



TOWN OF CHAPEL HILL
Planning Department

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DESIGN STANDARDS APPROVAL

This certifies that the Design Standards for University Place have been reviewed by the Town Staff and the Community Design Commission, and approved by the Town Manager. Approval of the Design Standards, inclusive of the following conditions, is issued pursuant to Stipulation #8 of the Special Use Permit Modification approved by the Town Council on June 28, 2021.

Name of Applicant: Adam McGuire – Land Design
Name of Property Owner(s): RRPV University Chapel Hill LP – Jeff Kurtz
Location: 201 S. Estes Drive;
Orange County Parcel Identifier Number 9799-12-5797
Project Number: #19-130

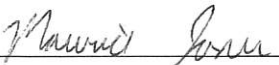
Description of Approval:

Standards for land use, street typologies, building mass and form, parking, drive-thrus, and outdoor amenity space. The standards shall support redevelopment of University Place according to the approved Framework Plan and the established intent, vision, and goals for design.

- Exhibit 1: Design Standards document last revised November 29, 2021 and approved January 12, 2022. Electronic copy attached.

Conditions:

1. Term of Approval: These Design Standards, as may be modified from time to time, shall remain in effect so long as the Special Use Permit Modification approved by the Town Council on June 28, 2021 (2021-06-28/R-6) remains effective and valid.
2. Design Standards Certification: Notwithstanding any exceptions identified in the Design Standards, submission of each Final Plans application for 201 S. Estes Drive shall include a signed and sealed certification that the proposed plan complies with these approved Design Standards.
3. Modification of Design Standards: Minor modifications to these Design Standards may be approved by the Town Manager in accordance with the section titled 'Administration.' Any change that would alter the terms of the approved Special Use Permit Modification shall constitute a major modification and shall be subject to approval by the Town Council.



Issued By: Maurice Jones, Town Manager

Date: 1-12-22

Exhibit 1:

Design Standards document last revised November 29, 2021 and approved January 12, 2022.

UNIVERSITY PLACE

DESIGN STANDARDS

November 29, 2021



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I. INTRODUCTION

INTRODUCTION

INTENT, VISION & GOALS

INTENT

The Special Use Permit (“SUP”) modification and Design Standards are intended to accommodate the evolution of a traditional enclosed mall into a vibrant mixed use community. The owner intends to preserve existing buildings and infrastructure where possible while maintaining enough flexibility to develop new improvements in a manner and at a time that is responsive to market demand. The owner seeks to create a public realm with the pedestrian experience in mind. The owner proposes to modify and create an internal street network through the site to create a more complete street network that will allow pedestrian, bicycle and automobile traffic to travel around and through the site. The overall development plan is intended to allow the site to evolve with the surrounding area, resulting in a well-designed, quality experience for the visitors, employees and residents, as well as the larger community. These Design Standards provide the basic framework for the site plan, addressing overall design goals, allowable uses, street frontages, building massing, building form, parking standards, drive-thru standards, outdoor amenity space and defined development options.

VISION

The redevelopment will transform University Place into a walkable town center and introduce new uses to complement the existing commercial uses. To realize this vision, parts of the existing mall will be retained and other parts may be demolished over time to make room for new buildings and outdoor spaces. Those parts retained will be reconfigured to convert space internally focused into new outward facing shops and restaurants designed for the pedestrian. Several public gathering spaces will be created of varying scale with everything from smaller semi-private areas and outdoor space for smaller groups to large social spaces. The larger outdoor amenity spaces will act as focal points, providing space for activities like the farmers market, outdoor fitness classes and other regular programming as well as special events throughout the year.

GOALS

The redevelopment of University Place will be defined and shaped by overarching design goals that act as a guide across all the development pods. All development must strive to meet the goals of inviting site design, public place connectivity, architectural character, block size & scale and street character. These elements will shape the built environment into an overall walkable town center.



INTERPRETATION OF LANGUAGE

All provisions, terms, phrases and expressions contained in these Design Standards shall be liberally construed in order that the true intent and meaning of the Planning Staff may be fully implemented. All words and terms used have their commonly accepted and ordinary meaning unless they are specifically defined in the Town of Chapel Hill Land Use Management Ordinance and Design Manual or the context in which they are used clearly indicates to the contrary. The Planning Staff and/or Town Manager are responsible for making any interpretations of the language in these Design Standards. When vagueness or ambiguity is found to exist as to the meaning of any word or term used, any appropriate canon, maxim, principle or other technical rule of interpretations or construction used by the courts of this state may be employed to resolve vagueness and ambiguity in language.

TERMS

In addition to the terms listed below, refer to all definitions in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual.

1. **Affordable dwelling unit (for sale):** A dwelling unit intended for sale that is restricted for occupancy by a household within the target income prescribed in Section 3.10 of the LUMO.
2. **Affordable dwelling unit (rental):** A dwelling unit intended for rent that is restricted for occupancy by a household within the target income as further defined herein.
3. **Applicant:** Any person or his/her duly authorized representative who submits an application as defined herein.
4. **Blank wall area:** Blank wall area means a portion of the exterior facade of the building that does not include: windows or doors; columns, pilasters or other articulation greater than twelve (12) inches in depth; or a substantial material change (paint color is not considered a substantial change).
5. **Building depth:** The largest total dimension of a building footprint measured perpendicular to the primary frontage.
6. **Building facade:** The face of a building that delineates the edge of conditioned floor space.
7. **Building height:** Building height is measured in both the number of stories and in feet. Building height is the vertical distance from the average grade at the foundation to the highest portion of the structure. Average grade is determined by calculating the average of the highest and lowest elevation along natural or improved grade (whichever is more restrictive) along the front of the building parallel to the primary street setback line.
8. **Build-to Zone (BTZ):** The portion of a lot which must contain a primary building facade or open space.
9. **Building step back:** The building step back shall be measured as the horizontal change in the building wall plane, perpendicular to the applicable frontage or property line.
10. **Building width:** The largest total dimension of a building footprint measured parallel to the primary frontage.
11. **Certificate of occupancy:** A document issued by the building inspector certifying compliance with all applicable state and local laws, including all terms of an approved zoning compliance permit, and authorizing occupancy of a building or structure.
12. **Design Alternative:** Where proposed alternative(s) to the requirements herein are found to be in keeping with the design goals and intent described in this document, staff may approve these Design Alternatives as part of a minor modification to the Special Use Permit (SUP). Design Alternatives do not constitute a substantial modification to the SUP. Design Alternatives shall be evaluated in terms of building form, aesthetic quality, general development order and public realm design quality, and must provide an equivalent or better result than would otherwise be required, as determined by Town Manager. Design Alternatives may also be approved where existing conditions, utility conflicts, sight lines, fire access or other unforeseen requirements conflict with the provisions herein. If staff does not approve the Design Alternative, the applicant may appeal to the Board of Adjustment.

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TERMS (CONTINUED...)

15. **Developer:** A person who undertakes development activities.
16. **Development:** Any man-made change to improved or unimproved real estate, including, but not limited to: the construction, structural alteration, enlargement, or rehabilitation of any buildings or other structures, including farm buildings; mining; dredging; filling; grading; paving; excavation or drilling operations; clearing vegetation; division of a parcel of land into two (2) or more parcels or some changes in use of structures or land. Development may also include any land disturbing activity on real estate that changes the amount of impervious surfaces on a parcel.
17. **Driveway:** Vehicular way, other than a street or alley, that provides vehicular access from a street to or through off-street parking and/or loading areas.
18. **Dwelling unit:** A single unit providing complete independent living facilities for one (1) or more persons, including permanent provisions for living, sleeping, eating, cooking, and sanitation.
19. **Dwelling units, multifamily:** A dwelling or combination of dwellings on a single lot consisting of three (3) or more dwelling units.
20. **Floor:** The top surface of an enclosed area in a building (including basement), i.e. top of slab in concrete slab construction or top of wood flooring in wood frame construction. The term does not include the floor of a garage used solely for parking vehicles.
21. **Front building setbacks:** Measured from the edge of the nearest right-of-way line.
22. **Gross land area:** All area within the boundaries of a zoning lot (net land area) plus half of the following areas located within or adjoining the lot: (1) publicly-owned or otherwise permanently dedicated open space, such as parks, recreation areas, water bodies, cemeteries and the like, and (2) public rights-of-way; provided that the total amount of credited open space and public streets shall not exceed ten (10) percent of the net land area of the zoning lot.
23. **Ground floor elevation:** The height of the ground floor relative to the height of the sidewalk and it is measured from top of the abutting curb to the top of the finished ground floor.
24. **Height (of a structure or part thereof):** The vertical distance from the average finished grade at the foundation along the street facade to the highest portion of the structure, or part thereof. To determine mean finished grade, take the spot elevations from the highest and lowest points of the foundation. The average of these two (2) spot elevations is the mean finished grade and the elevation from which height measurements are made.
25. **Height limitation, setback height:** The maximum height allowed for any structure located at the perimeter setbacks on a zoning lot, as defined by the applicable minimum setback lines. The setback height is the lower limit of the vertical portion of the building envelope. Setback heights are established in division 3.8.2(f) of the LUMO.
26. **Hotel or motel:** A building or group of buildings containing in combination ten (10) or more lodging units intended primarily for rental or lease to transients by the day or week, as distinguished from residence halls, in which occupancy is generally by residents rather than transients. This term does not include short-term rentals.
27. **Impervious surface:** A surface composed of any material that impedes or prevents natural infiltration of water into the soil.
28. **Land Use Management Ordinance (LUMO):** Appendix A of the Town of Chapel Hill Code of Ordinances.
29. **Minimum parking requirements:** The minimum number of parking spaces required pursuant to subsection 5.9.7(c) of the LUMO or as modified within the project SUP application.

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TERMS (CONTINUED...)

- 30. Minor Change or Modification of the Special Use Permit (SUP):** The Town Manager is authorized to approve minor modifications in the approved final plans as long as such changes continue to comply with the approving action of the Town Council and all other applicable requirements, but shall not have the authority to approve changes that constitute a major modification of the Special Use Permit. Any change requiring evidential support in addition to that presented at a public hearing on applications for the original Special Use Permit or subsequent modifications of Special Use Permit shall constitute a major modification of the Special Use Permit, instead of a minor change. This is a defined process that should reference Section 4.5.4 of the LUMO. If the proposed action is determined to be a minor modification, the Town Manager shall require the filing of an application for approval of the minor modification. An application for modification of a Special Use Permit shall be reviewed in accord with the procedures established in subsection 4.5.3 of the LUMO.
- 31. Module offset:** The module offset shall be measured as the horizontal change of a portion of the building wall plane at ground level, perpendicular to the applicable frontage. The module width shall be measured for the portion of the wall plane closer to the sidewalk, while the width of offset shall be measured for the portion further from the sidewalk. The dimensions of modules and offsets should maintain a sense of proportion to the building as a whole.
- 32. Outdoor Amenity Space:** Outdoor Amenity Space must be provided on the lot, or lands permanently designated as publicly accessible open space, and must be available as unenclosed exterior space appropriately improved as a pedestrian amenity or for aesthetic appeal and cannot include areas used for vehicles, except for incidental service, maintenance or emergency actions. Outdoor Amenity Space shall be made available to the general public.
- 33. Pedestrian connectivity:** A publicly accessible route between buildings that allows pedestrians to move from one (1) side of a building or lot to another through a privately owned or publicly dedicated area. The route must connect to or allow future connection to other such routes, sidewalks, greenways, or thoroughfares. Pedestrian connectivity may include a through-street or alley, and may be designed as a trail, greenway or other similar passage. Vehicular use may be allowed as desired by the applicant, provided that the design of the pedestrian route prioritizes pedestrian movements.
- 34. Preserved building:** A building is considered to be preserved if it is shown to remain, in full or part, on the framework plan within this document.
- 35. Primary elevation of a building:** The elevation is the side of a building which fronts the main street serving the building. It will usually contain the main architectural features such as large windows, the main entrance door, lighting, signage and canopies or awnings.
- 36. Primary frontage:** The side of a building which faces a public right-of-way, or which faces the most prominent internal street typology if there is no adjacent public right-of-way.
- 37. Primary height:** The primary height limitation is the maximum height allowed for any structure located at the minimum setback required for the structure.
- 38. Primary entrance:** An entrance providing both ingress and egress, operable to residents at all times or to customers during normal business hours that is required along each side of the building facing a public streetscape or any other non-vehicular, publicly accessible area.
- 39. Proposed development:** The development requested by an applicant that includes all buildings and land uses subject to an application.
- 40. Public realm:** The streetscape or any other non-vehicular, publicly accessible area located along the designated frontage of a street, alley shared between sites, or non-vehicular thoroughfare.
- 41. Right-of-way:** A fee simple dedication of private property or an easement, whereby public access and utility easements are granted.

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TERMS (CONTINUED...)

