendial streams may also be identified using other methods	. See p. 35 of manual.				
Voles:		Dior s Walker			
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TOWN OF CHAPEL HILL

Town Hall 405 Martin Luther King Jr. Boulevard Chapel Hill, NC 27514

Item Overview

Item #: 16., File #: [20-0869], Version: 1 Meeting Date: 12/9/2020

East Rosemary Parking Deck Update.

Staff:Dwight Bassett, Economic Development

Manager's Office

Overview: General update on the East Rosemary Parking Deck project and consideration of parking technology.

General update:

- Walker Consulting developed Opinion of Probable Costs for Deck (financial)
- Completed TIA and created live modeling
- Discussed overall façade design with recommendations (CDC & Council)
- Walker Consulting completed over-sight review of plans and made recommendations
- Reviewed comments from Walker's oversight review with design Team
- Endorsement of EDA and began due diligence period
- Submittal of LOI to UNC and reviewing returned draft
- Working on final draft Wallace Deck Lease and beginning final review of Management Agreement and Site License Agreement
- Davenport review of the Financial Projections Models presented to Council
- Initiated an oversight review of financial projections based on occupancy and demand for parking by Walker Consultants

Ongoing:

- 1. North Street property acquisition Appraisal complete, offer made to property owner. The property owners are seeking their own appraisal but have found it may take months to complete.
- 2. North Street design Plat to determine right-of-way complete, design firm to begin to work on design and intersection improvements in the coming weeks.
- 3. Walker oversight review First review complete and shared with the design team. We held a work session to work through all the key points and changes and have a few issues left to negotiate.
- 4. Parking technology Greater technology than the walk-up pay stations to be able to track uses in the Deck.
- 5. UNC LOI returned with comments from our team and we are working to schedule a joint working meeting to work through key issues.
- 6. Façade Design Rosemary Street/East Façade: the architects have developed a potential refinement to the precast concrete "frame" in response to comments and truer colors. Samples to be provided for Council to see in person.(Attachement)
- 7. Street trees Exploring planting areas and underground utilities to understand planting beds first and then finalize tree types.
- 8. Due diligence work On-going soil borings/testing and finalizing title work.

Parking technology

1. Utilize the Parkeon technology and simple gate counters \$250,000.00

Item #: 16., File #: [20-0869], Version: 1

2. Gate and tag readers with pay stations	\$750,000.00
3. Gate, tag readers, pay stations and single space count and wayfinding system	\$1,300,000.00

- 1- The first item is the same kind of parking meters and gate counters we presented to Council when considering the East Rosemary project. There are extreme limitations on what is collected from a data perspective. This option would require hourly monitoring by a parking attendant to determine if there are cars parking that have not paid. This system also has no way to tracking or reserving spaces for monthly parkers.
- 2- The gate and license tag reader gives easier access to frequent and monthly customers as their information is on file with the tag. A parker can enter the gate, get a ticket and pay at a pay station. If pre-paid when exiting, tag reader opens gate for them for quick exit. If they attempt to exit without paying, they are required to pay at the automated gate with credit card. This option requires limited monitoring of the parking spaces and support can be given remotely by intercom.
- 3- The gate, tag reader, pay station and single space count is the ultimate in user friendliness and background data for monitoring the deck. There are two operational systems: the tag and gate and the single space count system. The data sources would be combined for single source of data to look at from a management perspective. This would give users, once they drive through the gate, a wayfinding system directing them to available spaces as well as a overhead marker system that shows them where the available spaces are located as they drive through the deck. This is extremely convenient and reduces harmful emissions as vehicle do not "circle" looking for spaces.



Recommendation(s):

That the Council consider giving direction on parking technology.

Decision Points:

- Does Council desire a more robust parking management system for the new parking Deck?
- Is Council willing to expand the budget to allow for the proposed PARCS (Parking and Revenue Control System) systems as labeled at #3?
- Should we continue using the same parking system as other location?

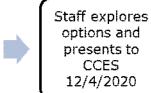
Key Issues:

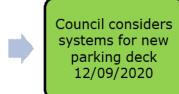
- Utilize same existing technology and limit the data output on parking
- Have an improved system for the parking deck that allows for easier management of open parking spaces

Fiscal Impact/Resources: up to \$500,000 for the third technology option.

Where is this item in its process?









Meeting Date: 12/9/2020



Attachments:

• Draft Staff Presentation

Item #: 16., File #: [20-0869], Version: 1

Meeting Date: 12/9/2020

- East Rosemary Parking Deck renderings
- Walker Consulting review of projected parking and finance for deck

The Agenda will reflect the text below and/or the motion text will be used during the meeting.