42. **Reconfiguration:** A change in the form or design of an existing development or structure. Reconfiguration shall be treated as development if it involves substantial improvement as defined in Appendix A of the LUMO.
43. **Resource Conservation District (RCD):** An overlay zoning district to protect stream corridors and prevent property damage from floods. The RCD is intended to preserve water quality, minimize danger and property damage from flooding, protect streams from erosion and sedimentation, and preserve urban wildlife corridors and plant habitats. The RCD is also used to manage development in FEMA regulatory floodplains.
44. **Secondary frontage:** The side of a building which faces an Internal Typical Driveway Typology but which are not designated as the Primary Frontage. Note: If a building faces multiple Internal Typical Driveways one (1) must be designated as the Secondary Frontage.
45. **Secondary height:** The secondary height is the absolute maximum height allowed for any structure and increases at a rate of one foot in height for every one foot of distance interior to the lot, measured away from the setback of any public street frontage. The secondary height allowed as measured in number of stories shall increase at a rate of 1 additional story for every 10' feet of distance interior to the lot, measured away from the setback of any public street frontage.
46. **Setback, street:** The horizontal distance between the street lot line of a zoning lot and any structure on such zoning lot, measured perpendicular to the street lot line.
47. **Short-term rental (STR):** A dwelling unit rented in whole or in part for fewer than thirty (30) consecutive days for a fee or other valuable consideration as defined in section 6.27 of the LUMO or a lodging unit contained in a building that primarily contains dwelling units.
48. **Special Use Permit (SUP):** A permit issued by the council authorizing the development of a zoning lot for a special use or a planned development.
49. **Story height:** The height of each story of building and it is measured from the top of the finished floor to the ceiling above.
50. **Street:** A right-of-way or easement thirty (30) feet or more in width containing a roadway which provides or is used primarily for vehicular circulation. A service lane is a type of street that is primarily used for service-related vehicular circulation and typically does not have any primary building frontage, but may still provide pedestrian connectivity.
51. **Street facing facade:** A building facade which directly abuts Willow Drive, Fordham Boulevard, Estes Drive, an internal main street driveway, internal typical driveway or service lane.
52. **Street, private:** A street consisting of a private easement and a privately maintained roadway.
53. **Street, public:** A street consisting of a publicly dedicated right-of-way and a roadway maintained by the Town of Chapel Hill or the State of North Carolina.
54. **Substantial improvement:** Any repair, reconstruction, or improvement of a structure, which increases or decreases the existing building footprint by 25% or more, as shown on the Framework Plan within this document, before the improvement or repair is started. The term does not, however, include either (1) any project for improvement of a structure to comply with existing state or local health, sanitary, or safety code specifications which are solely necessary to assure safe living conditions, or (2) any alteration of a structure listed on the National Register of Historic Places or a State Inventory of Historic Places. In the event of casualty or condemnation where the building is destroyed and has to get rebuilt, the building can be built back to previous state without having to adhere to these Design Standards.
55. **Thoroughfare:** A route provided for the purposes of creating connectivity and/or establishing blocks, to include all types of streets, alleys, and non-vehicular paths and greenways with a defined right-of-way.
56. **Town Council:** The governing body of the Town of Chapel Hill, consisting of a mayor and eight (8) council members, as established in the Charter of the Town of Chapel Hill.

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TERMS (CONTINUED...)

- 57. **Town Manager:** The Town Manager of the Town of Chapel Hill, or his or her designee.
- 58. **Transparency:** The minimum percentage of windows and doors that must cover a ground or upper story facade. Transparency is required for any building facade facing a street.
- 59. **Use:** The specific activity or function for which land, a building, or a structure is designated, arranged, intended, occupied, or maintained.
- 60. **Use, accessory:** A use on the same lot or in the same structure with, and of a nature and extent customarily incidental and subordinate to, the principal use of the lot or structure.
- 61. **Use, principal:** The primary or main use of land or structures, as distinguished from a secondary or accessory use.
- 62. **Use, special:** A use of land, buildings, or structure that is identified in Appendix A of the LUMO as a use that because of its inherent nature, extent, and external effects, requires special care in the control of its location, design, and methods of operation in order to ensure protection of the public health, safety, and welfare.
- 63. **Variance:** A relaxation of the strict terms of a specific provision of Appendix A of the LUMO authorized by the board of adjustment in accord with the provisions of section 4.6 of Appendix A of the LUMO.
- 64. **Zoning Compliance Permit (ZCP):** A permit issued by the Town Manager authorizing the recipient to make use of property in accord with the requirements of this appendix.

II. OVERALL DESIGN GOALS

1. SITE DESIGN AND PUBLIC PLACE CONNECTIVITY

- A. Place buildings on the site to define positive outdoor space.** An outdoor space is positive when it has a distinct and definite shape, as definite as the shape of a room, and when its shape is as important as the shapes of the buildings which surround it. In contrast, a negative space is shapeless, vague, and amorphous. Plan outdoor spaces as part of overall site design and for each project.
- B. Activate street frontages to provide an inviting, interesting and comfortable pedestrian environment.** Use a harmonious “urban” palette of landscape materials (including plant materials, paving materials, and site furnishings) to establish a sense of visual continuity and sense of place. When a building wall is set back from the public streetscape or a natural feature, design the intervening space to be attractive to pedestrians.
- C. Promote “greenness” throughout the development.** Include canopy trees, planter beds and additional plant materials whenever possible, to continue the green tradition of Chapel Hill. Seek opportunities to increase the amount of plant material along sidewalks, in pocket parks and other public outdoor amenity spaces.
- D. Define corners at a key intersections with distinctive design elements to provide visual interest, an active street edge, and to create gateways throughout the development.** Distinctive design elements may include iconic design features, ground floor design, Outdoor Amenity Space, primary building entrance, public art, and building corner articulation.
- E. Integrate public art.** Locate public art strategically to frame or enhance a public view or corridor, encourage the use of public outdoor amenity space, activate recreational space, or create visual interest on blank walls along a site.
- F. Incorporate bicycle parking facilities in highly visible and accessible locations.** Design bicycle parking facilities to be covered.
- G. Design outdoor amenity spaces throughout the site in such a manner that they will be usable year-round.** Connect these amenity spaces with pedestrian pathways or sidewalk systems that create a complete and interconnected public space network throughout the site.
- H. Divide a large parking area into interconnected, smaller modules with landscape buffers.**
- I. Use best efforts to visually screen mechanical and electrical equipment, as well as trash, recycling and loading areas, from public view.** Integrate service areas and utilities into the design of adjacent buildings using compatible materials.
- J. Design the multiuse path such that it is inviting and connective.** Create a pathway that can be easily accessed and invites the public to enjoy and make use of the path at many points throughout the project. Ensure that the path incorporates plantings, seating and shade trees. Consider innovative water features that will bring people to use and enjoy the space. Locate the path and connections to the path to create a trail-like feature that will connect throughout the overall site and beyond to other parts of Chapel Hill.
- K. Create overall site design.** Ensure that features such as the multi-use path and streets are defined in the overall site design. Connect each pod’s site design, green spaces, landscaping and building placement so that it is cohesive with the overall site design.
- L. Use methods and materials to minimize water runoff and flooding.** Wherever possible, use surfaces that will slow down water flow. Incorporate pervious surfaces and reduce parking surfaces.

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2. ARCHITECTURAL CHARACTER

- A. Use innovative new design that draws upon regional design traditions.** Design a building to provide a sense of authenticity in its form and materials.
- B. Establish a sense of human scale in the design of a new building.** Use vertical and horizontal articulation techniques to reduce boxy or monolithic building masses. Use materials to convey a sense of human scale and visual interest to pedestrians, and apply them in units, panels or modules. Create a sense of texture through shadow lines which also provide a sense of depth and visual interest. Reduce large scale massing wherever possible.
- C. Use design to foster a unique sense of place.** Incorporate design features such as well-defined entries or tower elements into the design of a new development that is large-scale or located in a highly-visible location.
- D. Design a building to provide interest on all sides that will be viewed from the public realm.**
- E. Use shielded and focused light sources to prevent glare and light pollution.**
- F. Exposed parking structures should be architecturally compatible with the rest of the development.** Where feasible, provide elements which enhance the pedestrian experience along the ground-floor frontage of these structures.

3. BLOCK SIZE AND SCALE

- A. Where possible, large development pods should be broken down into smaller block sized components,** more typical of traditional urban blocks which improves the human scale of an urban environment and promotes walkability.
- B. Blocks can be defined on their edges by either streets or pedestrian facilities such as walkways, greenways, or other linear public spaces.**
- C. Providing additional pedestrian connections through mid-blocks, or other routes across or through portions of a block is encouraged.** Outdoor Amenity Spaces should be provided between buildings to help break up the scale of blocks.

4. STREET CHARACTER

- A. The internal street system of the redeveloped mall will not only provide a means for vehicular circulation but streets must also be designed to be welcoming, comfortable, and interesting public places for pedestrians.**
- B. Establish entrances to major buildings and retail shops along the streets to activate the street frontages of new buildings and the renovated mall structure.** Provide ample glazing along these frontages, pedestrian-scaled signage, interesting storefront design, and elements such as awnings and overhead canopies to enhance walkability.
- C. Design the internal streets with an urban character,** featuring sidewalks, street trees, amenity zones for restaurant seating or gathering, and other landscape elements where appropriate.
- D. Design the entrance drives to include sidewalks, street trees, and other urban streetscape features** to provide a sense of entrance and connection to the surrounding neighborhood context.
- E. Where possible, provide unique signage, or artwork at site entries** to create the sense of a “gateway” at each major entrance to the redeveloped mall.

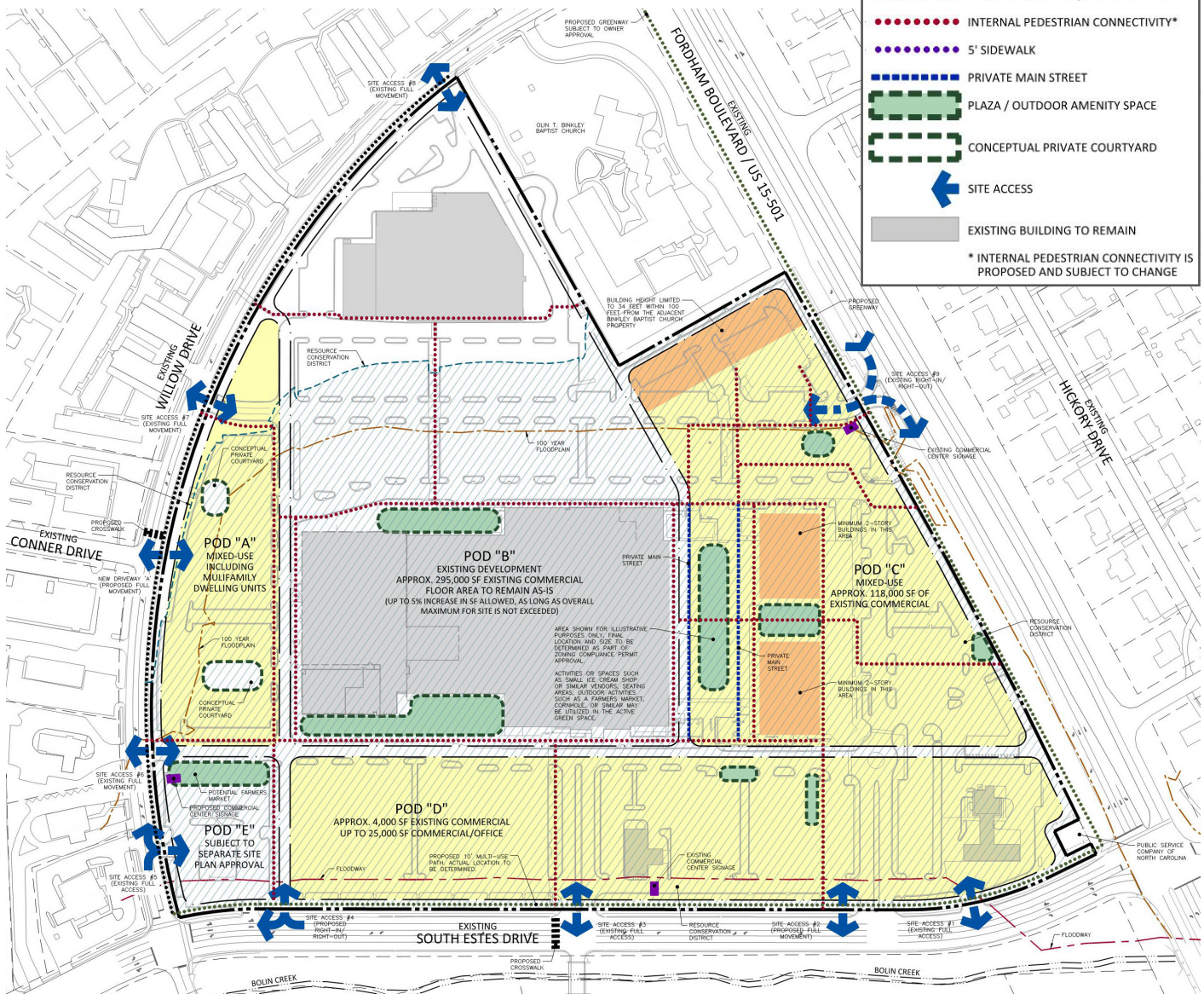
III. FRAMEWORK

The site plan configures the property into different development areas referred to as “Pods”. The configuration of each Pod is based on the current built environment (buildings, road network and areas protected by tenant’s lease rights), the proposed future built environment, as well as the natural resources surrounding the property such as the floodway, floodplain and Resource Conservation District (“RCD”). Within each Pod, development is dictated by specific rights designated to that Pod as well as overall densities allowed for the property as a whole. While overall densities are outlined in the Data Table on the site plan, the Design Standards will generally serve as the guide for development within each Pod. Public plazas and green spaces are placed throughout the property’s development and are connected by pedestrian-oriented walkways and streetscapes.

LEGEND:

	SITE AREA (PIN 9799125797)
	RESOURCE CONSERVATION DISTRICT (RCD)
	100 YEAR FLOODPLAIN
	FLOODWAY
	DEVELOPMENT POD
	FUTURE GREENWAY / MULTI-USE PATH
	INTERNAL PEDESTRIAN CONNECTIVITY*
	5' SIDEWALK
	PRIVATE MAIN STREET
	PLAZA / OUTDOOR AMENITY SPACE
	CONCEPTUAL PRIVATE COURTYARD
	SITE ACCESS
	EXISTING BUILDING TO REMAIN

* INTERNAL PEDESTRIAN CONNECTIVITY IS PROPOSED AND SUBJECT TO CHANGE

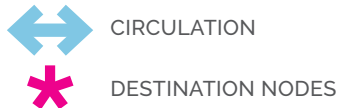


FRAMEWORK PLAN

Note: Connections are diagrammatic to show linkages between pods. The final location of connections will depend upon final building layout.

The proposed redevelopment envisions a lively pedestrian environment with numerous connection points between the project perimeter and the various uses within. The pedestrian network is intended to provide direct connections to all site features, including buildings, markets and outdoor amenity spaces.