PRESENTER: Dwight Bassett, Economic Development Officer

RECOMMENDATION: That the Council provide direction on parking technology.

East Rosemary Redevelopment Proposal



December 9,2020



D R F H



RECOMENDATION:

Council consider giving direction on parking technology.

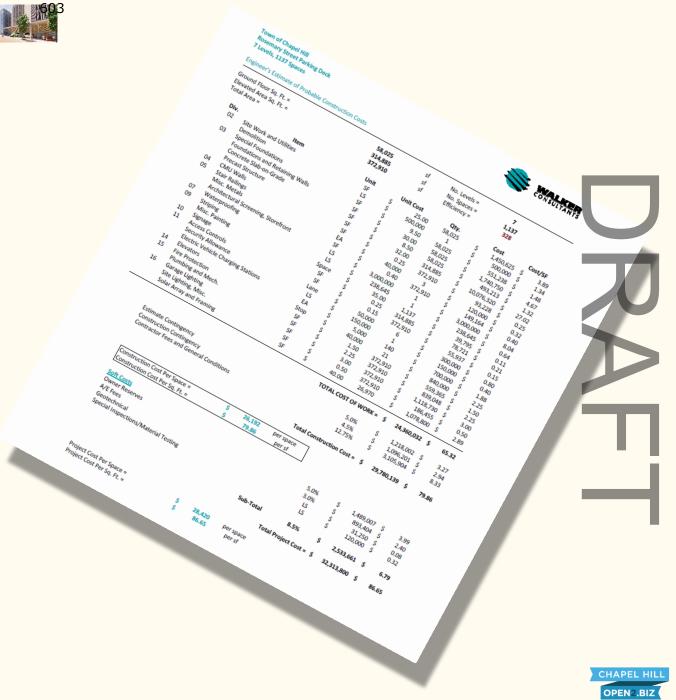






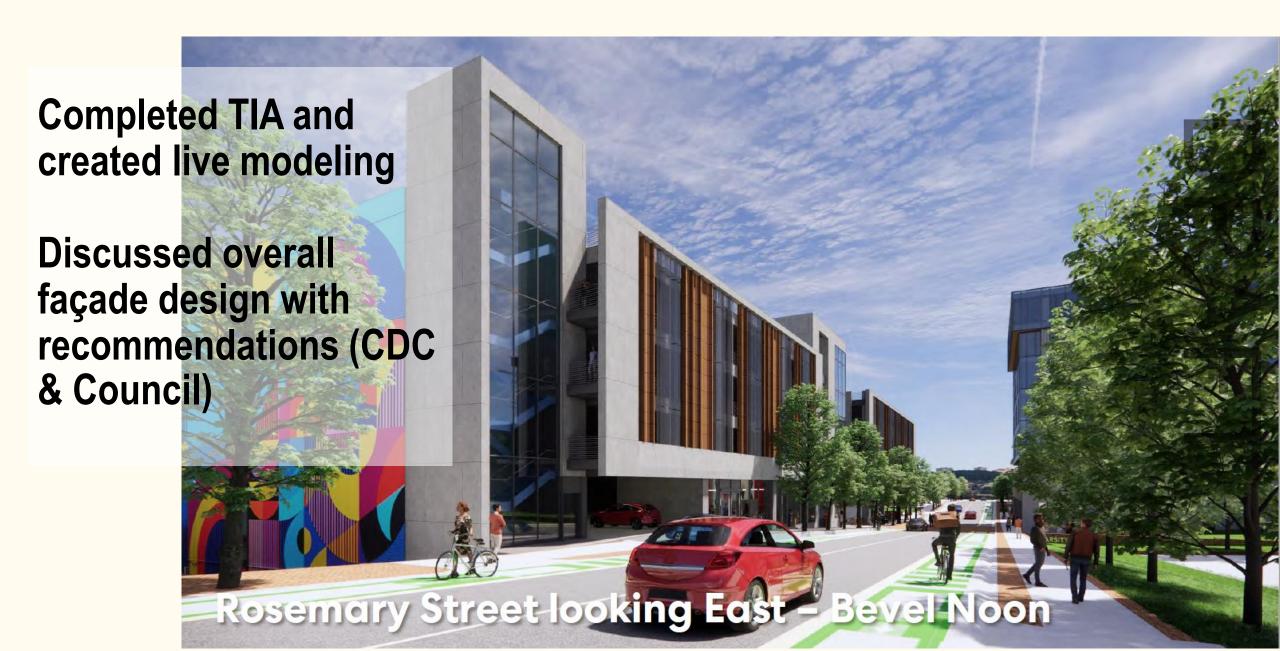
Walker Consulting developed **Opinion of Probable Costs** for Deck (financial)

Walker projection \$32,313,800 Samet/Grubb \$31,826,211 **Difference** 487,000*



^{*} Several differences include North Street land, solar array and other





- 1. Walker Consulting completed over-sight review of plans and made recommendations
- 2. Appraised 108 North Street and made a purchase offer
- 3. Reviewed comments from Walker oversight review with design Team
- 4. Endorsement of EDA and began due diligence period
- 5. Submittal of LOI to UNC and reviewing returned draft
- 6. Working on final draft Wallace Deck Lease and beginning final review of Management Agreement and Site License Agreement







Initiated a oversight review of financial projections based on occupancy and demand





Parcs technologies



- PROXIMITY CARD (PROX)
- AUTOMATIC VEHICLE IDENTIFICATION (AVI)
- BLUETOOTH ® TECHNOLOGY
- LICENSE PLATE RECOGNITION

REVENUE CONTROL SYSTEMS

- TRADITIONAL CASHIERING
- INTEGRATED CREDIT CARDS
- PAY-ON-FOOT (POF) TECHNOLOGY
- CREDIT CARD EXIT VERIFIER
- CREDIT CARD IN/OUT
- APPLE / GOOGLE PAY
- OR BARCODE READER
- PAY BY MOBILE DEVICE
- RESERVATIONS
- FREQUENT PARKER PROGRAMS

VALIDATION SYSTEMS

- OFFLINE VALIDATIONS
- CHASER TICKETS
- ONLINE VALIDATIONS









Essence of system:

- LPR (license plate recognition) system
- Gated control for tickets if LPR doesn't recognize tag
- Space occupancy system with wayfinding signs





Parking technology

<u>Utilize the Parkeon technology and simple gate counters</u>

\$250,000

The first item is the same kind of parking meters and gate counters we presented to Council when considering the East Rosemary project. There are extreme limitations on what is collected from a data perspective. This option would require hourly monitoring by a parking attendant to determine if there are cars parking that have not paid. This system also has no way to tracking or reserving spaces for monthly parkers.





Parking technology

Gate and tag readers with pay stations

\$750,000

The gate and license tag reader gives easier access to frequent and monthly customers as their information is on file with the tag. A parker can enter the gate, get a ticket and pay at a pay station. If pre-paid when exiting, tag reader opens gate for them for quick exit. If they attempt to exit without paying, they are required to pay at the automated gate with credit card. This option requires limited monitoring of the parking spaces and support can be given remotely by intercom.





Parking technology

Gate, tag readers, pay stations, single space count and wayfinding system

(up to)

\$1,300,000

The gate, tag reader, pay station and single space count is the ultimate in user friendliness and background data for monitoring the deck. There are two operational systems: the tag and gate and the single space count system. The data sources would be combined for single source of data to look at from a management perspective. This would give users, once they drive through the gate, a wayfinding system directing them to available spaces as well as a overhead marker system that shows them where the available spaces are located as they drive through the deck. This is extremely convenient and reduces harmful emissions as vehicle do not "circle" looking for spaces.







Parking technology

1. <u>Utilize the Parkeon technology and simple gate counters</u> \$250,000

2. Gate and tag readers and pay stations \$750,000

3. Gate, tag readers, pay stations and single space count and wayfinding system (up to) \$1,300,000

Staff believes that based on our current budget, we can cover up to \$750,000 out of our existing budget for parking technology and maintain a 5% contingency.







RECOMENDATION:

Council consider giving direction on parking technology.















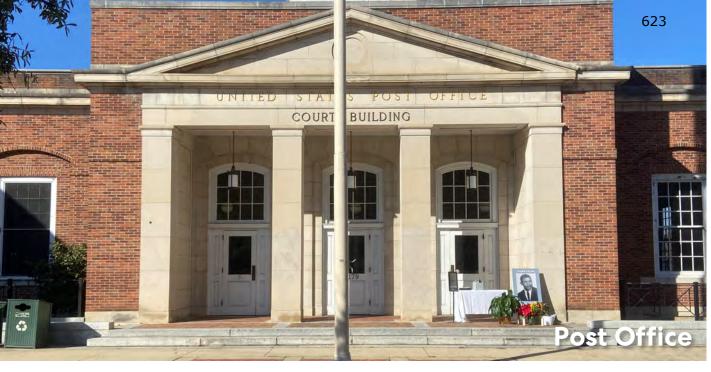








Material Palette









Precast Precedents







13860 Ballantyne Corporate Place Suite 140 Chapel Hill, NC 28277

704.247.6230 walkerconsultants.com

December 2, 2020

Mr. Dwight Bassett Economic Development Officer 405 Martin Luther King Jr. Boulevard Chapel Hill, North Carolina 28514