LEGEND:

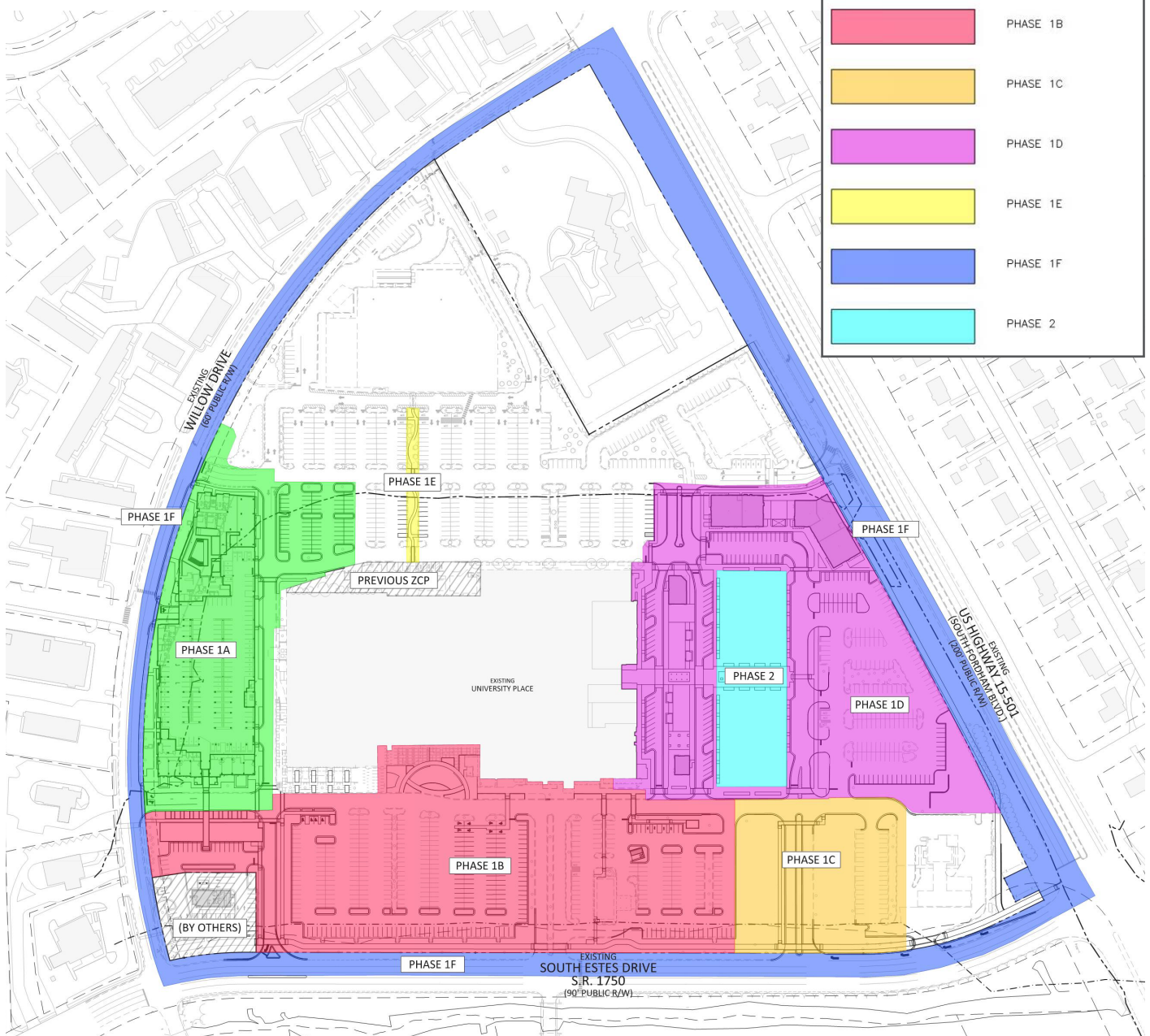


Note: This plan is conceptual in nature and subject to change. Illustration is provided to demonstrate overall intent of pedestrian connectivity only and should not be construed to imply specific connections, building arrangements, or other site elements.

The site will be broken into multiple phases for permitting and construction as generally illustrated in the plan below. Phasing will not necessarily follow the outline of the development pods due to market demands, construction sequencing and other logistical needs which will require flexibility and adaptability of the plans. Throughout each phase, access to the site and various existing stores will be maintained to the greatest extent possible and as required to ensure proper emergency access, at a minimum. Required roadway improvements will similarly be phased to provide proper circulation and access.

LEGEND:

	PHASE 1A
	PHASE 1B
	PHASE 1C
	PHASE 1D
	PHASE 1E
	PHASE 1F
	PHASE 2



PHASING PLAN

Note: The phasing plan is conceptual in nature and subject to change.

Each Pod within the Site Plan contains allowable use types that include commercial, office, residential and hotel, each of which are further defined below. For uses not specifically defined within the Design Standards for University Place, the uses permitted within Community Commercial ("CC") zoning district shall govern allowed uses unless otherwise listed herein as specifically prohibited.

PERMITTED USES					
RESIDENTIAL USES	POD A	POD B	POD C	POD D	POD E
Dwelling Units, as listed below:					
Single Family	--	--	--	--	--
Single Family w/ accessory apartment	--	--	--	--	--
Duplex	--	--	--	--	--
Multifamily - 3 to 7 dwelling units	P	--	P	--	--
Multifamily - Over 7 dwelling units	P	--	P	--	--
Live-Work	--	--	--	--	--
Triplex	--	--	--	--	--
Upper Story	--	--	--	--	--
Fraternity Dwelling	--	--	--	--	--
Group Care Facility	--	--	--	--	--
Home Occupation	A	--	A	--	--
Home Occupation, Major	--	--	--	--	--
Independent Senior Living Facility	P	--	P	--	--
Manufactured Home Park	--	--	--	--	--
Manufactured Home, Class A	--	--	--	--	--
Manufactured Home, Class B	--	--	--	--	--
Residence Hall	--	--	--	--	--
Residential Support Facility	--	--	--	--	--
Rooming House	--	--	--	--	--
Shelter	--	--	--	--	--
Short-Term Rentals	--	--	--	--	--
Tourist Home	--	--	--	--	--
OFFICE USES	POD A	POD B	POD C	POD D	POD E
Business, Office-Type	P	P	P	P	P
Clinic	P	P	P	P	P
Hospital	--	--	--	--	--
Institutional Uses, as listed below:					
College or University	P	P	P	P	P
Fine Arts Educational Institution	P	P	P	P	P
Place of Worship	P	P	P	P	P
Public Cultural Facility	P	P	P	P	P

KEY

P = Permitted as Principal Use
A = Permitted as Accessory Use
S = Permitted as Special Use
-- = Use Not Permitted

Public Service Facility	A	A	A	A	A
Public Use Facility	P	P	P	P	P
School, Elementary or Secondary	P	P	P	P	P
Vocational School	P	P	P	P	P
Flex Office	--	--	--	--	--
Flex Space	--	--	--	--	--
Maintenance & Storage Facility	A	A	A	A	A
Research Activities	P	P	P	P	P
Research Activities, Light	--	--	--	--	--
HOTEL USES	POD A	POD B	POD C	POD D	POD E
Hotel or Motel	P	--	P	--	--
COMMERCIAL USES	POD A	POD B	POD C	POD D	POD E
Adult Day Care Facility	A	A	A	A	A
Agriculture, except as listed below:	--	--	--	--	--
Community Garden	A	A	A	A	A
Automatic Teller Machines (Walk-up)	P	P	P	P	P
Automatic Teller Machines (Drive-up)	P	P	P	P	P
Automotive Repair	P	P	P	P	P
Automotive, Trailer & Farm Sales or Rental	--	--	--	--	--
Bank	P	P	P	P	P
Barber Shop / Beauty Salon	P	P	P	P	P
Business - Convenience	P	P	P	P	P
Business - General	P	P	P	P	P
Business - Wholesale	P	P	P	P	P
Car Wash	A	A	A	A	A
Cemetery	--	--	--	--	--
Child Day Care Facility	P	P	P	P	P
Club	P	P	P	P	P
Drive-in Window / Drive-thru Facilities	P	P	P	P	P
Essential Services	P	P	P	P	P
Extraction of Earth Products	--	--	--	--	--
Food Truck	A	A	A	A	A
Funeral Home	--	--	--	--	--
Hangar, Medical Aircraft	--	--	--	--	--
Kennel	A	A	A	A	A
Landfill	--	--	--	--	--
Manufacturing, Light	A	A	A	A	A

KEY

P = Permitted as Principal Use
 A = Permitted as Accessory Use
 S = Permitted as Special Use
 -- = Use Not Permitted

Outdoor Skateboard Ramp	--	--	--	--	--
Park/Ride	A	A	A	A	A
Parking, Off-Street	A	A	A	A	A
Personal Services	P	P	P	P	P
Place of Assembly, +2,000 Seating Capacity	--	--	--	--	--
Place of Assembly, up to 2,000 Seating Capacity	P	P	P	P	P
Publishing and/or Printing	A	A	A	A	A
Recreation Facility, Commercial	P	P	P	P	P
Recreation Facility, Non-Profit	P	P	P	P	P
Recreation Facility, Outdoor Commercial	A	A	A	A	A
Self-Storage Facility, Conditioned	--	--	--	--	--
Service Station/Convenience Store	--	--	--	P	P
Solid Waste Management Facility	--	--	--	--	--
Supply Yard	A	A	A	A	A
Temporary Portable Building, Construction Related	A	A	A	A	A
Temporary Portable Building, Not Construction Related	--	--	--	--	--
Veterinary Hospital or Clinic	P	P	P	P	P
Water & Wastewater Treatment Plant	--	--	--	--	--
WIRELESS COMMUNICATION FACILITIES	POD A	POD B	POD C	POD D	POD E
Collocation on existing tower or base station	P	P	P	P	P
Small Wireless Facility concealed: new base station or new dual purpose tower	P	P	P	P	P
Small Wireless Facility non-concealed: new base station or new tower	P	P	P	P	P
Macrocell Facility: new concealed base station	A	A	A	A	A
Macrocell Facility: new concealed dual-purpose tower	S	S	S	S	S
Macrocell Facility: new non-concealed base station	P	P	P	P	P
Macrocell Facility: new non-concealed tower	S	S	S	S	S

KEY

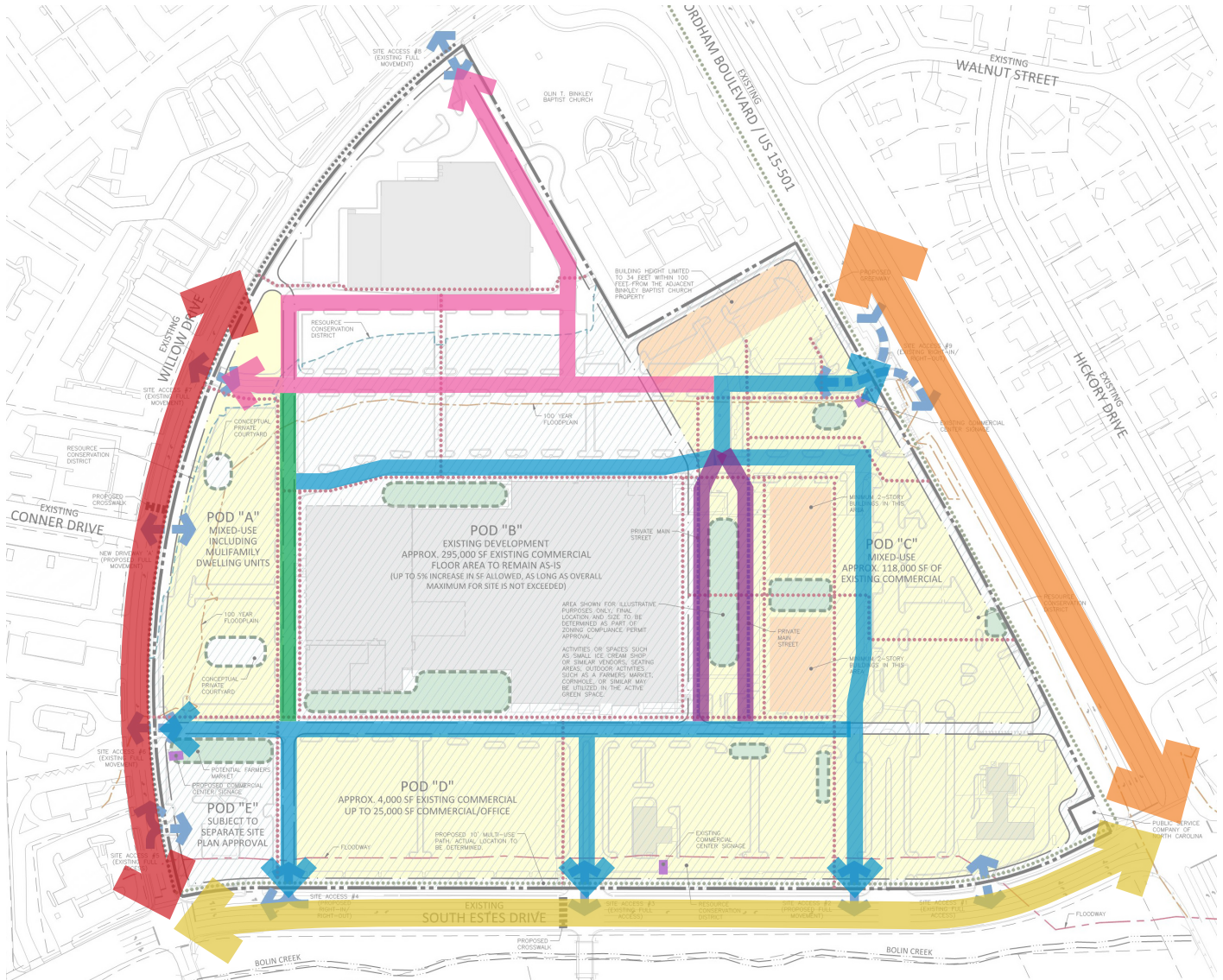
P = Permitted as Principal Use
 A = Permitted as Accessory Use
 S = Permitted as Special Use
 -- = Use Not Permitted

NOTES

1. A lot or building must be occupied with only the principal uses permitted on the permitted use table above. Accessory uses and structures customarily incidental to a permitted principal or special use shall be permitted.
2. If residential uses are constructed as permitted herein, a minimum of 10% of the total residential units constructed must be designated as affordable dwelling units for a minimum period of 30 years from certificate of occupancy.
3. All affordable dwelling units within residential uses designated as “for sale” shall be subject to the requirements of Section 3.10 of the Land Use Management Ordinance (LUMO).
4. All affordable dwelling units within residential uses designated as “rental units” shall be subject to the requirements contained herein:
 - a. Rental units designated as affordable dwelling units shall be units provided for households earning 65% or less of the area median income (AMI) based on household size for the Durham-Chapel Hill Metropolitan Statistical Area, as determined by the United States Department of Housing and Urban Development (HUD).
 - b. If the required number of affordable dwelling units includes a fraction, the applicant shall have the option to (i) round up and provide one additional affordable dwelling unit, or (ii) the fraction of a unit shall be provided in the form of a payment in lieu of providing the affordable dwelling unit as further outlined in Section 3.10.3 of the LUMO.
 - c. Each rental unit designated as an affordable dwelling unit shall be similar to the market rate unit size mix.
 - d. The owner may seek alternatives to on-site development of affordable dwelling units as outlined in Section 3.10.3 of the LUMO.
 - e. An affordable housing plan shall be required as outlined in Section 3.10.4 of the LUMO.
 - f. An affordable housing performance agreement shall be required as outlined in Section 3.10.5 of the LUMO.
 - g. The owner shall be entitled to all development cost offsets allowed under Section 3.10.6 of the LUMO.
 - h. Rental units designated as affordable dwelling units shall be subject to all requirements under Section 3.10.7 of the LUMO.
5. Food trucks shall be a permitted use by right, however, food trucks will need to comply with all vendor requirements under Sec. 6.20 of the LUMO.
6. The Business - Convenience use as permitted herein which includes the use for eating and drinking establishments shall include brewery, cidery, seltzery, distillery, coffee roasting & winery uses, so long as any such use includes a retail component open to the public. No production-only facilities shall be permitted.
7. Drive-thru facilities, including drive-up automatic teller machines and drive-in windows shall be permitted, subject to the drive-thru standards contained herein.
8. Co-working space shall be permitted under Business, Office-Type.
9. Short-Term Rentals, as defined herein, shall be specifically prohibited.
10. The property contains an existing Service Station / Convenience Store use located on Pod D. No additional independent Service Station / Convenience Store uses shall be permitted by right without first obtaining a Special Use Permit.

IV. DESIGN CRITERIA

To help facilitate a pedestrian friendly and walkable environment, street typology requirements shall be used to apply certain development standards along thoroughfares. Street typology requirements shall be used to dictate a building's proximity to the road through the use of a Build-to Zone ("BTZ"), where new buildings must be placed in order to qualify as a building frontage. New buildings shall also be required to have a minimum percentage of building facade along the linear street frontage. Given the environmental constraints along Estes Drive, there shall be no street typology requirements for buildings in Pod D that are fronting Estes Drive, however, streetscape standards shall apply as detailed herein. Note that these Design Standard street typology requirements shall take the place of perimeter buffer requirements that are outlined in LUMO Section 5.6.



STREET TYPOLOGIES PLAN

Note: This graphic is diagrammatic and shows primary vehicular routes only. The final location of vehicular connections will depend upon final building layout.

LEGEND:

█	WILLOW DRIVE
█	FORDHAM BOULEVARD
█	ESTES DRIVE
█	INTERNAL MAIN STREET DRIVEWAY
█	INTERNAL TYPICAL DRIVEWAY
█	SERVICE LANE
█	EXISTING INTERNAL CIRCULATION

1. WILLOW DRIVE

BUILDING LOCATIONS

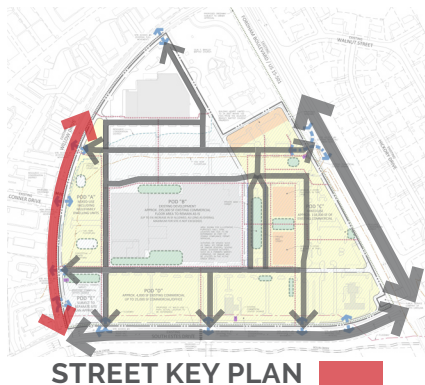
- Front Setback/BTZ (min/max) 0-20'
- Building Facade in BTZ (min) 50%

Given that development will occur in phases, the required building facade in the BTZ shall be calculated based on the new building area within the BTZ against the width of land area being disturbed for that specific building or phase of development, as measured within 20' of the street frontage.

PARKING

No new parking shall be permitted between new buildings and the Willow Drive frontage.

Note: Willow Drive streetscape dimensions are designed to facilitate the preservation of existing mature trees. As the existing mature trees on Willow Drive fail and need to be replaced, new standards for streetscape requirements may be considered.



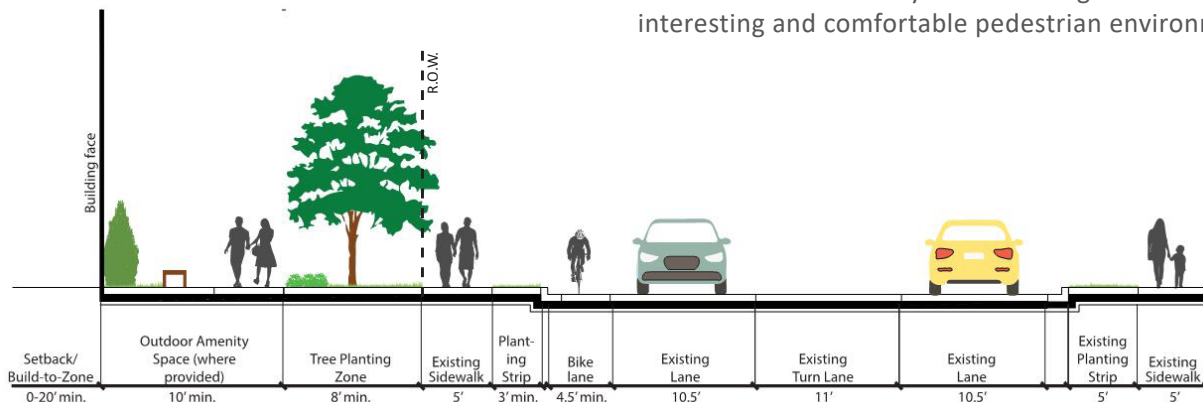
STREET KEY PLAN

STREETSCAPE

- Sidewalk (min): 5' (existing to remain)
 - Where feasible, alternate pedestrian pathways will be provided to enhance connectivity and encourage an inviting, interesting and comfortable pedestrian environment.
- Planting strip (min): 3' (existing to remain)
- Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible using tree protection methods outlined in the LUMO.
 - Hardscape is allowed within the planting zone or canopy of existing trees so long as prudent measures are taken to preserve the trees.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.
- Bike Lane (min): 4.5'
- Outdoor Amenity Space (min): 10' (where provided)
 - Refer to requirements in these Design Standards.



Conceptual example of alternate pedestrian pathways to enhance connectivity and encourage an inviting, interesting and comfortable pedestrian environment.



SOUTHBOUND WILLOW DR. SECTION

Note: Other side of Willow Dr. that is not adjacent to our property is provided for reference only.

2. FORDHAM BOULEVARD

BUILDING LOCATIONS

- Front Setback/BTZ: 0 (min)
 - 77' (max with parking)
 - 0-20' (max without parking)
- Building Facade in BTZ (min): 40%

Given that development will occur in phases, the required building facade in BTZ shall be calculated based on the new building area within the BTZ against the width of land area being disturbed for that specific building or phase of development, as measured within 20' of the street frontage.

PEDESTRIAN ZONE

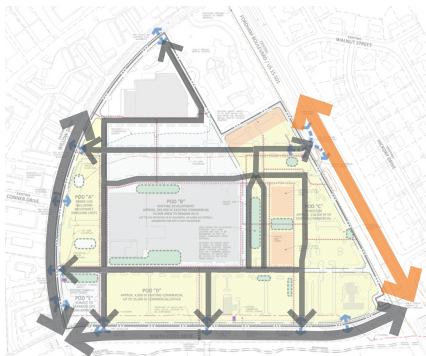
- Sidewalk (min): 8'
 - (See Street Frontages note 5)

PARKING

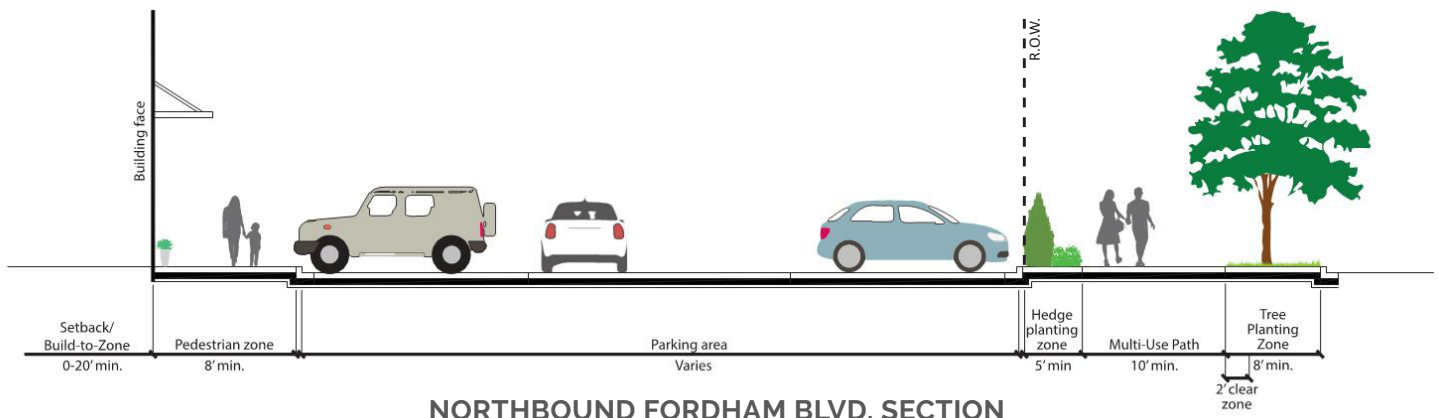
- Parking width (max, if provided): 62'

STREETSCAPE

- Hedge Planting Zone (min): 5'
 - Only required when there is parking that is to be screened.
 - Refer to landscape buffer and screening requirements outlined in the LUMO for plant material (only).
- Sidewalk (min): N/A*
 - *Multi-use path proposed as alternative to sidewalk.
 - (See Street Frontages note 5)
- Multi-use Path: 10' (with 2' clear zone)
- Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible using tree protection methods outlined in the LUMO.
 - Hardscape is allowed within the planting zone or canopy of existing trees so long as prudent measures are taken to preserve the trees.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.



STREET KEY PLAN



3. ESTES DRIVE

BUILDING LOCATIONS

- Front Setback: 22'
- BTZ (min/max): N/A
- Building Facade in BTZ (min): N/A

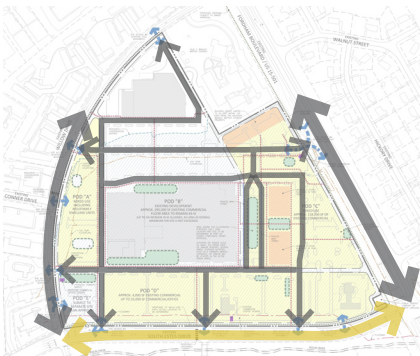
Given that the Estes Drive frontage is within the FEMA floodway, there shall be no required BTZ since buildings cannot be placed within the floodway.

PARKING

- Parking Width (min): Varies
- Hedge Planting Zone (min): 5'
 - Only required when there is parking that is to be screened.
 - Refer to landscape buffer and screening requirements outlined in the LUMO for plant material.
 - Drainage inlets, river rock and similar shall be permitted within the hedge planting zone as needed for stormwater measures, only.

STREETSCAPE

- Sidewalk (min): N/A*
 - *Multi-use path proposed as alternative to sidewalk.
- Multi-use Path: 10' (with 2' clear zone)
- Tree Planting Zone (min): 8'
 - Preserve existing mature trees where possible using tree protection methods outlined in the LUMO.
 - Hardscape is allowed within the tree planting zone or canopy of existing trees so long as prudent measures are taken to preserve the trees.
- Tree Spacing (on center, avg): 40'
 - Space proposed trees around and between existing mature trees as needed.



STREET KEY PLAN



WESTBOUND ESTES DR. SECTION

4. INTERNAL MAIN STREET DRIVEWAY

DESIGN INTENT & FLEXIBILITY

The internal main street driveway section shown below is mainly shown for design intent only.

BUILDING LOCATIONS

- Front BTZ (min/max): 0-20'
- Building Facade in BTZ (min): 50%

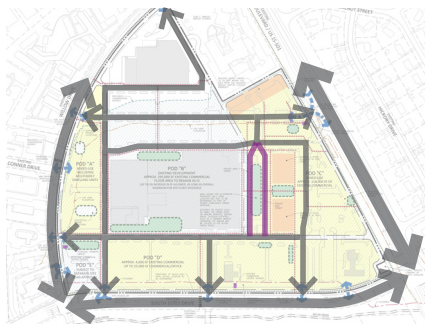
Given that development will occur in phases, the required building facade in BTZ shall be calculated based on the new building area within the BTZ against the width of land area being disturbed for that specific building or phase of development, as measured within 20' of the street frontage.

PARKING

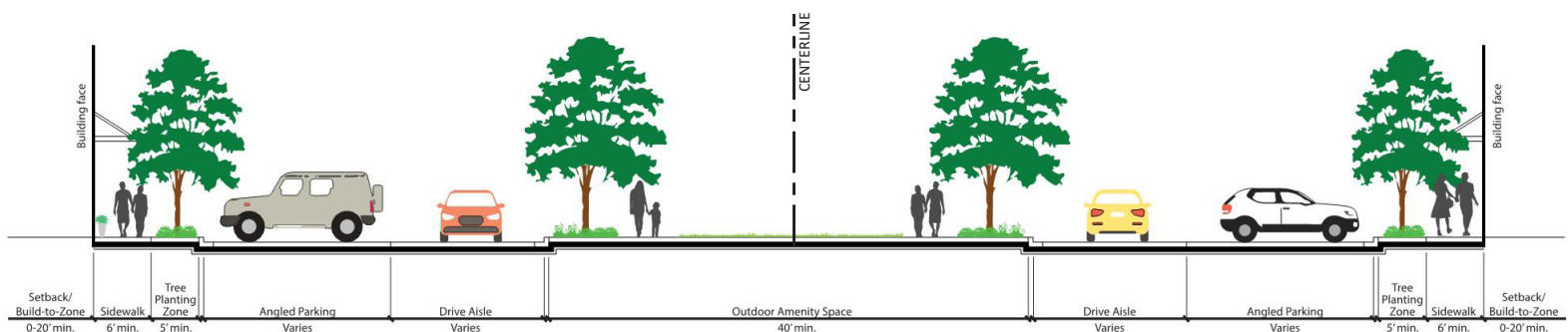
- Angled or parallel parking
- Ensure pedestrians have convenient access from the parking area directly to the sidewalk and building facades.