Re: Preliminary Financial Analysis

East Rosemary Garage

Walker Project # 19-001227.10

Dear Mr. Bassett,

Walker Consultants is pleased to submit the enclosed finalized letter report that discusses the potential parking revenue, operating expenses and debt service associated with the addition of a new 1,143 parking structure on East Rosemary Street in Chapel Hill, NC.

Projections of potential net income for the proposed garage over the initial 5-year period of operations, from opening through full stabilization of the facility are provided. Walker's preliminary financial model assumes that industry standard best-practices will be implemented to ensure successful management of the new structure as an automated commercial parking facility.

We appreciate this opportunity to provide professional parking consulting services for the Town of Chapel Hill and we look forward to discussing our report with you at your convenience.

Respectfully submitted,

WALKER CONSULTANTS

Jon Martens, ACIP, CAPP Parking Consultant

Parking Consultant

Jody Todd

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INTRODUCTION

Walker Consultants ("Walker") is pleased to submit our findings with regard to the preliminary financial analysis developed for Town of Chapel Hill (the "Client") regarding a public parking facility (the "Facility") proposed for development on the parcel that is currently occupied by the CVS Garage in Chapel Hill, NC (the "Subject Property") and adjacent surface lot.

The information used to prepare the enclosed preliminary financial analyses for the Subject Property was developed based on the following factors:

- 1) Chapel Hill Parking Management Plan, completed in 2019.
- 2) Parking demand data from ParkMobile and FlowBird system reporting for 2019. (pre-Covid)
- 3) Land-use program information provided by the Client.

OVERVIEW

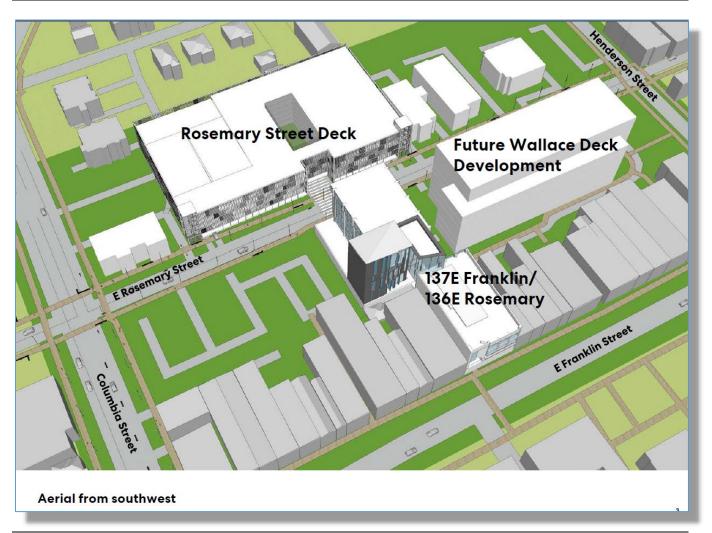
The Client engaged Walker to perform a preliminary financial analysis that is suitable for helping to judge the reasonableness of the Town of Chapel Hill's financial projections developed for the proposed 1,143-space parking garage. Using data provided by the Client and Walker's experience in developing preliminary financial models for commercial facilities, we developed a Base Case financial model that is intended to depict the projected parking revenues and expenses for a 5-year projection period, once the proposed developments are all online. The new parking garage is part of a larger-scale project that includes the following:

- East Rosemary Parking Garage—a new 1,143-space parking garage replacing the existing CVS Deck and private parking lot at 139 E. Rosemary Street;
- Wallace Office a new 200,000 square foot office building to be constructed at the site of the existing Wallace Parking Deck. The building is planned to include two floors of wet/lab space and 200+ spaces of underground parking; and
- **Grubb Office** a 118,000 square foot office redevelopment of the existing 137 E. Franklin Street building. The project will also displace approximately 25 parking spaces in the existing Rosemary/Columbia Lot.

The proposed new parking garage is intended to accommodate existing demand that will be displaced by the removal of the CVS and Wallace Decks and the displacement of surface parking in the 139 E. Rosemary Lot and the Rosemary/Columbia Lot. Additionally, new demand is anticipated from the two office building projects. This analysis includes the projected demand from these new land uses.