STREETSCAPE

- Sidewalk (min): 6'
- Tree Planting Zone (min): 5'
 - Street furnishings such as but not limited to light poles, bollards, tree grates, benches, planters and hardscape areas/ extended sidewalk are allowed in the Tree Planting Zone so long as measures are taken to provide adequate area for trees to grow.
- Tree Spacing (on center, avg.): 40'
 - A tree shall be provided every 40' on center average, measured linearly along the perimeter of the internal main street driveway. These trees may be placed within the Outdoor Amenity Space, Tree Planting Zone or in the parking islands; however, only the trees located on each side of the centerline may count towards that half of the streetscape's tree requirement.
 - Tree planting requirements of the parking landscape standards in section 5.9.6 of the LUMO shall apply.
- Outdoor Amenity Space (min): 40'
 - Refer to requirements in these Design Standards to follow.
 - Retail kiosks and other structures may be placed in the Outdoor Amenity Space. Retail kiosks shall include buildings of less than 2,500 SF of enclosed heated GLA. Enclosed heated GLA of kiosks shall not count towards required Outdoor Amenity Area(s).

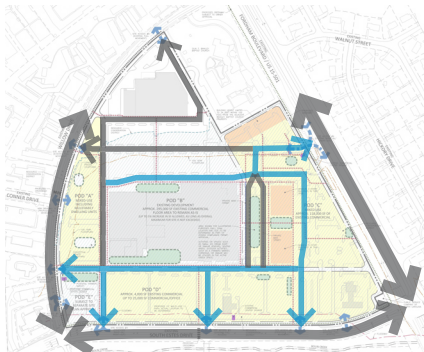


STREET KEY PLAN



EXAMPLE INTERNAL MAIN STREET DRIVEWAY SECTION

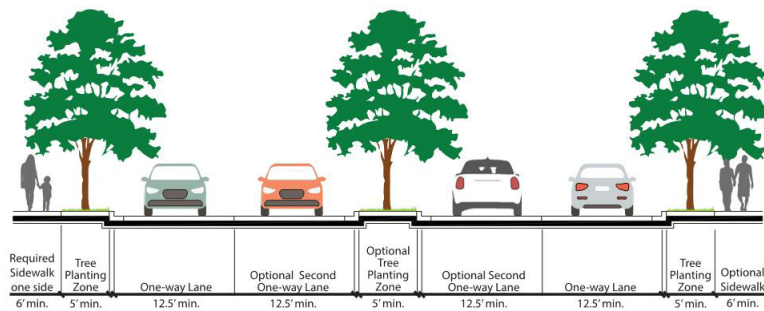
5. INTERNAL TYPICAL DRIVEWAY



STREET KEY PLAN

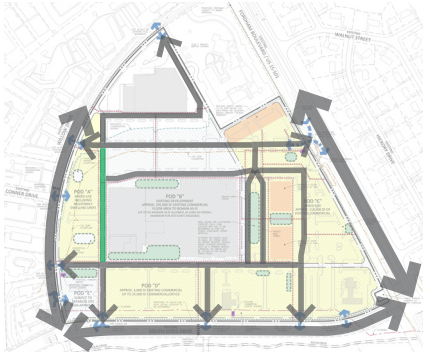
Throughout the site there are numerous existing internal driveways that may remain as-is or be modified over time. Given the various configurations existing today, internal drives shall not be subject to any specific cross-section. However, each drive being improved will be required to provide a sidewalk connection on minimum one side, with connections to adjacent pedestrian infrastructure where feasible. Where possible, trees will be provided at 40' on-center, average, and may include existing trees to remain.

For any proposed buildings with a primary entrance facing an internal driveway, a minimum 6' sidewalk, 5' Tree Planting Zone and trees at 40' on-center average shall be required between the building and the internal typical driveway along the full width of the building that is within 20' of an internal driveway back of curb. Existing mature trees can be retained along the internal typical driveway as long as the overall intent is met. Pedestrian accommodation and enhancement will be provided along all the internal typical driveways according to the specific cross sectional properties of each street segment as future development occurs.



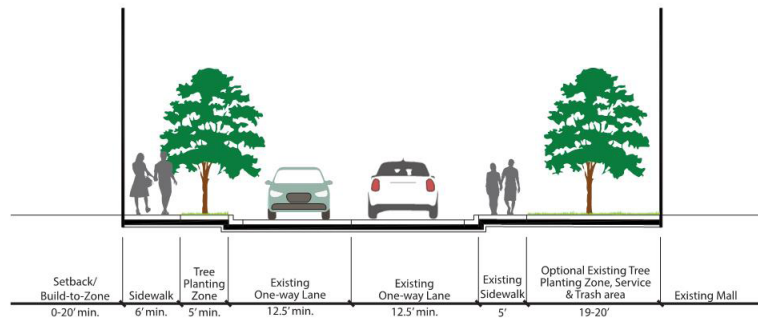
EXAMPLE INTERNAL TYPICAL DRIVEWAY SECTION

6. SERVICE LANE



STREET KEY PLAN

The service lane is located near the rear of the building and not visible from the public right-of-ways as much as possible given existing parameters of the site. The service lane is oriented to easily access necessary service areas around Pod A and Pod B. The service lane does not have any primary building frontage, but shall still provide pedestrian connectivity with a minimum 6' sidewalk on at least one side of the road. Although this type of street is mainly service-related, where appropriate, design elements may be incorporated to help activate the corridor and to provide an inviting, interesting and comfortable pedestrian environment.



EXAMPLE SERVICE LANE SECTION

NOTES

1. Setbacks/BTZ shall be measured from the edge of the right-of-way line within a dedicated right-of-way, or from the back of sidewalk within a public access easement. No interior, side or rear yard setbacks shall be required; however, if building is not placed on the side or rear property line and a setback is provided, it must be a minimum of 5 feet from the property line.
2. The required percentage of building facade in the BTZ is measured by the width of the building along the street frontage divided by the buildable width of the lot. Since the project may occur in phases, the buildable width of the lot shall only include the portion of the lot included and being disturbed in any such phase of construction. If phasing creates a non-conforming frontage request, a Design Alternative may be considered.
3. Outdoor seating and dining areas shall qualify as building facade for the purposes of calculating the BTZ percentage so long as the outdoor seating and dining area is not more than 2/3 the width of the building and is separated from the sidewalk by a wall or fence no higher than 4' above the sidewalk. Each outdoor seating and dining area may only include up to 20' of qualifying width with no more than two outdoor seating and dining areas included in the BTZ calculation for any one building.
4. Drive-thru windows or similar structures related to drive-thru facilities, whether attached or detached to the use it serves shall not be counted towards the BTZ percentage requirement, nor shall walk-up automatic teller machines.
5. For buildings fronting Fordham Blvd. with no parking between the building and R.O.W., one min. 6' sidewalk connection from the multi-use path to the building's primary entrance shall be required in lieu of the pedestrian zone requirements. For buildings set back from the Fordham Blvd. R.O.W. more than 10', or where parking is located between the building and the R.O.W, a minimum 8' sidewalk shall be required directly adjacent to the building's primary entrance(s).
6. While only one row of parking (62' max) shall be permitted between any new buildings constructed and the Fordham Blvd. right-of-way, so long as the minimum BTZ percentage requirement has been met, additional rows of parking shall be allowed beside those buildings constructed or in the areas between where buildings are constructed along Fordham Blvd.
7. Where a new building is being constructed on a portion of the property that is outside of the BTZ, such construction shall be permitted when the BTZ percentage required has been met for the frontage directly adjacent to where the non-conforming building is being proposed. Note that if a building fronts the Main Street it is exempt from other frontage/BTZ requirements.
8. Building additions to any existing structures shall be permitted and shall not be subject to BTZ requirements unless the building addition proposed is within the BTZ of any street frontage. Note that this statement also applies to a structure that exists in the future and is then expanded.
9. Note that streetscape improvements are only required along areas that are being redeveloped.
10. All landscaping for streets shall meet requirements set forth in Section 5.9.6 of the Town of Chapel Hill LUMO and Design Manual, with the exception of any modifications, variances or design alternatives provided for in these Design Standards or that are approved with the Special Use Permit. For any NCDOT roads, streetscape & landscaping requirements are subject to NCDOT approval and may be modified as needed with Staff input.
11. Given that screening and other applicable LUMO requirements are met, structured parking may be located in the BTZs.
12. All on-site signage visible from a street frontage shall meet LUMO signage regulation unless otherwise specified within the SUP.

(Continued on following page)

13. Tree species shall be selected to provide proper growth habits given the built environment anticipated with preference given to canopy trees. Near existing trees, in narrow areas adjacent to buildings, and along retail storefronts, understory and ornamental trees may be used as appropriate.
14. Adequate measures shall be taken to provide, on average, 500-square feet of soil volume per new canopy tree where feasible.
15. Streetscape elements may vary from dimensions shown so long as they meet the design intent and goals outlined in this document. Such deviations shall be considered a minor modification.
16. New buildings which have a primary frontage on the Internal Main Street shall not locate loading docks or vehicular service access points along the Main Street Frontage.

MASS

Building mass variation is the way the form and shape of a building change to establish a sense of human scale. This may be achieved by changing the heights of different parts of a building and by creating offsets in wall planes to express individual building modules. All new buildings in University Place shall be subject to the building height requirements listed below. In addition, all buildings 4 stories in height or greater shall meet either the building step back or module offset criteria listed below.

1. BUILDING HEIGHT

Building height shall be measured from the average grade at the foundation to the highest portion of the structure. Building height requirements are broken down into primary and secondary. Primary height shall be the maximum height allowed for any structure located at the minimum setback required for such structure, while secondary height shall be the absolute maximum height allowed for any structure. If there is no minimum setback, then primary height applies to the facade closest to the right-of-way line (see notes). The maximum heights for University Place shall be as follows:

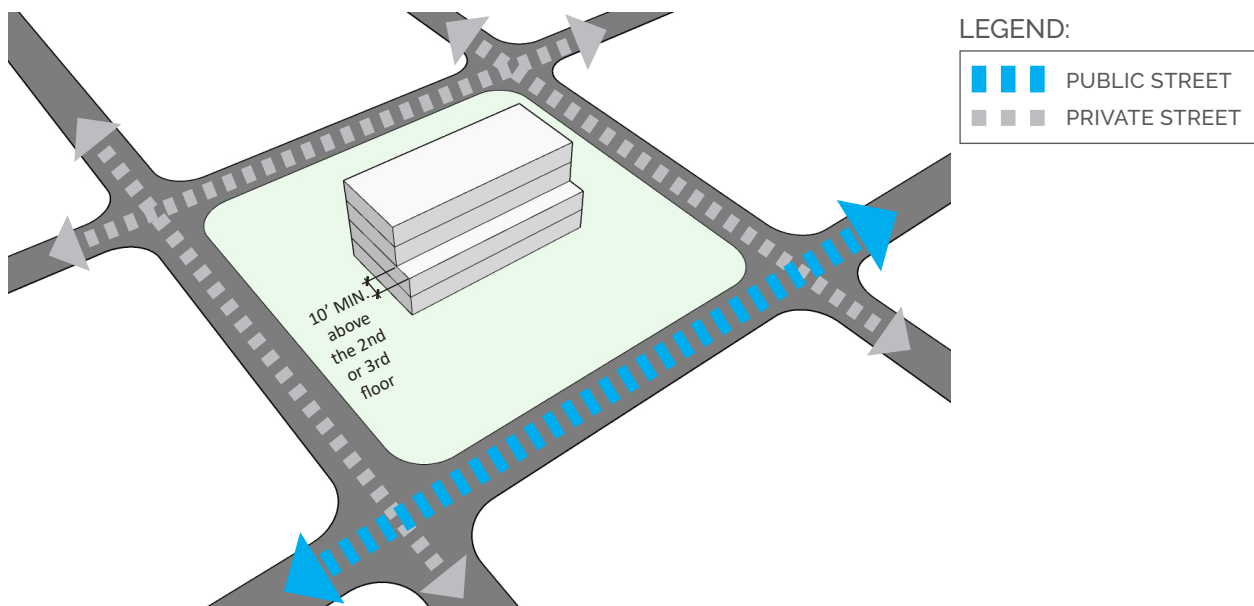
	Primary Height (max.)	Secondary Height (max.)
Pod A	75' (5-story)	90' (7-story)
Pod B	N/A - No Change	N/A - No Change
Pod C**	75' (5-story)	90' (7-story)
Pod D	34' (3-story)	45' (3-story)
Pod E	N/A - No Change	N/A - No Change

** For any portion of a structure located within 100' of the Binkley Baptist Church property line (PIN# 9799234058), the height shall be limited to 34'.

Ground Floor Height: In addition, ground floor height for residential and hotel uses shall be a minimum of 9' from floor to ceiling and a minimum of 12' from floor to ceiling for commercial and office uses with upper story height being a minimum of 9' from floor to ceiling for all permitted uses.

2. BUILDING STEP BACK

The building step back shall be measured as the horizontal change in the building wall plane, perpendicular to the primary frontage or property line. A 10' foot building step back above the second or third floor is required for buildings 4 stories or greater, unless module offset is provided. The building step back requirements are also displayed in the diagram below:



BUILDING STEP BACK DIAGRAM

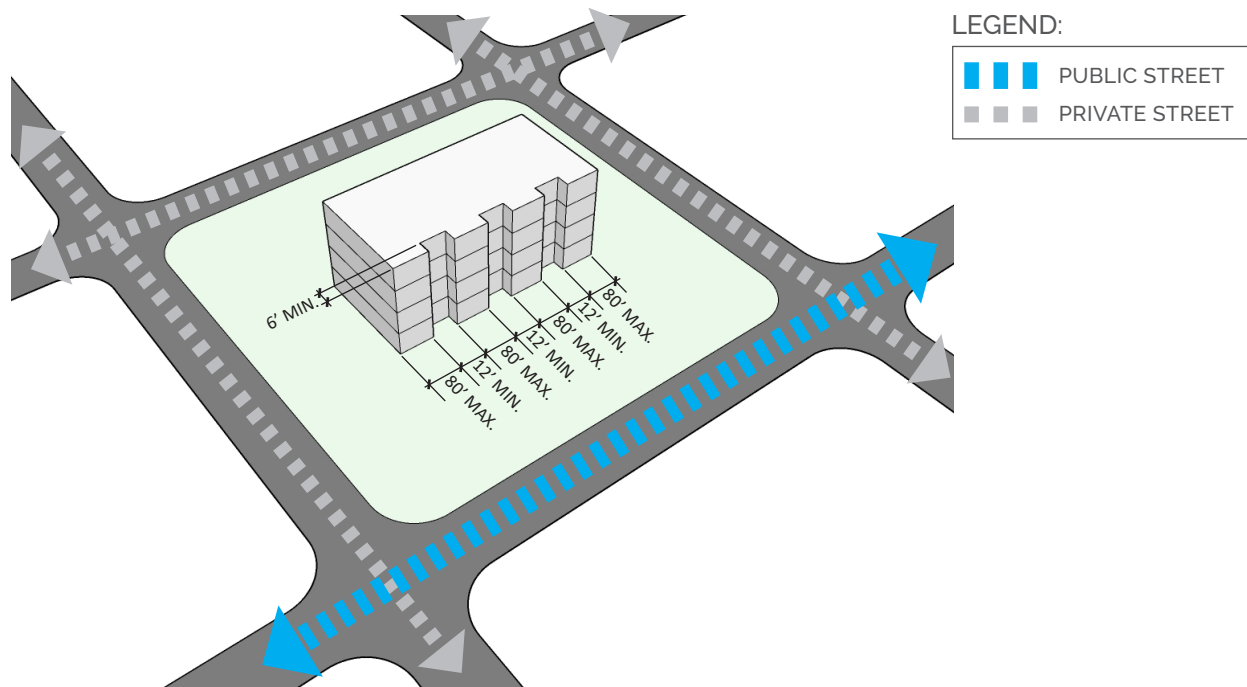
MASS (CONTINUED...)

3. MODULE OFFSET

Building modules refer to a set of subordinate volumes that compose the total mass of a building. Modularity also can be expressed by the following, but not limited to, changes in wall planes, building materials and architectural details. The module offset requirement shall be measured as the horizontal change of a portion of the building wall plane at ground level, perpendicular to the primary frontage. The module width shall be measured for the portion of the wall plane closer to the sidewalk, while the width of offset shall be measured for the portion further from the sidewalk. The dimensions of modules and offsets should maintain a sense of proportion to the building as a whole. Module offset is required for buildings 4 stories or greater, unless a building step back is provided. Requirements for module offset in University Place shall be as follows:

Average Module Width (max)	80'
Depth of Offset (min)	6'
Width of Offset (min)	12'

The module offset requirements are also displayed in the diagram below:



MODULE OFFSET DIAGRAM

FORM

The Building Mass & Form design criteria will encourage human interaction and activity at the street level with clear connections to building entries and edges. In conjunction, the Street Typologies design criteria will outline an appropriate pedestrian framework to create safer pathways to different buildings. Primary entrances should be distinctive and identifiable to pedestrians with clear lines of sight. There should be an emphasis on street level transparency to facilitate interaction between people in the interior of a building and people near the exterior of a building. The primary elevation of a building is considered as that side which fronts the main street serving the building. It will usually contain the main architectural features such as large windows, one or more primary entries, lighting, signage and canopies or awnings. To facilitate these goals mentioned above, the following shall be required on the primary elevations of buildings:

Transparency - Ground Floor (min)	20% (Residential/Hotel Uses)
	50% (Commercial/Office Uses)
Transparency - Upper Floors (min)	20% (All Uses)
Blank Wall Area (max)	2,500 SF
Primary Entrance Facing Public Realm	Required
Building Width (maximum)	330'-0"



NOTES

1. The requirements of this section shall not apply to preserved buildings unless substantial improvements are proposed.
2. The primary height limitation is the maximum height allowed for any structure located at the minimum setback required for the structure. The secondary height is the absolute maximum height allowed for any structure and increases at a rate of one foot in height for every one foot of distance interior to the lot, measured away from the setback of any public street frontage. The secondary height allowed as measured in number of stories shall increase at a rate of 1 additional story for every 10' feet of distance interior to the lot, measured away from the setback of any public street frontage.
3. Transparency is the minimum percentage of windows and doors that must cover a ground or upper story facade. Transparency is required for any building facade facing a primary frontage. The transparency requirement on ground story facades is measured between 2 and 9 feet above the adjacent sidewalk. Along Willow Drive, When grading results in the ground story shifting up or down a floor, the transparency requirement shall be limited to areas where the finished floor is vertically within 2 feet of the adjacent sidewalk/grade. The transparency requirement on upper story facades is measured from the top of the finished floor to the top of the finished floor above. When there is no floor above, upper story transparency is measured from the top of the finished floor to the top of the wall plate. Glass is considered transparent where it has a transparency higher than 80% and external reflectance of less than 15%. Windows must be clear, unpainted, or made of similarly-treated glass; spandrel glass or back-painted glass does not comply with this provision. For ground story commercial uses, a minimum of 60% of all windows must allow views into the ground story for a depth of at least 6 feet.
4. Blank wall area means a portion of the exterior facade of the building that does not include: windows or doors; columns, pilasters or other articulation greater than 12 inches in depth, art or murals, or a substantial material change (paint color is not considered a substantial change). Blank wall area applies in both a vertical and horizontal direction, and is measured by overall square footage. Blank wall area applies only to ground and upper story street-facing facades.
5. An entrance providing both ingress and egress, operable to residents at all times or to customers during normal business hours, is required along each side of the building facing a public streetscape or any other non-vehicular, publicly accessible area to meet the public realm primary entrance requirements. Additional entrances are permitted. An angled (clipped corner) entrance may be provided at any corner of a building along the street to meet the street entrance requirements. A primary entrance may be oriented perpendicular to the adjacent public realm where the entrance is clearly defined by a building element.
6. Overhead walkways between buildings (such as skywalks) are permitted and shall not be calculated as a part of Building Width.
7. Build-to-Zone requirements shall not be applied to portions of the site which are not favorable to development, such as ecological sensitive areas or portions of the site, which due to existing geometries, do not allow for efficient development (such as triangular corners).
8. Average grade of a building is determined by calculating the average of the highest and lowest elevation along natural or improved grade (whichever is more restrictive) along the front of the building parallel to the primary street setback line.

(Continued on following page)

NOTES (CONTINUED...)

9. Any height encroachment not specifically listed is expressly prohibited except where the Town Manager determines that the encroachment is similar to a permitted encroachment listed below.
 - a. The maximum height limits do not apply to spires, belfries, cupolas, domes not intended for human occupancy; monuments, water tanks/towers or other similar structures which, by design or function, must exceed the established height limits.
 - b. The following accessory structures may exceed the established height limit of the district provided they do not exceed the maximum height by more than fifteen (15) percent of the maximum height limitation that defines the portion of the building envelope penetrated by such structures:
 - i. Chimney, flue or vent stack, spire, smokestack, water tank, windmill
 - ii. Rooftop deck, patio, shade structure, skylight
 - iii. Monument, steeple, flagpole
 - iv. Accessory radio or television antenna, relay tower
 - v. Transmission pole, tower or cable
 - vi. Garden, landscaping
 - vii. Cupola, clock tower or decorative tower not exceeding twenty (20) percent of the principal building footprint
 - viii. Parapet wall
 - ix. Solar panel, wind turbine, rainwater collection system
 - c. The following accessory structures may exceed the established height limits provided they do not exceed the maximum building height by more than ten (10) feet, do not occupy more than twenty-five (25) percent of the roof area, and are set back at least ten (10) feet from the edge of the roof: Elevator or stairway access to roof, Greenhouse and Mechanical equipment.
 - d. An accessory structure located on the roof must not be used for any purpose other than a use incidental to the principal use of the building.
10. Building width maximums apply to Pod C + D, only.
11. Building Form Design Criteria will apply to a building's designated Secondary Frontage, where applicable. Building Mass Design Criteria (except Building Height) do not apply to a building's designated Secondary Frontage.

Standalone Parking structures shall have sides designated as primary and secondary frontages, consistent with the definitions that apply to buildings. Parking structures shall not be subject to requirements for Step Back, Module Offset, or Transparency. The primary and secondary frontages shall be subject to the following Mass and Form standards:

Height (max.)	Equal to maximum building height as established per Pod
Setback (min.)*	0' where an active ground floor use is provided 20' where no active ground floor use is provided
Vehicular Access Points	Not allowed along primary frontage
Blank Wall Area (max.)	2,500 SF
Structure Width (max.)	330'-0"

*Setbacks are applicable to any Street Typology except Service Lane.

NOTES

1. Exposed Parking Structures: The architectural treatment and materials of parking garages shall be compatible with surrounding buildings. Exterior surfaces of parking structures should clad with other materials, painted or have an applied finish. When combined with other architectural treatments (e.g. architectural screens, green walls, etc.) the visibility of exposed concrete should be minimized.
2. To the extent feasible, consider locating garage ramps (sloping floors) off street facing facades to standalone parking structures should be designed to allow a horizontal floor expression on the street facade and ensure screening of vehicle lighting.
3. Where feasible parking structures should provide elements which enhance the pedestrian experience along the ground-floor frontage of these structures including the incorporation of retail or active uses that engage with the street. If retail or other active uses are not feasible, then the ground floor should be designed at a pedestrian scale with the inclusion of green walls, public art or other design features to animate the façade as well as relate to nearby open space.
4. Driveway ingress and egress to standalone parking structures is not permitted on the Primary Frontage. Driveway entrances should not connect to the Main Street Driveway.
5. Architectural precast concrete may be used on the standalone parking garage only with a texture, color, and finish that is compatible with the character of adjacent buildings.
6. Poured in place concrete, if utilized, shall be limited in use as a material for the primary structure of the building, and fully screened with materials compatible to the character of adjacent buildings so that the poured in place concrete is clearly a background material, and does not appear as a primary visual component of the building elevations.
7. Should a green wall be employed on the standalone parking structure, maintenance, including watering, must be guaranteed for the plants to thrive over time and fully cover the background concrete. Providing a temporary screening system to cover the structure while the plants grow is strongly encouraged.
8. Where a parking structure is proposed adjacent to a primary frontage, Transparency shall not be required so long as the structure meets the blank wall area requirements.
9. Any structured parking visible to the public shall be required to include architectural screening compatible with the principal building it serves. All parking structure screening must be reviewed and approved by the Planning Staff. When feasible, consideration shall be given to providing commercial, office or other active uses on the ground floor of parking structures.

Parking is an essential element to a successful mixed use project, but access and sight-lines must not interfere with the pedestrian experience or safety. Given that the project has existing tenants with varying degrees of protection over existing parking areas, a portion of the existing surface lots will remain as-is in the near-term, but will offer more opportunities for redevelopment over time. In addition to the parking regulations outlined in the Street Typologies requirements herein, the minimum and maximum number of required parking spaces for vehicles and bicycles for new buildings shall be as follows:

PARKING REQUIREMENTS

	VEHICLE		BICYCLE	
	MIN.	MAX.	MIN.	SHORT/ LONG TERM
RESIDENTIAL USES				
Multifamily Dwelling Units, as follows:				
Studio / 1 Bedroom	1.0 per Unit	1.25 per Unit	1 per 4 units	20% / 80%
2 Bedroom	1.25 per Unit*	1.75 per Unit	1 per 4 units	20% / 80%
3 Bedroom	1.75 per Unit	2.25 per Unit	1 per 4 units	20% / 80%
4+ Bedroom	2.0 per Unit	2.5 per Unit	1 per 4 units	20% / 80%
Independent Senior Living Facility	0.5 per Unit	1.0 per Unit*	1 per 2 units*	20% / 80%
COMMERCIAL USES				
All Commercial Uses, Except:	1 per 300 SF*	1 per 200 SF*	1 per 2,500 SF*	80% / 20%
Restaurant/Bar	1 per 150 SF*	1 per 75 SF*	1 per 2,500 SF*	80% / 20%
OFFICE USES				
All Office Uses	1 per 300 SF*	1 per 200 SF*	1 per 2,500 SF*	50% / 50%*
HOTEL USES				
Hotel or Motel	0.9 per Lodging Unit	1.25 per Lodging Unit	1 per 15 Lodging Units	20% / 80%

*Denotes a modification from LUMO Standards.

NOTES

1. Existing buildings may be renovated or otherwise modified without providing additional bicycle or vehicular parking, so long as there is no increase to the overall building's finished square footage. If an existing building's overall finished square footage is increased, additional parking is only required for the new square footage added. A change of use to any existing structure shall not require additional parking. Note that this statement also applies to a structure that exists in the future and is then expanded.
2. If parking, access or other internal driveways, etc. are modified to accommodate the reconfiguration of existing buildings and not new buildings, those modifications shall not be subject to any frontage, BTZ or other requirements unless modifications are to accommodate new buildings, in which case they shall be subject to all requirements for new buildings including frontage, BTZ, etc.
3. The minimum parking requirement (vehicle or bicycle) may be reduced by up to 30% for shared parking between a mix of uses. Any reduction to the minimum parking shall be reviewed and approved by the Town Manager.
4. Short-term bicycle parking minimums required may be reduced if existing short-term bicycle parking is located within 100' of the proposed building. The requirement shall be reduced on a one for one basis for each space located within 100' up to 25% of the total required short-term bicycle parking.
5. Any structured parking visible to the public shall be required to include architectural screening compatible with the principal building it serves. All parking structure screening must be reviewed and approved by the Planning Staff. When feasible, consideration shall be given to providing commercial, office or other active uses on the ground floor of parking structures.
6. In the event that on-street parking is constructed as part of the construction of new buildings at the property, each on-street parking space may be used to count towards the required minimum parking.
7. Bicycle parking shall be required in accordance with the table above. Short-term parking shall be publicly accessible and be located within 100' from a building entrance that the bicycle rack is intended to serve. Long-term parking is intended for employees and residents and must be covered, weather-resistant and within 100 feet from a building entrance that the bicycle rack is intended to serve. Long-term bike parking can also be provided interior to a building or parking structure, without the need for a distance requirement.
8. All landscaping for parking areas shall meet requirements set forth in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual, with the exception of any modifications, variances or alterations that are approved with the Special Use Permit.
9. Pods may be developed with parking to serve the uses within the pod in excess of the maximum given that the parking provided on the overall site does not exceed the maximum.
10. A minimum of 20% of the new parking spaces provided will be pre-wired for electric charging stations. In all pods, new parking lots with more than 50 spaces shall have electric vehicle charging stations provided on at least 3% of all new parking spaces. The new parking along the internal main street driveway shall be exempt from this requirement.