Figure 1: Planned Development



Source: Town of Chapel Hill, NC

EXISTING CONDITIONS

The proposed 1,143± space parking facility will reside on the CVS Deck site, 137 E. Rosemary Street. The CVS Deck contains 270± spaces. The facility accommodates monthly permit parking and daily parking. The new parking facility will also displace a 91 space-private parking lot at 139 E. Rosemary Street. This private lot accommodates only permit parking.

The Wallace Parking Deck is located just east of the project site, also along Rosemary Street. **The Wallace Deck contains approximately 309 parking spaces**, for both monthly permit holders and daily parkers. The Wallace Deck is planned to be removed and replaced with a 200,000 square feet office building ("Wallace Office Building"). The existing parking demand will be served by the proposed parking garage.



Transient revenue is collected using multi-space meters and ParkMobile, an app-based payment. Users park and pay for the amount of time they plan on parking before leaving the facility. The method requires enforcement to ensure users have paid for their parking. Permit parking is paid on a monthly basis.

An additional parking lot is located at the corner of Rosemary and Columbus Streets. The 104-space lot accommodates hourly parkers only. The majority of this lot will remain, however approximately 25 parking spaces will be displaced for green space adjacent to the redeveloped office building.

Figure 2: Existing Parking Facilities Map



Source: Google Earth 2020



PROPOSED PARKING CHANGES

The addition and displacement of parking spaces in the project area will result in a net increase in supply of approximately 648 spaces. The changes, by facility, are noted below:

Table 1: Changes in Parking Supply

Location	Spaces
New East Rosemary Garage	1,143
CVS Deck	(270)
139 E. Rosemary Lot	(91)
Wallace Deck	(309)
Rosemary/Columbia Lot (partial)	(25)
New Wallace Building (underground)	200
Net Change in Supply	648
Total Supply	1,343

PARKING DEMAND

Projected parking demand for the proposed garage is made up of 1) Absorbed demand to be relocated from displaced facilities and 2) New demand projected from the new office building developments.

<u>Absorbed Demand</u>, or existing parking demand, is anticipated from the displaced decks and surface spaces. To quantify this demand and separate monthly demand from transient demand we reviewed the parking study Walker performed in 2018, as well as permit parking data provided by the client.

Table 2: Projected Absorbed Parking Demand

Existing Parking Facility	Supply	Occupancy	%	Monthly Spaces	Hourly Spaces
CVS Deck (125 E. Rosemary)	270	200	70%	70	130
Wallace Deck	309	300	98%	50	250
139 E. Rosemary Lot	91	93	85%	91	0
Rosemary/Columbia Lot (partial)	25	104	100%	0	25
Absorbed Demand		Monthly Space	es	211	+
		Transient Space	es		405
Total Absorbed Demand					616

^{*}Occupancy figures are from the 2018 Walker Study

3.14

3.15



<u>New Demand</u> for the proposed office buildings has been calculated using a blend of widely accepted parking ratios within the industry as provided by publications "ITE Parking Generation 5th edition" and "ULI Shared Parking, 3rd edition". These publications are the industry standards for parking demand ratios and were informed by thousands of field parking occupancy studies performed by dozens of parking and transportation professionals over decades. These ratios have been vetted by a team of consultants who specialize in parking demand analyses and who mutually agreed upon the use of these ratios.

Table 3: New Demand Parking Ratios

Source	Category	Location	Range	Ave.	85th %tile	Ave.
Parking G	Generation 5th edition					
	General Office Building (710)	<u></u>				
	per 1000 sf GFA	urban	1.09 - 2.33	1.84	2.33	
	per 1000 sf GFA	urban	0.97 - 2.33	1.63	2.14	
	per 1000 sf GFA	urban/sub.	0.86 - 5.58	2.51	3.42	
	per 1000 sf GFA	urban/sub.	0.5 - 5.58	2.39	3.30	2.7975
	Research & Dvlpt. Ctr. (760)	_				
	per 1000 sf GFA	urban/sub.	1.27 - 3.55	2.58	3.14	
	per employee	urban/sub.	0.62 - 1.00	0.73	0.98	
ULI Share	ed Parking 3rd Edition					
	Office 100 to 500 ksf				3.55	3.55
	Account of the Ofth Demonstile					
	Average of the 85th Percentile					rounded
	Office				3.17	3.15

Sources: ITE Parking Generation, 5th edition (2020), ULI Shared Parking, 3rd edition(2020)

Research & Development

For the purposes of this analysis, we have used a blended demand ratio of 3.15/1,000 sf for the new office spaces. We estimate that 90% of that demand will be employees of those building and therefore will be demand for Monthly parking, with the remaining 10% will be allocated to transient demand for visitors. Based on these ratios, Walker projects a new parking demand of 702 spaces, 602 for monthly parking and 100 spaces for transient parkers. The following table shows the total demand projected for the new garage:



Table 4: Projected Parking Demand

	Square Feet	Ratio/1,000sf	Projected	Demand
New Demand			Monthly*	Transient **
New Wallace Building			90%	10%
Office Space	126,900	3.15	260	40
Wetlab Space	73,100	3.15	107	23
Renovated Office (Grubb) ***	118,000	3.15	335	37
Total New Demand			702	100
Absorbed Existing Demand				
CVS Deck			70	130
139 E. Rosemary Lot			91	0
Wallace Deck			50	250
Rosemary/Columbia Lot (partial)			<u>0</u>	<u>25</u>
Total Absorbed Demand			211	405
Total New Garage Demand			913	505
			TOTAL	1,418

^{*} Represents number of anticipated permits, not a daily presence.

NEW EAST ROSEMARY GARAGE

The new garage is proposed to have 1,143± total parking spaces. The University of North Carolina (UNC) is planning to purchase 100 of those spaces for their reserved use. UNC will contribute capital costs upfront for the purchase of these spaces and will not contribute towards annual revenue. The Town has assumed that UNC will contribute approximately \$40,000 annually to a capital repair fund. Walker has included that assumption in our analysis. Capital costs are reported as \$33,200,000, including financing costs, or just over \$30,000 per space. The client has reported that the intention is to finance this cost over a twenty-year term at a rate of 2%. This equates to about \$2M in annual debt service.

^{**}Represents 85th percentile demand, not a daily demand.

^{***}Monthly demand for the Wallace Office has been reduced by 200 to account for the underground spaces provided. within the new building.



PARKING REVENUES

Parking revenues have been projected based on the demand calculations presented in this report, with consideration to existing parking rates, length of stay for transient parkers and historical parking behaviors in the parking facilities to be replaced. As part of this analysis, Walker evaluated raw reporting data from the automatic pay station and mobile app payment reports from the existing CVS and Wallace decks from 2019. Based on this analysis, we have assumed the following assumptions in building our revenue model:

Assumptions:	
Total Spaces	1,143
Revenue Spaces	1,043
Fiscal Year 2022	7/01/21-6/30/22
Starting Rates:	
Monthly	\$125
Hourly	\$1.50
Hourly Presence:	
Length of Stay	3
Daily Turns	2.5
Days per year (Mon-Sat)	312
Office Occupancy:	
Year 1	85%
Year 2	95%
Year 3+	100%
Financing:	
Total Cost	\$33,200,000
Term	20
Rate	2%

OPERATING EXPENSES

Walker developed annual operating expense projections for the Facility using our experience and data contained in Walker's database of historical parking facility operating expenses. The expense projections have also been escalated annually to adjust for inflation.

The projected operating expenses assume the Facility will be managed and operated internally by the Town. The projected labor costs shown in the model are based upon estimated staffing schedules developed by Walker, with comparative analysis of historical expenses at the existing facilities and also with comparable properties from Walker's internal database. while all other expenses are projected on a cost per space basis. A brief description of the line-item categories included in the model follows below:

- Salaries, Benefits & Workers' Compensation: The staffing plan, positions, and wage structure projected are typical for other automated facilities operating within the market.
- <u>Utilities:</u> Assumes the projected cost of electricity, water, and telephone services.



- o <u>Insurance:</u> Includes the cost of general liability, garage keeper's legal liability, crime, umbrella, and auto insurance coverages; the cost of property insurance coverage is excluded from the Base Case model.
- Supplies: Includes the projected cost of garage and office supplies, parking tickets, forms, postage, replacement keycards for contract parking, lamps, bulbs, and employee uniforms.
- Routine Maintenance: Includes the anticipated annual expense for routine cleaning, painting curbs, relamping, life-safety system, doors, and locks, keys, basic parking equipment service agreement, waste removal, and pest control.
- Misc.: Assumes the projected cost for local business licenses and permits, driveway, sign and/or other permits paid to the local governing agency or agencies.
- General Expenses: Assume the following: base management fee, bank charges, payroll processing fees, auto damage claims, and armored car services; the projected cost of real estate taxes is excluded from the model.

As stated, Walker has assumed that the Facility will be managed and operated by the Town.