NOTES

Drive-thru facilities shall be permitted in each pod, but must adhere to the following guidelines.

1. Drive-thru windows, lanes, menu boards, trash receptacles, ordering box, and other objects associated with the drive-thru, may not be placed along the public street facing facade of the associated building. This shall exclude any entry or exit lanes from a drive-thru when necessary for adequate vehicular circulation so long as it does not conflict with primary pedestrian access points to and from the associated building.
2. New buildings with a drive-thru use must be designed in a manner such that one side of the building is pedestrian oriented. Pedestrian and vehicular circulation patterns should be kept separate, but where site constraints force pedestrian and vehicular circulation patterns to cross, special care must be given to prioritize pedestrian circulation over vehicular, including design considerations such as textured paving, ease of access to building entrance, and where applicable, open areas and outdoor dining. If an internal-facing pedestrian oriented side of a building can avoid having pedestrian and vehicular circulation patterns cross, then modifications to allow drive-thru windows, menu boards, etc. facing public streets at the perimeter may be considered.
3. Adequate stacking space must be made available on-site for any use having a drive-thru. No more than 2 drive-thru lanes are permitted for any single use with drive-thru, with the exception of a bank being allowed to have 3 drive-thru lanes maximum. Required stacking shall be as follows:
 - a. restaurant (including a coffee shop) with a drive-thru must provide a minimum of 5 spaces before the order board, with another 3 spaces provided between the order board and the transaction window
 - b. a bank with a drive-thru must provide a minimum of 3 spaces measured from the teller box
 - c. a pharmacy with a drive-thru must provide a minimum of 3 spaces measured from the order box
 - d. a dry cleaner with a drive-thru must provide a minimum of 3 spaces measured from the pick up door
 - e. stacking required for all other uses will be determined by the Town Manager. The number of required stacking spaces includes the space at the window or communication/ mechanical device (e.g., order board, pick up window). If a drive-thru has multiple order boxes, teller boxes or pick up windows, the number of required stacking spaces may be split between each order box, teller box or pick up window.
3. Vehicles may not encroach on or interfere with the public use of streets and sidewalks by vehicles, bicycles or pedestrians. Drive-thru lanes must be separated by striping or curbing from other parking areas. Individual lanes must be striped, marked or otherwise distinctly delineated.
4. Drive-thru windows and lanes must be screened from the public realm and any adjacent ground floor residential use for the entire length of the drive-thru lane, including but not limited to menu boards, stacking lanes, trash receptacles, ordering box, drive up windows, and other objects associated with the drive-thru must be screened. Screening must be a continuous compact evergreen hedge. At the time of installation, the screening must be at least 3 feet in height and reach a height of 4 feet within 3 years of planting. In lieu of the compact evergreen hedge, a screening wall with a minimum height of 4 feet may be installed. The wall must be compatible with the principal building in terms of texture, quality, material and color.
5. The above standards are not applicable to existing drive-thru facilities located at the property.

DESIGN CRITERIA

OUTDOOR AMENITY SPACE

Outdoor Amenity Space shall be provided and may include green space, courtyards, plazas, water features, amphitheaters, patios, rooftops, art, dog parks, playgrounds, pavilions, sport courts, community gardens and other public elements. It should be designed and furnished to encourage activity and create comfortable space for all to enjoy. All Outdoor Amenity Space must be open to public use, resident-only or access-controlled private areas do not count. The Outdoor Amenity Space shall include trees for shade along with other landscaped areas that coordinate with the overall site development. When possible, use these green spaces or plazas to link adjoining buildings and provide clear connections to pedestrian circulation routes. Conceptual examples of those spaces are as follows:



play in the grass.



specimen tree.



urban tree grove.

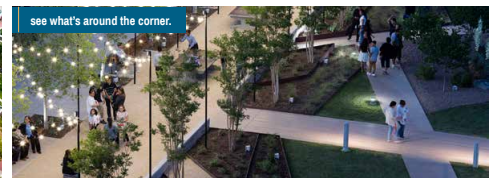
EXAMPLE 1



hang on the patio.



events + performances.



see what's around the corner.

EXAMPLE 2

NOTES

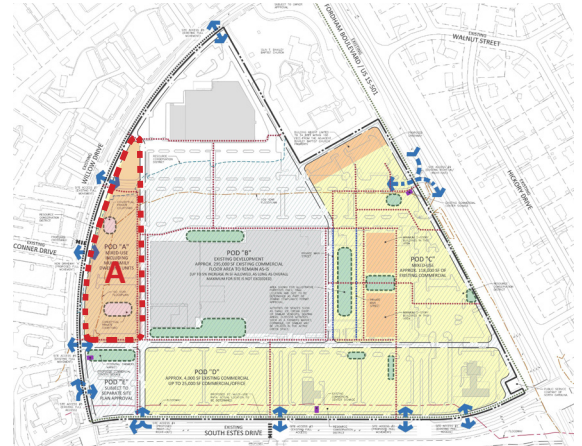
1. Outdoor Amenity Space shall be provided for the entirety of the property at a minimum ratio of 4.6% of gross land area and shall be distributed throughout the site. Each Pod shall be required to provide Outdoor Amenity Space at a minimum ratio of 3.6% of Pod area, so long as overall property minimum is met.
2. Outdoor Amenity Space shall be calculated as a function of gross land area of the development. Outdoor Amenity Space may be met in multiple open areas within the pod and must meet minimum dimensions described as follows:
 - a. Where not located adjacent to a building, or where located adjacent to a building that is three stories in height or less, an Outdoor Amenity Space must be at least 10 feet in width and length, with a minimum area of 100 square feet.
 - b. Where located adjacent to a building that is four stories in height or greater, the Outdoor Amenity Space shall have greater dimensions, such that the space is in proportion to the associated building, provides a comfortable scale for pedestrians, and invites public use and enjoyment. In no case shall the area of a single outdoor amenity space be required to exceed the minimum Outdoor Amenity Space ratio as specified for the property.
 - c. Where located in the build-to zone and used to create inviting space along a street facing facade, the width of the Outdoor Amenity Space measured perpendicular to the right-of-way may be less than the dimension prescribed above, subject to approval of a Design Alternative.
3. Outdoor Amenity Space may be counted to meet the build-to-zone percentage requirements; however, only half the width of the applicable Outdoor Amenity Space can be counted toward the required percentage.
4. Outdoor Amenity Space cannot be parked or driven upon, except for emergency access and permitted temporary events.
5. Note that parking lot landscaping shall not be included in the Outdoor Amenity Space calculations except where improved to the extent to constitute one of the examples identified in #7, below.
6. Outdoor Amenity Spaces may include but are not limited to facilities such as examples listed below:

a. green space	i. rooftops
b. plazas	j. public art
c. courtyards	k. dog parks
d. seating area plazas	l. playgrounds
e. water features	m. pavilions
f. amphitheaters	n. sport courts
g. patios	o. community gardens
h. parks	p. other outdoor public elements
8. All Outdoor Amenity Space shall either adjoin a street frontage or have a pedestrian connection to the street frontage. Outdoor Amenity Space shall meet ADA accessibility standards.
9. Where buildings are separated by a designated Outdoor Amenity Space, a minimum width of 30'-0" shall apply to the Outdoor Amenity Space.

V. DEVELOPMENT OPTIONS

POD A

Pod A converts an existing surface parking area into a more dense, multi-use part of the site. A new parking structure is proposed with a residential wrap screen. Given the significant grade change along Willow Drive and the interior street being a service lane for trash and deliveries, no maximum building width shall be required on Pod A; however, a public Outdoor Amenity Space will be provided along Willow Drive.



KEY MAP

DEVELOPMENT EXAMPLE

- Building A:
 - ±250 units Residential
 - ±8,000 SF Retail, including 5,000 SF of incubator retail
 - 2 private resident amenity courtyards
- Parking Structure: Parking spaces for covered retail parking + multifamily residents/guests
- Outdoor Amenity Space



CONCEPT MASSING*

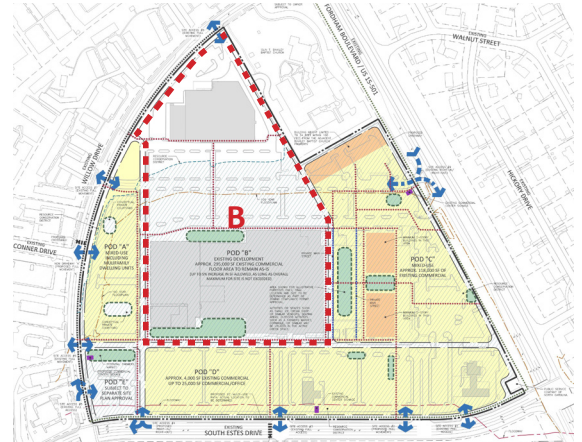
*This concept massing view is conceptual in nature and is provided for reference only. Building locations, access points, outdoor amenity space locations, and other design decisions shall be finalized during the Zoning Compliance Permit (ZCP) process.

DEVELOPMENT OPTIONS

PODS

POD B

There is $\pm 295,000$ SF of existing commercial to remain and be renovated, with conversion of interior mall space into exterior space. Enhanced Outdoor Amenity and green spaces are to be provided throughout Pod B along with parking improvements. Examples of conceptual facade and Outdoor Amenity Space improvements to occur in Pod B are shown in the storefront rendering views below.

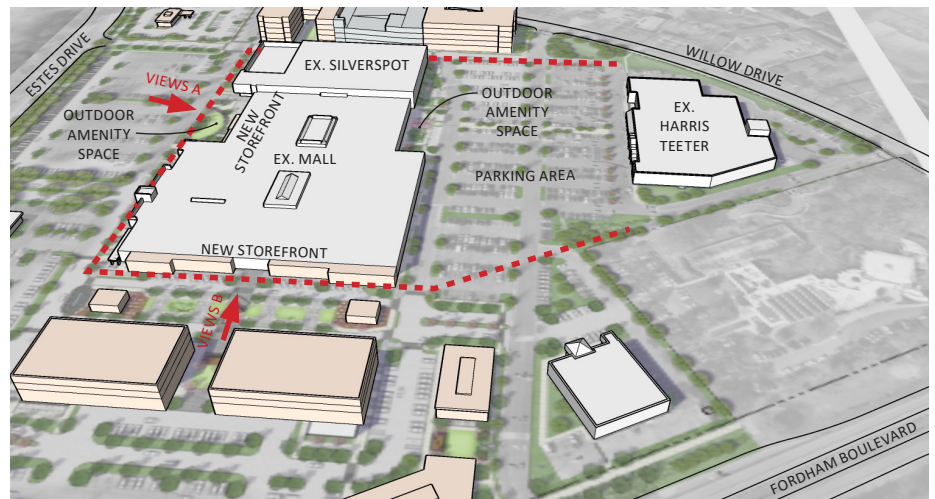


KEY MAP

DEVELOPMENT EXAMPLE

- $\pm 295,000$ SF Existing Commercial to Remain
- New Storefronts along portions of existing mall (see views below)
- Outdoor Amenity Space

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CONCEPT MASSING*



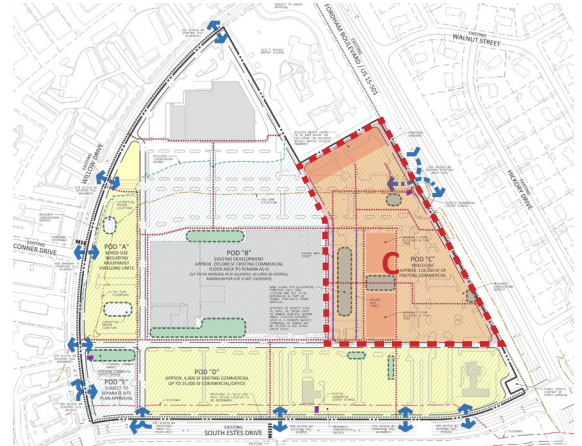
CONCEPT STOREFRONT VIEWS A



CONCEPT STOREFRONT VIEWS B

POD C

Pod C will be focused around a central park or green space that will be pedestrian focused and include programmed space for recurring events and everyday gatherings. The central park area will be flanked by urban 'main streets' with a consistent streetscape experience that is designed to promote safety and minimize pedestrian-vehicle conflicts. This area will act as the heart of the project, with pathways busy during the day and into the evening in this vibrant urban environment.



KEY MAP

DEVELOPMENT EXAMPLE 1

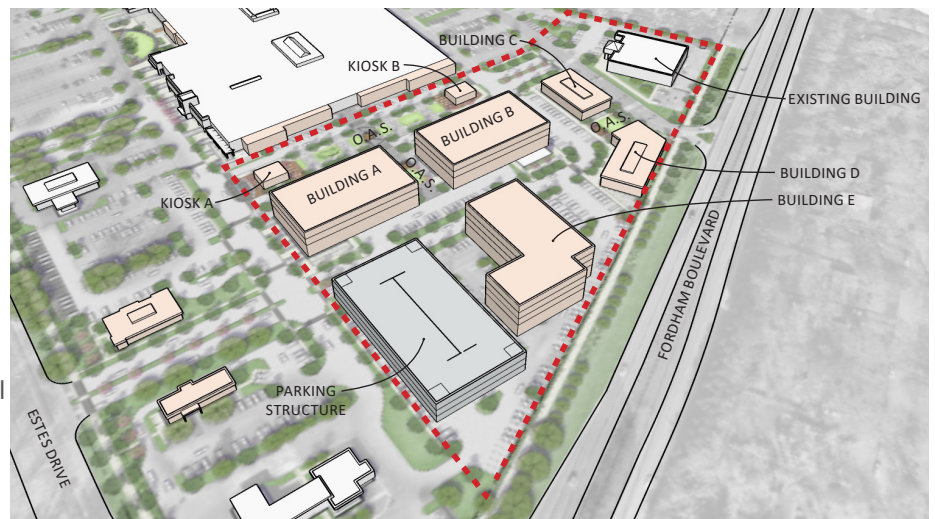
- Demolition of ±105,000 SF of Existing Mall
- Existing Building: ±11,200 SF Commercial
- Building A: ±13,500 SF Commercial and ±25,000 SF Office
- Building B: ±13,500 SF Commercial and ±25,000 SF Office
- Building C: ±6,000 SF Commercial
- Building D: ±8,800 SF Commercial
- Kiosk A: ±1,200 SF Commercial
- Kiosk B: ±1,200 SF Commercial
- Outdoor Amenity Space (O.A.S.)



CONCEPT MASSING*

DEVELOPMENT EXAMPLE 2

- Demolition of ±105,000 SF of Existing Mall
- Existing Building: ±11,200 SF Commercial
- Building A: ±13,500 SF Commercial and ±23 units Residential
- Building B: ±13,500 SF Commercial and ±23 units Residential
- Building C: ±6,000 SF Commercial
- Building D: ±8,800 SF Commercial
- Building E: ±125 room Hotel w/ associated retail
- Parking structure: ± 4,000 SF Retail
- Kiosk A: ±1,200 SF Commercial
- Kiosk B: ±1,200 SF Commercial
- Outdoor Amenity Space (O.A.S.)

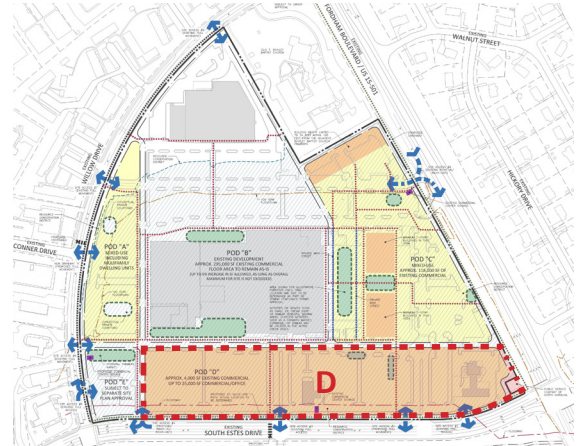


CONCEPT MASSING*

*These concept massing views are conceptual in nature and are provided for reference only. Building locations, access points, outdoor amenity space locations, and other design decisions shall be finalized during the Zoning Compliance Permit (ZCP) process.

POD D

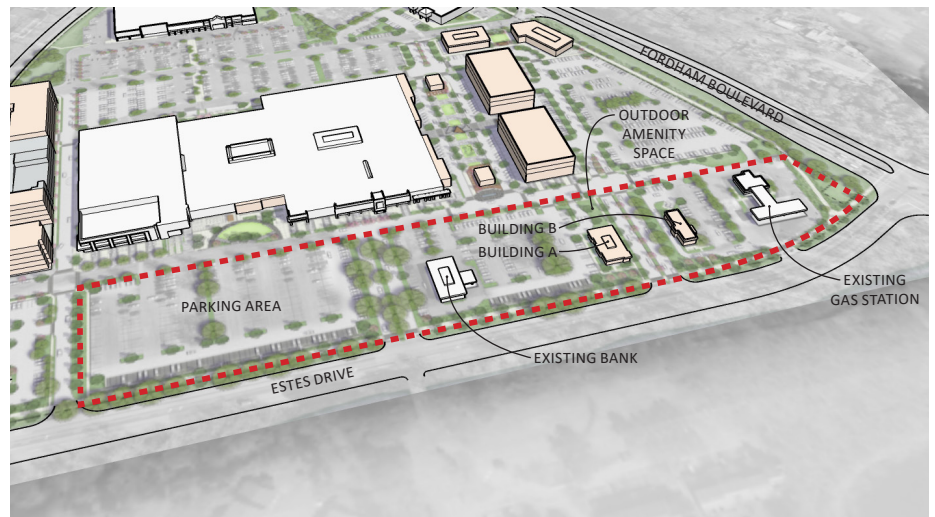
Pod D includes existing parking areas and buildings to remain, amongst 2 proposed commercial buildings lining a relocated driveway. Note that this pod faces limitations to major design changes because of sensitivity to being in a floodplain. The proposed development should assist with flooding concerns because it overall reduces the amount of impervious area on site.



KEY MAP

DEVELOPMENT EXAMPLE

- Existing Gas Station: ±2,000 SF
- Existing Bank: ±2,400 SF
- Building A: ±3,500 SF Bank
- Building B: ±2,800 SF Commercial
- Outdoor Amenity Space

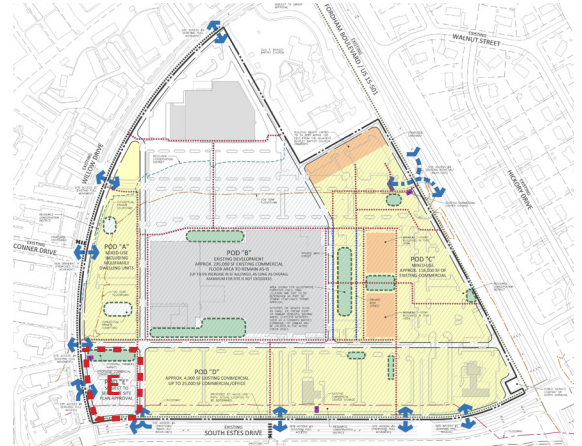


CONCEPT MASSING*

*This concept massing view is conceptual in nature and is provided for reference only. Building locations, access points, outdoor amenity space locations, and other design decisions shall be finalized during the Zoning Compliance Permit (ZCP) process.

POD E

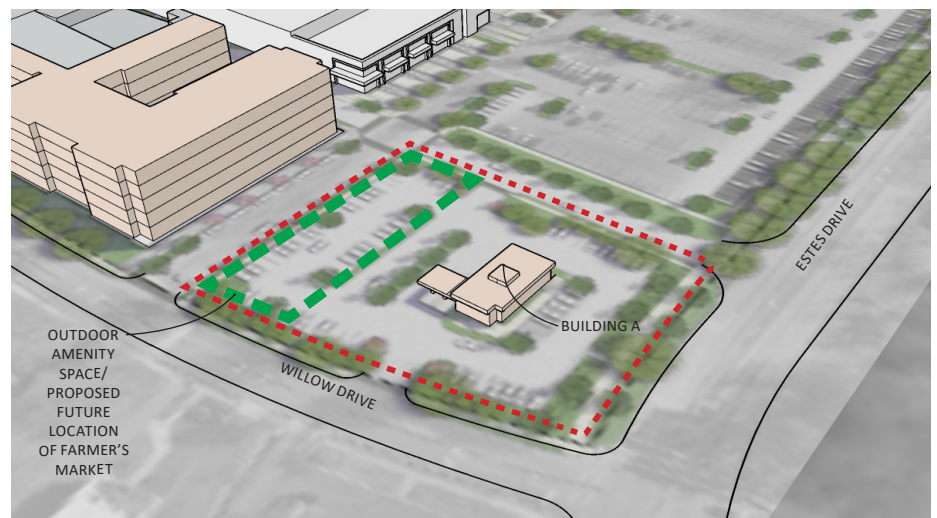
In Pod E, a Zoning Compliance Permit (ZCP) is under review with the bank use remaining and parking and driveway modifications proposed. Additional improvements to Pod E include the development of Outdoor Amenity Space with the relocation and improvement of the farmer's market.



KEY MAP

DEVELOPMENT EXAMPLE

- Building A: ±2,000 SF Bank
- Outdoor Amenity Space/ future farmer's market location



CONCEPT MASSING*

*This concept massing view is conceptual in nature and is provided for reference only. Building locations, access points, outdoor amenity space locations, and other design decisions shall be finalized during the Zoning Compliance Permit (ZCP) process.

VI. ADMINISTRATION

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS:

This list is provided for convenience only and is not meant to be exhaustive. Refer to the balance of the document for more information.

1. Amendment and Modification: Minor modifications to these standards may be approved by the Town Manager as long as such changes continue to be in substantial compliance with the approving action of the Town Council and all other applicable requirements and result in a configuration of buildings / development that is generally consistent with these Standards. The Town Manager shall not have the authority to approve changes that are in conflict with the approved SUP without prior Town Council approval.
2. Where the street typology standards listed herein conflict due to utility locations, fire access, required sight lines or other existing conditions, an alternative design shall be proposed, reviewed and approved by the Town Manger.
3. Willow Drive street frontage section modified from current streetscape requirements in the Town's mobility plan to preserve existing mature trees along Willow Drive frontage. A 4.5' foot bike lane and 5' foot sidewalk shall be provided along the Willow Drive frontage.
4. Estes Drive street frontage section modified from current streetscape requirements in the Town's mobility plan due to environmental constraints and to preserve existing mature trees along Willow Drive. A 10' foot multi-use path shall be provided with 2' foot clear zone along Estes Drive in lieu of a 5' foot bike lane and 5' foot sidewalk.
5. No build-to-zone shall be required on Estes Drive due to the flood sensitivity and proximity to the FEMA floodway where buildings may not encroach.
6. As of the date of these Design Standards, a ZCP and minor modification to the current SUP is under review and shall not be subject to these Design Standards unless future modifications occur following completion of the pending minor modification and subsequent construction related to the minor modification.
7. The service lane between Pod A and Pod B currently functions as a service area for commercial tenant's trash, grease disposal, loading/unloading and to access parking. Given that the function of that alley is not intended to change, certain modifications to the Design Standards have been requested herein to specifically address how new development interacts with the alley versus how it would otherwise interact with a street (public or internal).
8. The property contains several mature trees around the perimeter, which are intended to remain. As such, all tree requirements shall allow flexibility to address those mature trees, which may require modifications from the required standards contained herein.
9. Outdoor Amenity Space shall be provided for the entirety of the property at a minimum ratio of 4.6% of gross land area and shall be distributed throughout the site. Each Pod shall be required to provide Outdoor Amenity Space at a minimum ratio of 3.6% of Pod area, so long as overall property minimum is met.
10. For any NCDOT roads, streetscape & landscaping requirements are subject to NCDOT approval and may be modified as needed so long as the Town Manager agrees with the requested changes.
11. Note that these Design Standard street frontage requirements shall take the place of perimeter buffer requirements that are outlined in LUMO Section 5.6.
12. Note that if a building fronts the Main Street it is exempt from other frontage/BTZ requirements.
13. The required percentage of building facade in the BTZ is measured by the width of the building along the street frontage divided by the buildable width of the lot. Since the project may occur in phases, the buildable width of the lot shall only include the portion of the lot included and being disturbed in any such phase of

(Continued on following page)

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS (CONTINUED...):

construction. If any such phasing creates a non-conforming frontage request until a future phase is built, a Design Alternative may be reviewed and approved by the Town Manager.

14. Co-working space shall be permitted under Business, Office-Type.
15. There may be a need for flexibility in the internal main street driveway section to accommodate specific urban amenities and groups that will utilize this space such as seasonal events, activity lawn events and larger community gatherings. Note that if a Design Alternative is proposed for this example streetscape section, the Town Manager can approve the alternative so long as it meets the overall intent.
16. Retail kiosks and other structures may be placed in the Outdoor Amenity Space. Retail kiosks shall include buildings of less than 2,500 SF of enclosed heated GLA. Enclosed heated GLA of kiosks shall not count towards required Outdoor Amenity Area(s).
17. Existing parking and vehicular access areas that are generally remaining the same will, where feasible, improve the landscaping of these areas. Any newly proposed parking areas (where parking or impervious cover did not previously exist) will be subject to all landscape requirement noted within this document, as well as any stipulations of the SUP. If the applicant and the Town Manager disagree, the Town Manager shall ultimately determine whether or not changes to the parking fields are considered 'new'.
18. The minimum parking requirement (vehicle or bicycle) may be reduced by up to 30% for shared parking between a mix of uses. Any reduction to the minimum parking shall be reviewed and approved by the Town Manager.
19. All landscaping for parking areas shall meet requirements set forth in the Town of Chapel Hill Land Use Management Ordinance (LUMO) and Design Manual, with the exception of any modifications, variances or alterations that are approved with the Special Use Permit.
20. When Outdoor Amenity Space is located in the build-to zone and used to create inviting space along a street facing facade, the width of the Outdoor Amenity Space measured perpendicular to the right-of-way may be less than the dimension prescribed in those criteria notes, subject to approval of a Design Alternative.
21. A minimum tree canopy coverage shall be provided through a combination of retained and replanted trees. The site requires an overall total 30% tree canopy coverage, with at least 20% coverage required on-site. The remaining 10% is allowed to be provided either off-site or by payment in lieu (per LUMO 5.7.2(c)). The 20% required on-site tree canopy coverage may be implemented in phases, so long as each phase will meet the overall intent and is approved by the Town Manager. Calculations demonstrating compliance with Chapel Hill Land Use Management Ordinance (LUMO) Section 5.7.2 shall be included. Progress towards the overall tree canopy coverage requirement shall be demonstrated in the planting calculations provided for each phase of development.
22. The site allows a total maximum impervious surface ratio of 75%. The 25% required pervious surface amount for the site can be implemented in phases; each phase is not required to meet this requirement individually. If a phase does not meet the requirement, a plan showing overall strategy to meet the requirement shall be presented and approved by the Town Manager. Each phase of development shall include a summary chart indicating the impervious area of that specific phase and the progress towards the overall maximum impervious surface ratio.

(Continued on following page)

SUMMARY OF MINOR CHANGES, MODIFICATIONS AND/OR DESIGN ALTERNATIVES REQUESTED IN THESE DESIGN STANDARDS (CONTINUED...):

23. In order to regulate use, categories of uses have been established above, providing a systematic basis for assigning land uses to appropriate categories with other similar uses. Use categories classify land uses and activities based on common functional, product or physical characteristics. The Town Manager is responsible for categorizing all uses into the use categories above. When determining the use category in which a proposed use fits, the Town Manager must consider the following criteria:
- a. the actual or projected characteristics of the proposed use
 - b. the relative amount of site area or floor area and equipment devoted to the proposed use
 - c. relative amounts of sales
 - d. the customer type
 - e. the relative number of employees
 - f. hours of operation
 - g. building and site arrangement
 - h. types of vehicles used and their parking requirements
 - i. the number of vehicle trips generated
 - j. how the proposed use is advertised
 - k. the likely impact on surrounding properties
 - l. whether the activity is likely to be found independent of the other activities on the site
 - m. where a use not listed is found by the Town Manager not to be similar to any other permitted use, the use is not permitted. No build-to-zone shall be required on Estes Drive due to the flood sensitivity and proximity to the FEMA floodway where buildings may not encroach.