EXPENSES	<u>Per space</u>
Salaries & Benefits	\$110
Supplies	\$20
Security	\$40
Administration, (inc. Mgmt fees)	\$200
Insurance	\$30
Utilities	\$30
Routine Maintenance	\$30
Misc.	\$10
Projected Total Expenses	\$470

Source: Walker Consultants Operating Expense Research

Note: Operating expenses are based on operations as a standalone facility. When combined with the system, some efficiencies in staffing and security may reduce the cost on a per space basis.

FINANCIAL MODEL SUMMARY

Based upon the proposed project schedule, the parking garage will be open for parking in FY2022. The Grubb Office is proposed to be completed by late 2021 and the Wallace Office is not anticipated to be completed until late 2023. The projected proforma accounts for the delayed parking demand and a ramp-up of general parking demand. Based on the proposed project timeline, we identify the FY2025 as the year of stabilization. A positive Net Operating Income (NOI) is projected in all years. NOI is projected to cover debt service costs in FY2026. Financial projections and assumptions are presented on the following pages.



.			TV2028	FV2028	FV2024	stabilized year	EV2026	EV202E	F1/2020
Assumptions:			FY2022	FY2023	FY2024	FY2025	FY2026	FY2027	FY2028
	Ramp up Factor		0.70	0.85	0.95	1.00	1.00	1.00	1.00
1	Monthly Rates (inc. 4% bi-annually)		\$125	\$125	\$130	\$130	\$135	\$135	\$140
2	Hourly Rates (inc. 6% bi-annually)		\$1.50	\$1.50	\$1.60	\$1.60	\$1.70	\$1.70	\$1.80
3	Expense Inflationary Adj			1.02	1.02	1.02	1.02	1.02	1.02
	REVENUES								
	Monthly								
4	Absorbed Demand	(permits)							
	CVS Deck (125 E. Rosemary)	70	\$73,500	\$89,250	\$99,750	\$105,000	\$105,000	\$105,000	\$105
	139 E. Rosemary Lot	91	\$95,550	\$116,025	\$129,675	\$136,500	\$136,500	\$136,500	\$136
	Wallace Deck	50	\$52,500	\$63,750	\$71,250	\$75,000	\$75,000	\$75,000	\$75
5	New Demand								
	Wallace Office	367	\$0	\$0	\$444,529	\$522,975	\$550,500	\$550,500	\$550
	Grubb Office	<u>335</u>	\$298,988	\$405,769	\$477,375	\$502,500	\$502,500	\$502,500	\$502
	Projected Monthly Revenue	913	\$520,538	\$674,794	\$1,222,579	\$1,341,975	\$1,369,500	\$1,369,500	\$1,369
	Hourly	(spaces)							
6	Transient Parking	313	\$769,000	\$933,800	\$1,113,300	\$1,171,900	\$1,245,100	\$1,245,100	\$1,318
7	Addtl. Avl. until Wallace Office Opens (50%)	184	\$450,900	\$547,500					
	Projected Transient Revenue		\$1,219,900	\$1,481,300	\$1,113,300	\$1,171,900	\$1,245,100	\$1,245,100	\$1,318
	Total Projected Revenue		\$1,740,438	\$2,156,094	\$2,335,879	\$2,513,875	\$2,614,600	\$2,614,600	\$2,687
O	EXPENSES	Dorongoo							
8	Salaries & Benefits	<u>Per space</u>	¢12F 700	¢120.200	¢120.000	¢122.400	¢126 100	¢120.000	¢1.4
		\$110	\$125,700	\$128,200	\$130,800	\$133,400	\$136,100	\$138,800	\$14
	Supplies	\$20	\$22,900	\$23,400	\$23,900	\$24,400	\$24,900	\$25,400	\$2!
	Security	\$40	\$45,700	\$46,600	\$47,500	\$48,500	\$49,500	\$50,500	\$5 \$35
	Administration, (inc. Mgmt fees)	\$200	\$228,600	\$233,200	\$237,900	\$242,700	\$247,600	\$252,600	\$25
	Insurance	\$30	\$34,300	\$35,000	\$35,700	\$36,400	\$37,100	\$37,800	\$38
	Utilities	\$30	\$34,300	\$35,000	\$35,700	\$36,400	\$37,100	\$37,800	\$3
	Maintenance	\$30	\$34,300	\$35,000	\$35,700	\$36,400	\$37,100	\$37,800	\$38
	Misc.	\$10	\$11,400	\$11,600	\$11,800	\$12,000	\$12,200	\$12,400	\$17
	Projected Total Expenses	\$470	\$537,200	\$548,000	\$559,000	\$570,200	\$581,600	\$593,100	\$605
	UNC Capital Repair Fund Contribution		\$40,000	\$40,000	\$40,000	\$40,000	\$40,000	\$40,000	\$40
	NET OPERATING INCOME		\$1,243,238	\$1,648,094	\$1,816,879	\$1,983,675	\$2,073,000	\$2,061,500	\$2,12
9	Debt Service		\$2,030,403	\$2,030,403	\$2,030,403	\$2,030,403	\$2,030,403	\$2,030,403	\$2,030
	Net Cash Flow after Debt Service		(\$787,166)	(\$382,309)	(\$213,524)	(\$46,728)	\$42,597	\$31,097	\$92

Source: Walker Consultants, 2020



ASSUMPTIONS

<u>:</u>	
1	Rates increase by an average of 2% per year, implemented every other year.
2	Rates increase by an average of 3% per year, implemented every other year.
3	Expenses increase, annually, by 2%.
4	Absorbed monthly demand = The number of permits reported for each of the displaced facilities in FY2018 X the recording parking occupancy for those facilities.
5	New Demand = The calculated parking demand for the new buildings using a blend of ITE Parking Generation 5th edition and ULI Shared Parking, 3rd edition ratios of 3.15/ksf x 90% (10% allocated to transient demand for visitors).
6	Transient Demand= The calculated available spaces for hourly parking by Total supply minus (Permits issued x80% presence factor)=1043-(713x80%).
7	Additional Transient Spaces available until demand for the Wallace Office Building comes online.
8	Projected operating expenses based on Walker's database of nearly 200 comparable facilities and also cross-checked with Chapel Hill historical expenses.
9	Annualized debt service payment provided by client



LIMIT OF LIABILITY

Walker makes a distinction between "preliminary" financial analyses (projections for budgeting purposes) and "comprehensive" financial analyses (detailed analyses used as part of the financing process). The latter must be written with an eye towards the underwriters, who typically require significantly more detailed analyses, discussions of methodology and written support. Furthermore, because of the risk inherent in the financing process to both the client and Walker, we put the project through an intensive review by a team of advisors both internal and external to the company. Due to the additional analysis and review required to complete a comprehensive analysis, these type engagements are significantly more expensive, and we do not recommend them unless a client is at the point of obtaining financing.

Walker understands that this preliminary financial analysis will be used internally by the client to assess the overall value of the projected net income associated with operating and maintaining a public parking facility constructed on the Subject Property. Moreover, Walker's model is based upon projections and assumptions compiled using our experience in developing similar type models. Therefore, we cannot guarantee results, and the model developed is not intended to provide a level of detail needed to obtain project financing.

STATEMENT OF LIMITING CONDITIONS

- 1. This report is to be used in whole and not in part.
- 2. Walker's report and recommendations are based on certain assumptions pertaining to the future performance of the local economy and other factors typically related to individual user characteristics that are either outside Walker's control or that of the client. To the best of Walker's ability, we analyzed available information that was incorporated in projecting future performance of the proposed subject site.
- 3. Financial projections presented in this report are conceptual estimates in nature and will differ from actual results.
- 4. All information, estimates, and opinions obtained from Town of Chapel Hill and others not employed by Walker Consultants are assumed to be true and correct; we can assume no liability resulting from misinformation.
- 5. None of this material may be reproduced in any form without our written permission, and the report cannot be disseminated to the public through advertising, public relations, news, sales, or other media.
- 6. We take no responsibility for any events or circumstances that take place after the date of our field inspections.
- 7. The quality of a parking facility's on-site management will have a direct effect on a property's economic viability. The financial projections presented assume responsible ownership and competent management. Any departure from this assumption can have a significant impact on the projected operating results.
- 8. The estimated operating results presented are based on an evaluation of the overall economy, and neither consider nor make provisions for the effect of any sharp rise or decline in local or national economic conditions. We do not warrant that the projections will be attained, but they have been prepared based on information obtained during this study and are intended to reflect the expectations of a typical parking patron.
- 9. This report was prepared by Walker Consultants; the opinions, recommendations, and conclusions expressed for this assignment are rendered by Walker's staff members as employees, rather than as individuals.



- 10. This report is set forth as a preliminary financial analysis for the proposed Subject Property and is not an appraisal report.
- 11. The conclusions and recommendations presented were reached based on Walker's analysis of the information obtained from Town of Chapel Hill and our own sources. Information furnished by others, upon which portions of this study are based, is believed to be reliable; however, it has not been verified in all cases. No warranty is given to the accuracy of such information, and any significant differences between these assumptions and actual performance can have an impact on the financial projections for the proposed parking operation.